

TOWN OF PARKER

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COMMERCIAL, INDUSTRIAL, & MULTIFAMILY
RESIDENTIAL DESIGN STANDARDS



NOVEMBER 1, 1999
Updated September 22, 2003
Updated June 16, 2008

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ACKNOWLEDGEMENTS

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INTRODUCTION

I. INTRODUCTION

With the explosion of development in Parker and concern with Parker's changing image and identity, the Town has developed design standards to provide the Planning Department, Planning Commission, Town Council and developers with an objective basis on which to evaluate and/or create new or re-development. The Town of Parker citizens have become increasingly sensitive to the potential for a proliferation of disparate or mediocre commercial and industrial development throughout the Town. The Town Council, Planning Commission and Planning Staff agree that the current Land Development Code (LDC) does not provide the necessary tools to evaluate and coordinate developments. The LDC contains generic regulations regarding permitted uses, parking standards, setbacks, height limits, landscaping, etc. The regulations are based solely on zoning classification, with no provisions for special districts or prominent locations within Town. The LDC is therefore limited in scope because it focuses only on individual properties without regard to how they relate to one another or work together to enhance the community's image and identity.

The Town of Parker Commercial, Industrial and Multifamily Residential Design Standards address this issue by providing information for site planning and architecture that allow for flexibility in individual building design while promoting a cohesive image of quality. These design standards incorporate the idea that the Town of Parker strives to be a diverse community with a refreshing mix of architectural styles, materials, and forms. And, fostering and maintaining a community with a rich blending of designs that compliment each other and build economic value is more important to Parker citizens than promoting one particular design style.

The standards are based upon commonly accepted principles of urban design and reflect the goals for the Town of Parker as set forth in the Town of Parker Master Plan. The standards address the following: (1) site design standards such as adjacent sensitivity, site development, building location and orientation, access, parking, landscaping and screening, and (2) architectural standards, such as mass and scale, architectural elements, colors, and roofing.

Purpose of the Design Standards

The intent of the design standards is to provide clear and concise direction to developers and property owners in order to promote quality and preserve value. The standards will be used as a tool in evaluating development submittals for all commercial, light industrial or multifamily projects. Specifically, the following standards will be used in the site review process for new development as well as significant architectural renovation or restoration of existing developments. Projects include renovations to existing buildings, adaptive reuse and/or change of existing use, construction of new building, and landscape improvements.

Property owners, developers and architects shall use these standards when preparing site plans and architectural plans for new development and for improvements to existing development. All standards contained in this document shall be reviewed and special care taken to address all situations where standards are applicable to a specific project. In addition, the standards shall be referenced when master planning a long-range, phased development project. Property owners and developers should check with the Planning Department to determine which projects are subject to a formal review process.

Relationship of the Design Standards to Other Planning Documents

These Design Standards are in addition to the standards and requirements identified in the Land Development Code. New developments, alterations to existing structures and changes of use shall require that the properties be in compliance with the Land Development Code and the Commercial, Industrial and Multifamily Design Standards. Modification of an existing building or change of use in an existing building shall be reviewed in accordance with section 13.04.230 Nonconforming Situations as stated in the Land Development Code or as may be amended from time to time. While these standards are intended to be consistent with the Land Development Code there may be occurrences where there is a conflict between the two documents. In the event that a conflict is discovered, the stricter of the two standards shall apply.

II. HOW TO USE THE DESIGN STANDARDS

The Design Standards are organized by geographic planning areas. These include General Areas, Neighborhood Centers, Community Centers, Regional Centers and Corridors, as shown in Figure 1.1. General Areas are all areas within the Town of Parker that are not specifically single family residential areas. General Areas standards are minimum standards of design quality intended to ensure coordinated development throughout the Town. Neighborhood, Community, and Regional Centers and Corridors are areas that serve unique functions within the Town of Parker and are deserving of site specific design standards. Centers and Corridors are prominent locations which contribute to the Town of Parker's unique sense of place. They are the the Town's main entryway streets and primary intersections, the visual quality of which has a significant effect on one's perception of the image and identity of the Town. The distinguishing features of these highly visible areas are significant factors in defining the character of the community as a whole.

General Area design standards are described in Sections III - V. All property within the Town of Parker shall be subject to the General Area Standards.

Center and Corridor design standards are described in Sections VI - VIII. In addition to meeting all General Area Standards, a development proposal for property located within one of the Neighborhood, Community or Regional Centers and/or Parker Road Corridor listed below, is also subject to the standards for the Center or Corridor in which it is located. In some cases these are new standards and in other cases they are intensifications of existing standards and will apply instead.

See Figure 1.2 for step-by-step instructions for using this document.

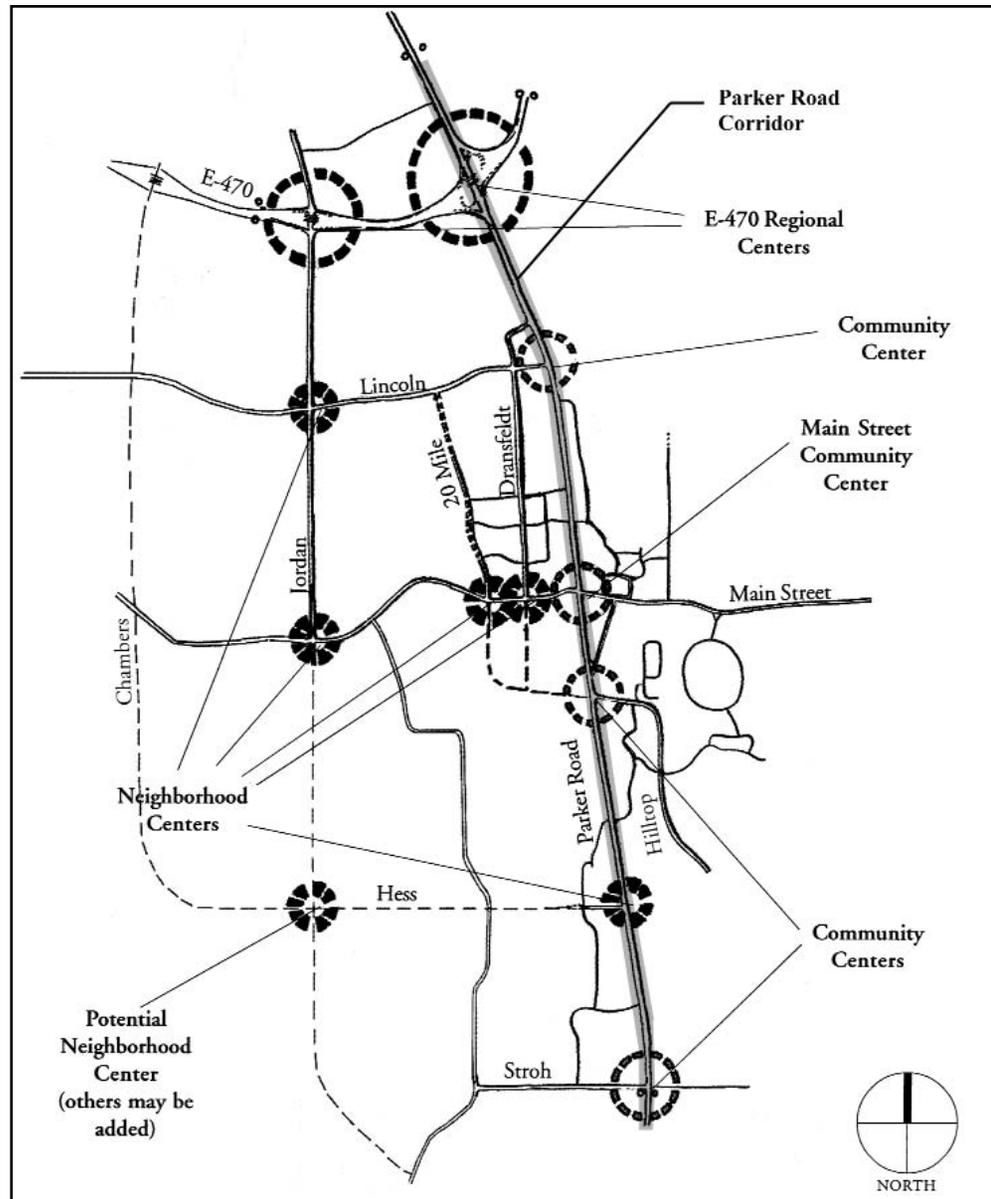


Figure 1.1: Map of Centers and Corridors

Steps to Using the Design Standards

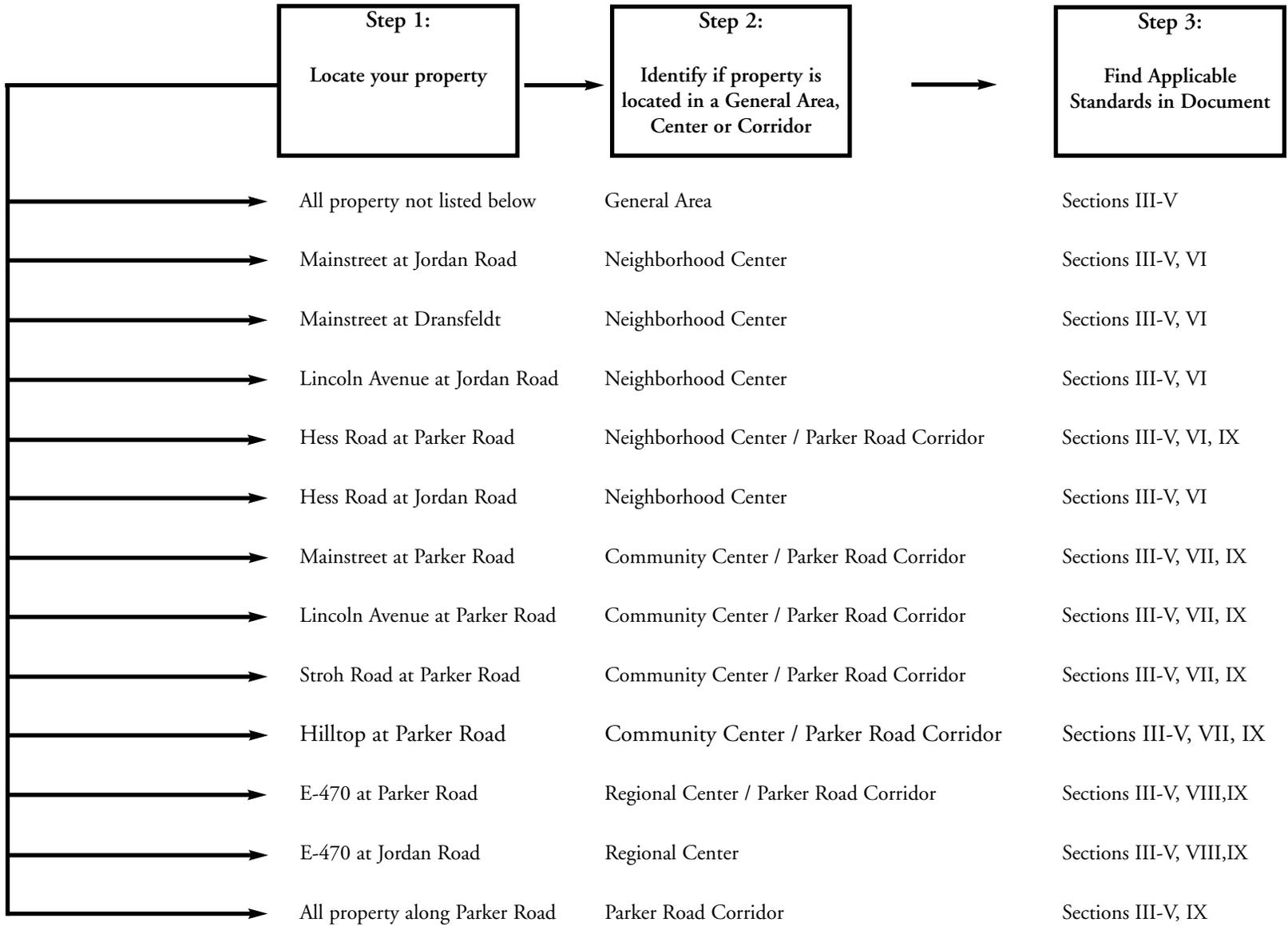


Figure 1.2: Steps to using the design standards

III. COMMERCIAL
III. COMMERCIAL



III. COMMERCIAL

GOALS AND OBJECTIVES

The primary goals for this area are to:

- Foster designs that reflect the Town of Parker's unique image and physical character.
- Prevent the development of generic architecture.
- Encourage development that presents an image of high quality and value.
- Set minimum quality standards for site development, organization, relationship to adjacent properties and building architecture.
- Develop sites in a manner that is sensitive to the existing and adjacent topography, maintaining panoramic views to the west.

III.1. Site Design Standards

III.1.A. Adjacent Sensitivity

Intent: Protect significant natural features and resources in order to minimize the impacts of development on the environment and create open space amenities, when possible. Examples of such areas include the Cherry Creek Corridor, Black Forest, and hillsides.

- 1) Buildings on sites adjacent to stream corridors and wetland areas shall be set back a minimum of 20 feet from the edge of the 100 year floodplain as shown on the most current FEMA map or from the edge of wetlands as shown on the most current Corps of Engineers map.
- 2) The design of site improvements shall minimize cut-and-fill in order to preserve the general character of the existing terrain and to minimize drastic differences between adjacent developed and undeveloped sites.
- 3) Views from open space and/or recreational areas shall be protected by attractive screen walls or landscaping. Development shall be buffered and screened from adjacent open space and/or recreational areas in the same manner as from an adjacent sensitive use, i.e. single family residential. See Section III.1.H. for detailed screening standards.
- 4) Development sites adjacent to open space and/or recreational areas shall limit the height of retaining walls to 4'-0". Terraced walls landscaped with live plantings between walls shall be permitted.
- 5) Development sites that include visually significant ridgelines shall limit the extent of building on top of the ridgeline, or rooflines that protrude above the ridgeline to 20 percent of the total length of the ridgeline within the property boundaries. Visually significant ridgelines are the top 25% of a natural topographic ridge that rises at least 50' above the adjacent land over a distance of 150' or less.

III.1.B. Site Development

Intent: Establish a set of site design principles that will provide a cohesive framework for development. Encourage a diversity of development forms and scales by providing for a variation in site plan designs.

- 1) Developments are encouraged to utilize creative, place-making, street sensitive site organization.
- 2) Site planning shall respect the relationship of the site to existing and proposed buildings and streets.
- 3) Development shall respect existing land slopes and respond to

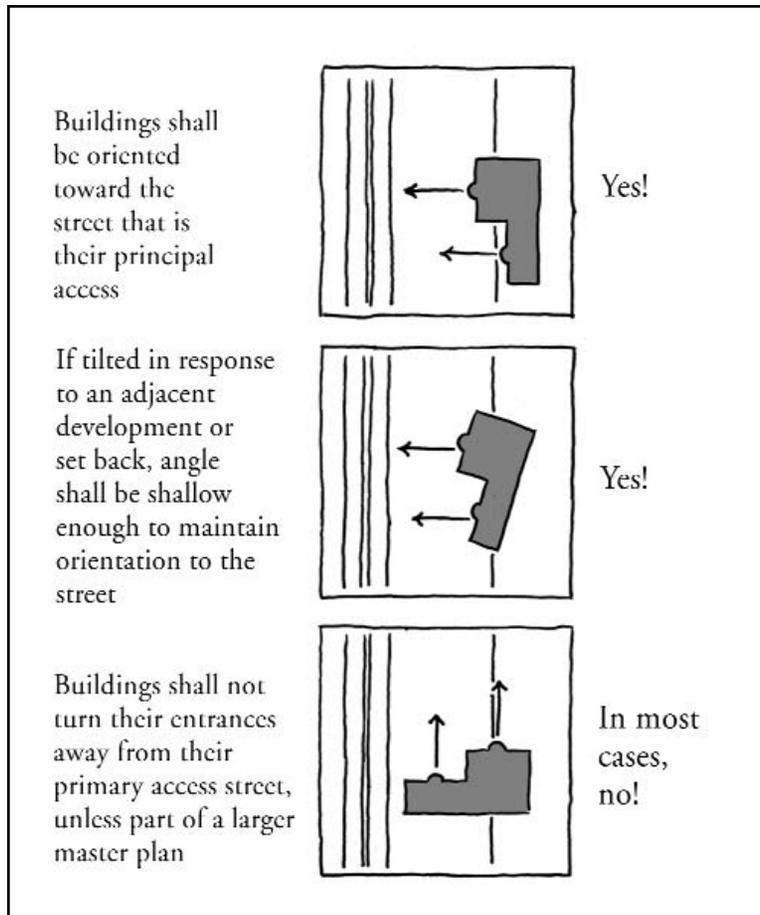


Figure 3.1: Entry facade orientation

significant natural features such as rock outcroppings, mature stands of healthy trees, etc.

- 4) Site planning and design shall, to the extent possible, preserve existing views and vistas.

III.1.C. Building Location and Orientation

Intent: Orient primary structures to the primary street and compliment existing, adjacent development to create a coordinated and visually attractive streetscape.

- 1) As depicted in Figure 3.1, entry facades shall orient towards the primary street or the active pedestrian zone within the site to create an inviting image and consistent front and street edge definition.
- 2) Developments shall be encouraged to locate buildings near the primary street(s) with most of the parking on the side or rear of buildings.
- 3) Garage bays shall orient away from primary streets.
- 4) Where development occurs at intersections, corners shall be identified with significant landscaping and buildings shall be oriented to relate to the street corner, as shown in Figure 3.2.
- 5) For multi-tenant buildings, at least 50 percent of the building group's walls which front most pedestrian activity, shall be oriented towards the primary street, as shown in Figure 3.3.
- 6) Buildings shall be grouped in ways which create positive space or "rooms" to accommodate parking and site circulation as well as to complete spaces established by adjacent developments, as shown in Figure 3.4.
- 7) Entrances shall be visible and accessible from pedestrian sidewalks.

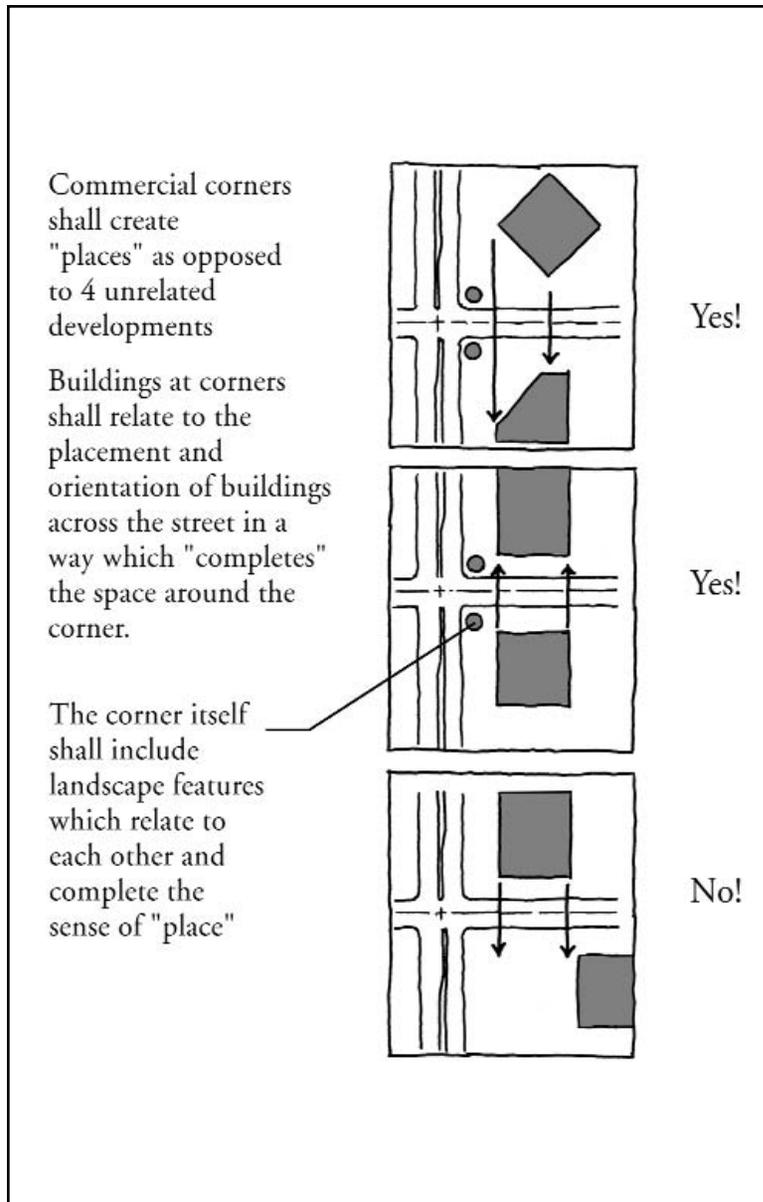


Figure 3.2: Corner orientation

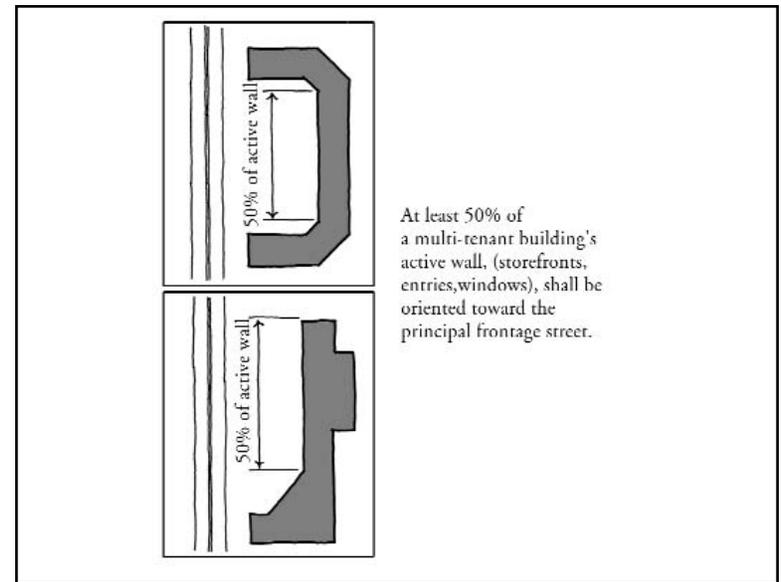


Figure 3.3: Multi-tenant building orientation

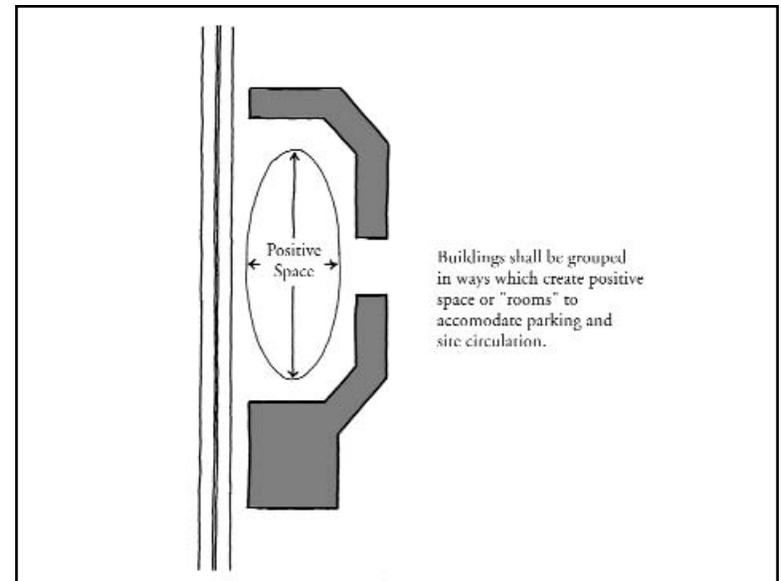


Figure 3.4: Orientation to create positive space

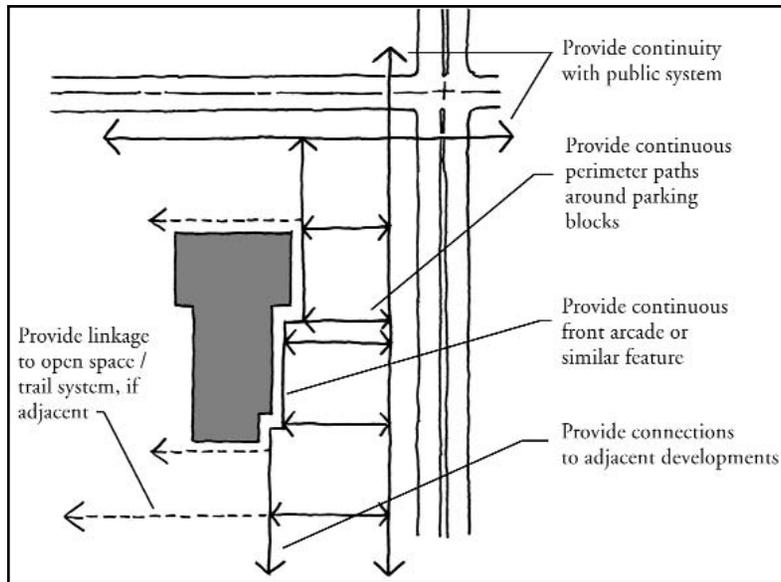


Figure 3.5: Pedestrian access

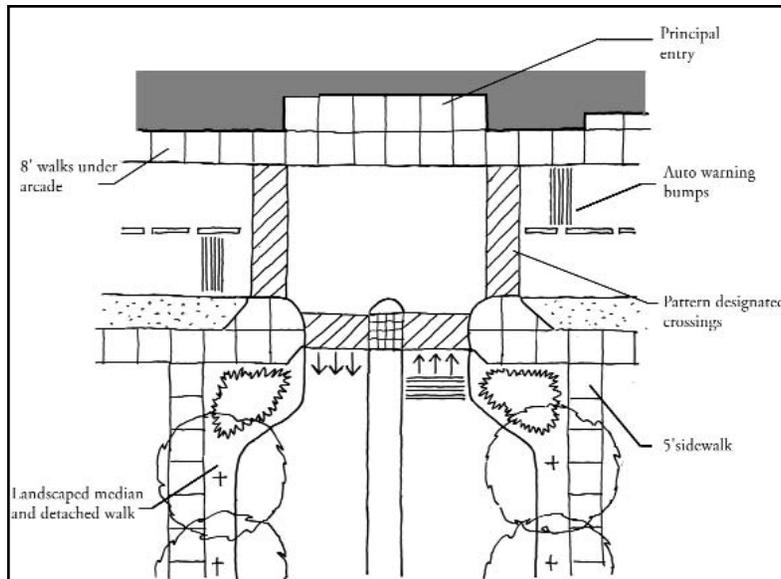


Figure 3.6: Pedestrian features

III.1.D. Pedestrian Access

Intent: Create a safe, continuous pedestrian network that minimizes conflict with automobile movement while promoting a convenient option for pedestrian movement within and between developments. See Figures 3.5.

- 1) Continuous site perimeter pedestrian walkways within a development site, no less than 5 feet in width, shall be provided from the public sidewalk or right-of-way to the principal customer entrance of all principal buildings on the site. At a minimum, walkways shall connect focal points of pedestrian activity such as, but not limited to, transit stops, street crossings, open space, building and store entry points, and adjacent pedestrian systems.
- 2) All parking lots shall have pedestrian crosswalks that are distinguished from driving surfaces through the use of durable, low maintenance surface materials such as pavers, bricks, or scored, colored concrete to enhance pedestrian safety and comfort, as well as the attractiveness of the walkways. See Figure 3.6.
- 3) For parking lots containing 120 parking spaces or more, continuous parking lot pedestrian walkways shall be located in landscaped medians that include trees and grasses or shrubs and groundcover, or other such materials for no less than 50 percent of its length. Landscaped walkways shall count toward interior parking lot landscape requirements in Section III.1.G.
- 4) Sidewalks no less than 8 feet in width shall be provided along the full length of the building along any façade featuring a customer entrance, and along any façade abutting public parking areas.
- 5) Internal pedestrian walkways provided in conformance with the above standard shall provide weather protection features such as awnings, arcades, or pergolas within 30 feet of all customer entrances.
- 6) Pedestrian connections to adjacent developments shall be provided. If adjacent properties are undeveloped, site plans shall indicate areas for future pedestrian connections to adjoining parcels.

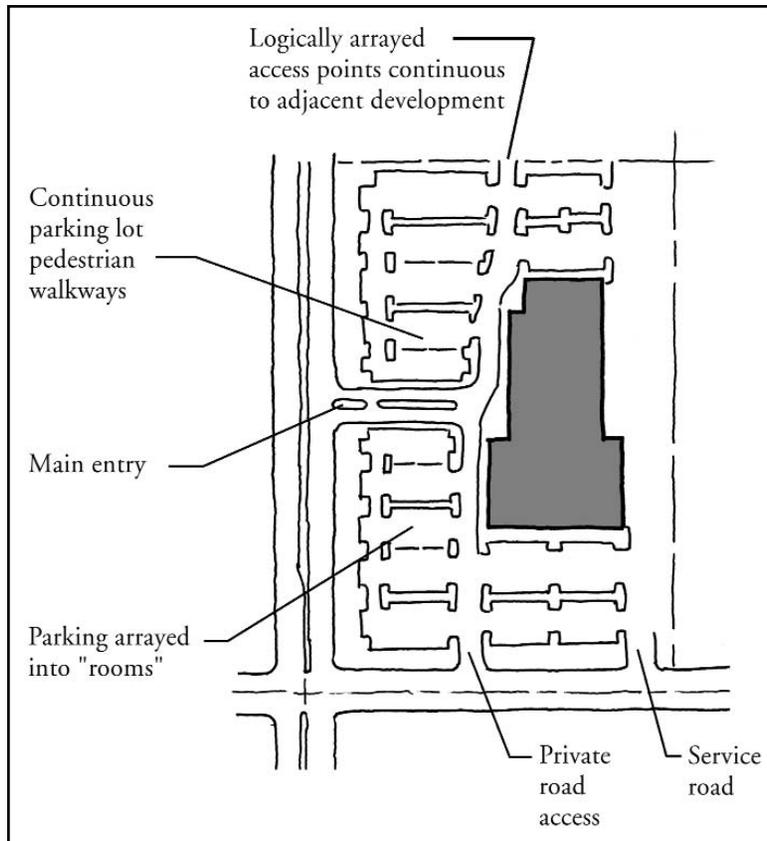


Figure 3.7: Vehicular access

III.1.E. Vehicular Access

Intent: Create an automobile circulation system that provides for safe and efficient movement within and between properties and minimizes impacts of commercial traffic on residential properties. See Figure 3.7.

- 1) Connections between private access driveways or roads shall be provided by siting a logical array of access points.
- 2) Main drive aisles must be continuous and connect to the main entrance of the site.
- 3) Vehicular access between adjacent properties shall be provided whenever possible, but vehicular speeds shall be limited through speed calming measures such as stop signs, access that is out of alignment, or speed bumps.
- 4) Commercial drives or roads shall not align with residential access drives. Exceptions can be made for mixed use developments that integrate residential and commercial uses.
- 5) When the opportunity exists, common or shared public entries shall be provided.
- 6) To the extent possible, shared service and delivery access shall be provided between adjacent parcels and/or buildings.

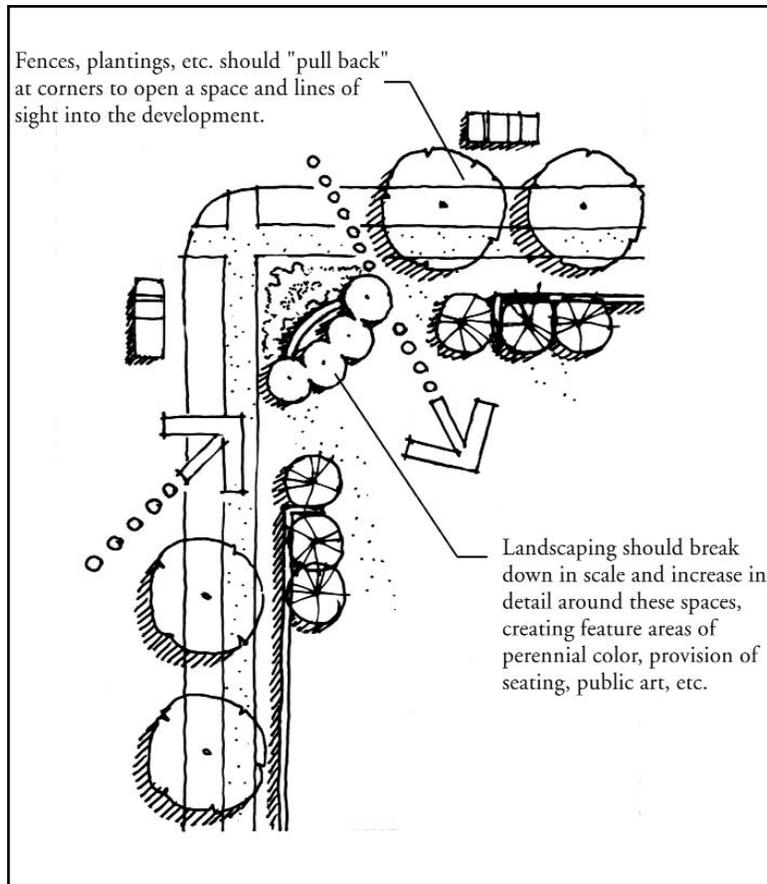


Figure 3.12: Corner landscaping

III.1.F. Screening

Intent: Screening shall effectively mitigate negative visual and acoustic impacts of commercial uses and shall be integrated into each project's overall site design.

- 1) Loading docks, truck parking, outdoor storage, utility meters, HVAC equipment, trash collection, trash compaction, and other service functions shall be screened and integrated into the overall design of the building and the landscaping so that the visual and acoustic impacts of these functions are fully contained and out of view from adjacent properties and public streets.
- 2) Architectural elements, materials, colors, and design of screening walls, coverings, and/or fences shall conform to those used as predominant materials, colors and elements on the building or landscaping.
- 3) All rooftop mechanical equipment and vents greater than eight inches in diameter shall be screened. Screening may be done either with an extended parapet wall or a free-standing screen wall of a material and color matching the building. Screens shall be at least as high as the equipment they hide.

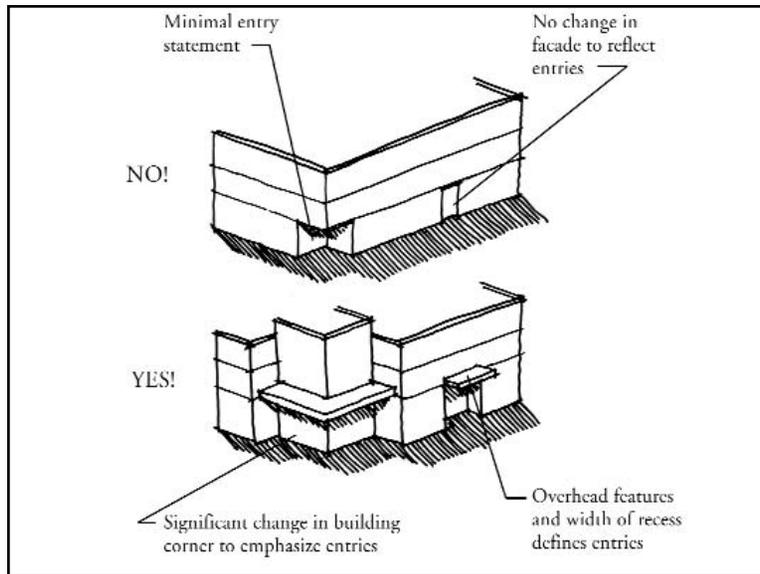


Figure 3.13: Building mass at entries

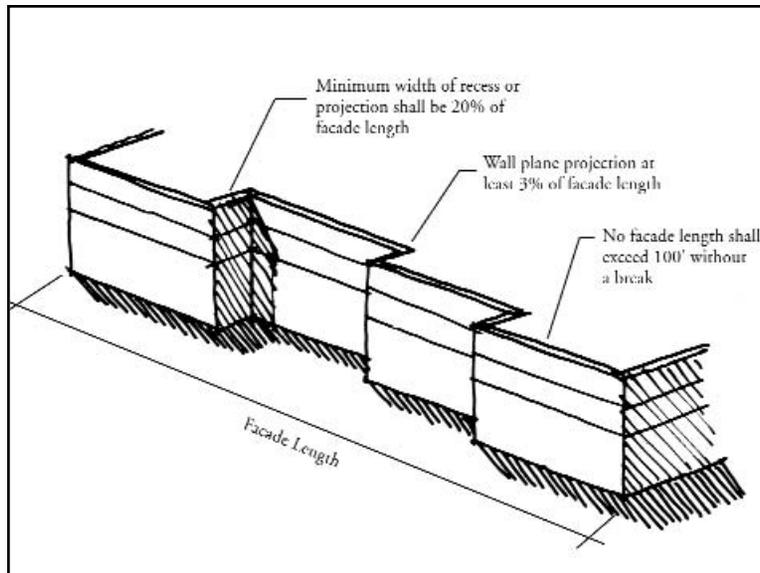


Figure 3.14: Facades greater than 100'

III.2 Architectural Standards

III.2.A. Mass and Scale

Intent: Building mass, form, length and proportions shall be designed to provide variety of visual interest, maintaining a human scale that is appropriate to surroundings.

Multi-Tenant and Big Box Retail Buildings

The following standards shall apply to all single-story commercial buildings containing 65,000 square feet or more of gross floor area:

- 1) A breakdown of building mass is required at all significant entryways and walls that front pedestrian activities to mark entryways and to provide a human scale. See Figure 3.13.
- 2) Building facades shall meet one of the following standards:
 - (a) facades greater than 100 feet in length shall incorporate wall plane projections or recesses having a depth of at least 3 percent of the length of the façade and extending at least 20 percent of the length of the façade. No uninterrupted length of facade shall exceed 100 horizontal feet, as shown in Figure 3.14; or
 - (b) each building façade shall have a repeating pattern that includes no less than three instances of either (1) color change, (2) texture changes, (3) material module change, or (4) expression of an architectural or structural bay through a change in plane no less than 12 inches in width, such as an offset, reveal, or projecting rib. At least one of those elements shall repeat horizontally at an interval of not more than 30 feet, as shown in Figure 3.15.; or
 - (c) Standards III.2.A.2(a) and III.2.A.3(b) may be waived if the applicant can demonstrate an alternative building design which

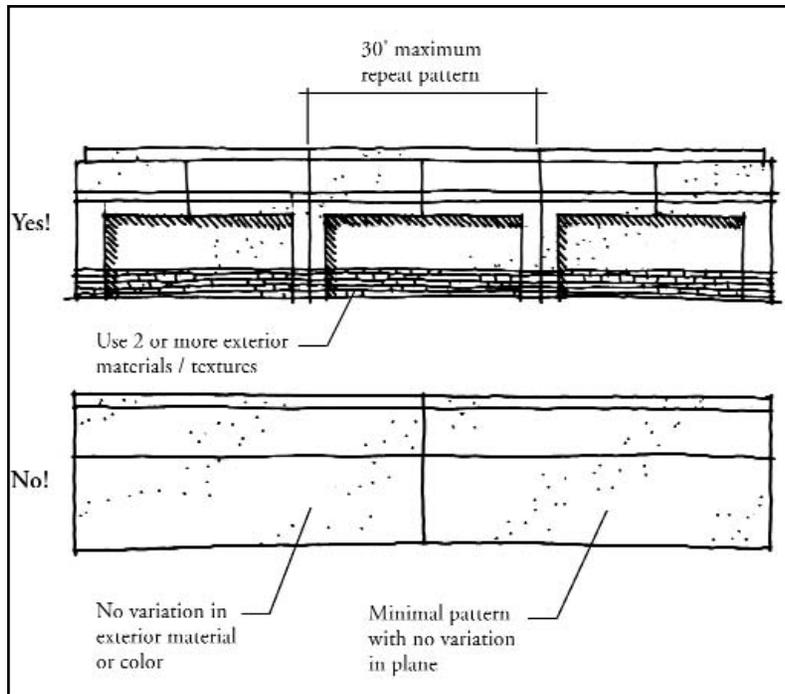


Figure 3.15: Building facade pattern

significantly articulates a wall plane.

Multi-Story Buildings

The following standard shall apply to all buildings greater than two stories:

- 3) The composition of the building shall present a clear base, middle, and top or a clearly defined alternative building composition, as shown in Figure 3.16.
- 4) The building mass shall break down and be clearly articulated to differentiate the first floor portion of the building from the remaining mass.
- 5) Horizontal rhythms, such as openings and articulations shall logically align between levels.

III.2.B. Architectural Elements and Articulation

Intent: Create an image of high quality development. Encourage a variety of architectural elements that avoid featureless design and uninterrupted repetition of building materials and colors. Facades shall be integrated and articulated to reduce the massive scale and the uniform, impersonal appearances of large commercial buildings and provide visual interest that will be consistent with the community's identity, character, and scale.

- 1) As shown in Figure 3.17, ground floor facades facing a primary access street shall have clearly defined, highly visible customer entrances that feature no less than three of the following: canopies or porticos, overhangs, recesses/projections, arcades, raised corniced parapets over the door, distinctive roof forms, arches, outdoor patios, display windows, integral planters or wing walls that incorporate landscaped areas and/or places for sitting.
- 2) Architectural details such as tile work, metal work, brick applied in two or more colors, textures, corbels, or other projections and moldings which are integrated into the building structure and design, shall be used as façade graphics to further articulate walls, as shown in

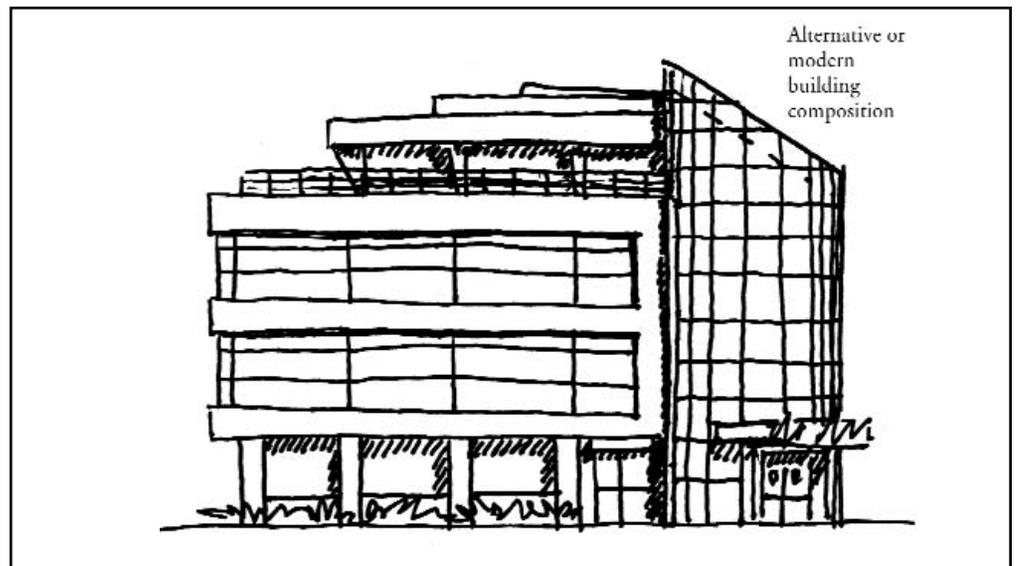


Figure 3.16: Building composition

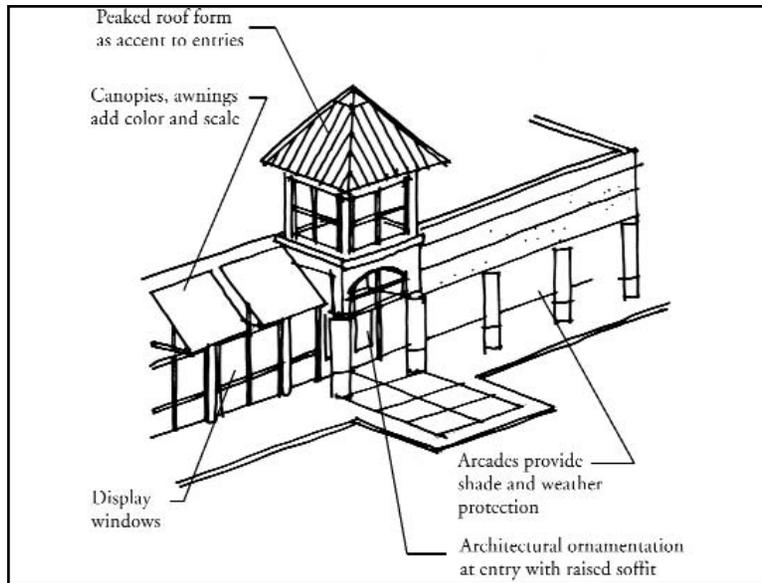


Figure 3.17: Customer entries

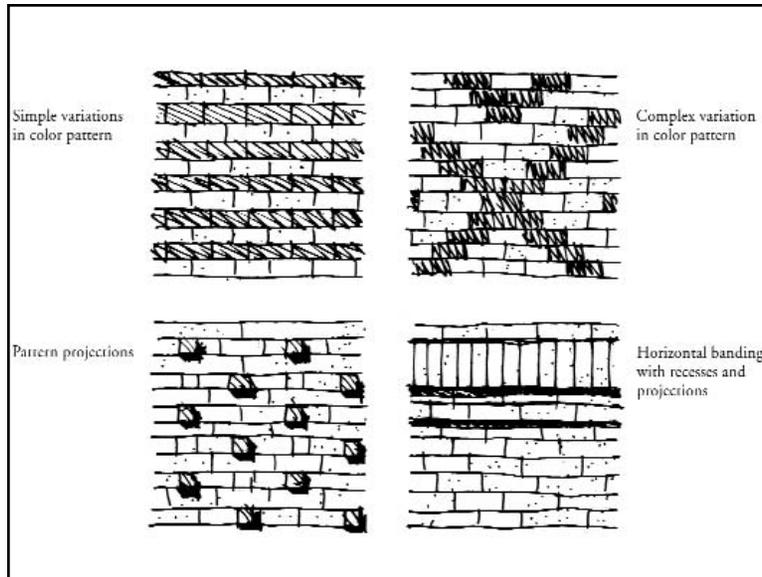


Figure 3.18: Wall articulation

Figure 3.18.

- 3) Side facades which are visible from adjoining properties and/or public streets shall comply with one of the two requirements of the above facade standards.
- 4) Rear building facades shall contain a simplified expression of the same materials and patterns used on other sides of the building. Blank, featureless walls are prohibited.
- 5) For hotels and motels, all stairwells and upper floor circulation components of the building shall be completely enclosed within the building envelope to create a secure space.

III.2.C. Building Materials

Intent: Building materials shall present an image of high quality and permanence.

- 1) Predominant exterior building materials shall be high quality, durable material such as, but not limited to: brick, wood lap siding, sandstone or other native stone, integrally colored, textured, or glazed concrete masonry units, pre-finished metal panel systems, high quality prestressed concrete systems, and water managed Exterior Installation Finish Systems (EIFS).
- 2) Exterior building materials shall not include the following: split shakes, rough sawn or board and batten wood, smooth-faced gray concrete block, painted concrete block, tilt-up concrete panels, field painted or pre-finished standard corrugated metal siding, standard single or double tee concrete systems, or barrier type EIFS.
- 3) Standards III.2.C.1 and III.2.C.2 can be waived if a project demonstrates the application of a material which satisfies the above intent statement.
- 4) All sides of the building shall include materials consistent with those on the front.
- 5) For hotels and motels, façade area at least equal to the total façade area of the first floor walls, shall be surfaced in brick or natural stone. The remainder of the exterior may be surfaced in stucco, water managed EIFS, integrally dyed decorative concrete or ceramic masonry units.

- Metal or vinyl siding is prohibited.
- 6) Applicants are required to submit a sample material board.

III.2.D. Colors

Intent: Exterior colors shall be aesthetically pleasing and compatible with colors of nearby structures.

- 1) Color schemes shall tie building elements together and shall be used to enhance the architectural form of the building.
- 2) Color choices for all buildings shall be made from those allowed within the range delineated according to the Munsell color notation system from the Munsell Book of Color on display in the Planning Office.
- 3) The Munsell system of color notation is broken into three categories: hue, chroma and value. Chroma is the only characteristic with one set limit, which is as follows: Body color is limited to a maximum chroma of 4. Trim color is limited to a maximum chroma of 6. Accent color is limited to a maximum chroma of 8. (If yellow or red is used, body color is limited to a maximum chroma of 8, trim color is limited to a maximum chroma of 8 and accent color is limited to a maximum chroma of 10).
- 4) Intense, bright, black, or fluorescent colors shall not be used as the predominate color on any wall or roof of any primary or accessory structure.
- 5) All exterior elements of a building that are metal, such as flues, flashings, etc., shall be painted a flat color or one that is a compatible color with the building and not be left nor allowed to become bare metal. Exceptions to this standard may occur, such as for copper roofs, etc., which do not require painting.
- 6) Applicants are required to submit a color pallete board.

III.2.E. Roofing

Intent: Roof forms shall contribute to the overall image of high quality and permanence, and shall be used to screen rooftop equipment where possible.

- 1) Buildings shall have distinctive roof profiles and colors and provide a variation in roof lines and forms between developments.
- 2) Buildings with flat roofs shall be designed to create visual interest by using variations in parapet height, articulation of cornice lines, decorative scuppers or other features.
- 3) Roof forms shall be designed to correspond and denote building elements and functions such as entrances and arcades.
- 4) Visible roof surfaces shall be made of durable materials such as clay or concrete tile, tern metal, copper, slate, or other prefinished architectural metals.
- 5) Asphalt shingles, if used, shall be architectural grade.

IV. INDUSTRIAL



IV. INDUSTRIAL

Goals and Objectives

The primary goals for this district are to:

Maximize the economic benefits of industrial development while minimizing the negative impacts on adjacent uses, roadways, and natural areas.

Provide site and building amenities that create a pleasant working environment for employees.

Encourage the use of high quality materials that reflect the Town of Parker's unique physical character and sense of place.

IV.1. Site Design Standards

IV.1.A. Adjacent Sensitivity

Intent: Protect significant natural features and resources in order to minimize the impacts of development on the environment and create open space amenities, when possible.

- 1) Buildings on sites adjacent to stream corridors and wetland areas shall be set back a minimum of 20 feet from the edge of the 100 year

- floodplain as shown on the most current FEMA map or from the edge of wetlands as shown on the most current Corps of Engineers map.
- 2) The design of site improvements shall minimize cut-and-fill in order to preserve the general character of the existing terrain and to minimize drastic differences between adjacent developed and undeveloped sites.
 - 3) Views from open space and/or recreational areas shall be protected by attractive screen walls or landscaping. Development shall be buffered and screened from adjacent open space and/or recreational areas in the same manner as from an adjacent sensitive use, i.e. single family residential. See Section IV.1.H. for detailed screening standards.
 - 5) Development sites adjacent to open space and/or recreational areas shall limit the height of retaining walls to 4'-0". Terraced walls landscaped with live plantings between walls shall be permitted.
 - 6) Development sites that include visually significant ridgelines shall limit the extent of building on top of the ridgeline, or rooflines that protrude above the ridgeline to 20 percent of the total length of the ridgeline within the property boundaries. Visually significant ridgelines are the top 25% of a natural topographic ridge that rises at least 50' above the adjacent land over a distance of 150' or less.

IV.1.B. Site Development

Intent: Establish a set of site design principles that will provide a cohesive framework for development. Encourage industrial development that respects and protects the value of adjacent property and uses.

- 1) Site plans must demonstrate sensitivity to the protection of both existing and future adjacent developments.
- 2) To the extent possible, multiple buildings on an industrial site should be internally focused to conceal operations.
- 3) To the extent possible, buildings shall be consolidated within a site.

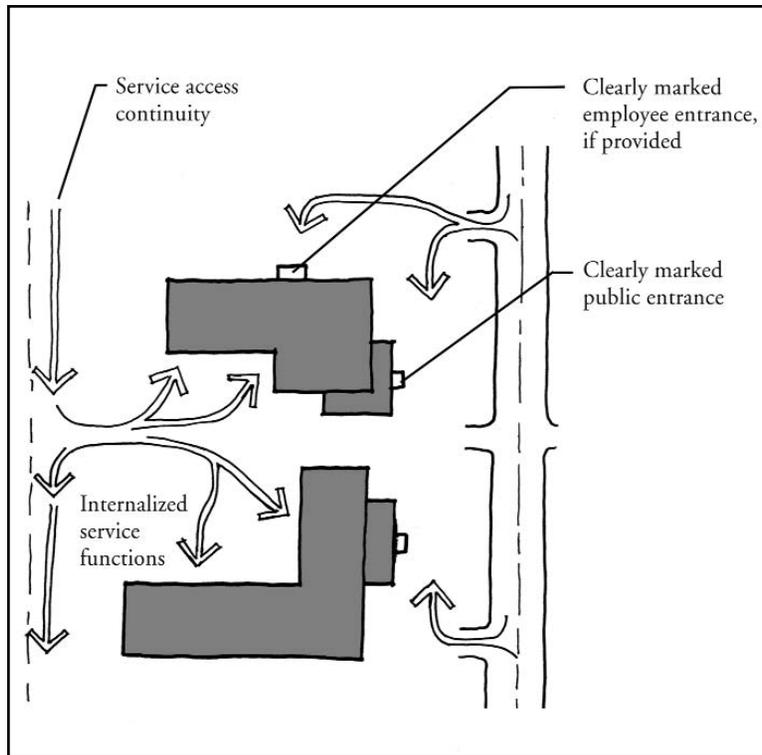


Figure 4.1: Building location and orientation

IV.1.C. Building Location and Orientation

Intent: Orient and consolidate structures to compliment existing, adjacent development and to create a coordinated and visually attractive industrial park setting. See Figure 4.1.

- 1) Development shall respect existing land slopes and respond to significant natural features such as rock outcroppings, mature stands of healthy trees, etc.
- 2) Customer entrance facades shall orient towards the primary street.
- 3) For buildings with multiple entrances, customer entrances shall be designated and differentiated from service or employee entrances.
- 4) For businesses with low customer interface, buildings should be positioned on a site to maximize setbacks from primary streets.
- 5) Accessory structures or uses shall not front a primary street and shall be oriented away from public rights-of-way, open space, and residential areas.

IV.1.D. Pedestrian Access

Intent: Create a safe, continuous pedestrian network that minimizes conflict with vehicle movement, (particularly trucks), while promoting a convenient option for movement to and from developments.

- 1) Continuous internal pedestrian walkways within a development site, no less than 5 feet in width, shall be provided from the public sidewalk or right-of-way to the principal customer/office entrance.
- 2) All internal pedestrian crosswalks shall be distinguished from driving surfaces through the use of crosswalk striping or other low maintenance surface materials.

IV.1.E. Vehicular Access

Intent: Create a vehicular circulation system that provides for safe and efficient transportation of goods and minimizes impacts of industrial traffic on other land uses.

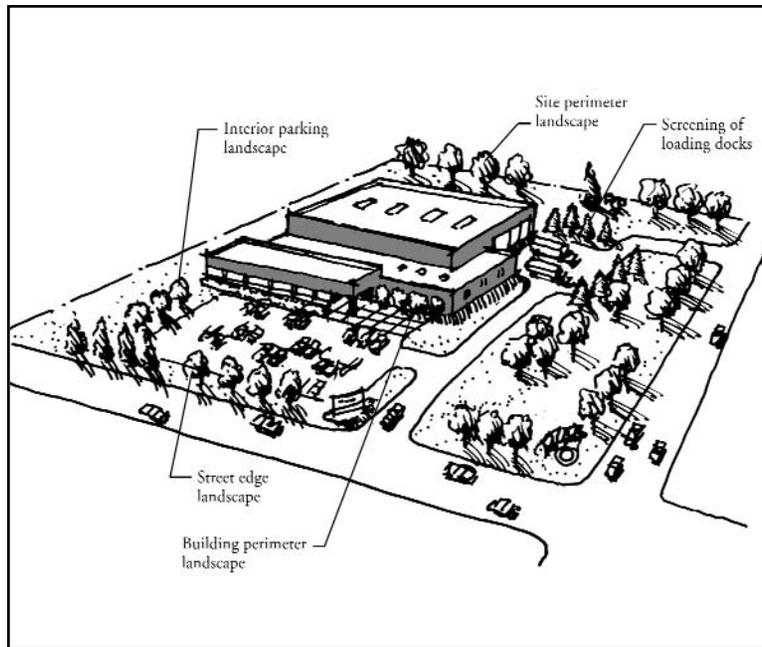


Figure 4.2: Industrial areas to be landscaped

- 1) To the extent possible, separate car and truck access drives shall be provided.
- 2) Industrial drives or roads shall not align with access points into residential neighborhoods or residential access drives.

IV.1.F. Screening

Intent: Screening shall effectively mitigate negative visual and acoustic impacts of industrial uses and shall be integrated into each project's overall site design.

- 1) Loading docks, truck parking, outdoor storage, utility meters, HVAC equipment, trash collection, trash compaction, and other service or accessory functions shall be screened and integrated into the overall design of the building and the landscaping so that the visual and acoustic impacts of these functions are fully contained and out of view from adjacent public rights-of-way, open space or residential areas.
- 2) Architectural elements, materials, colors, and design of screening walls, coverings, and/or fences shall conform to those used as predominant materials, colors and elements on the building or landscaping.
- 3) All rooftop mechanical equipment shall be screened. An extended parapet wall or a free-standing screen wall of a material and color matching the building may be used for screening. Screens shall be at least as high as the equipment they conceal.

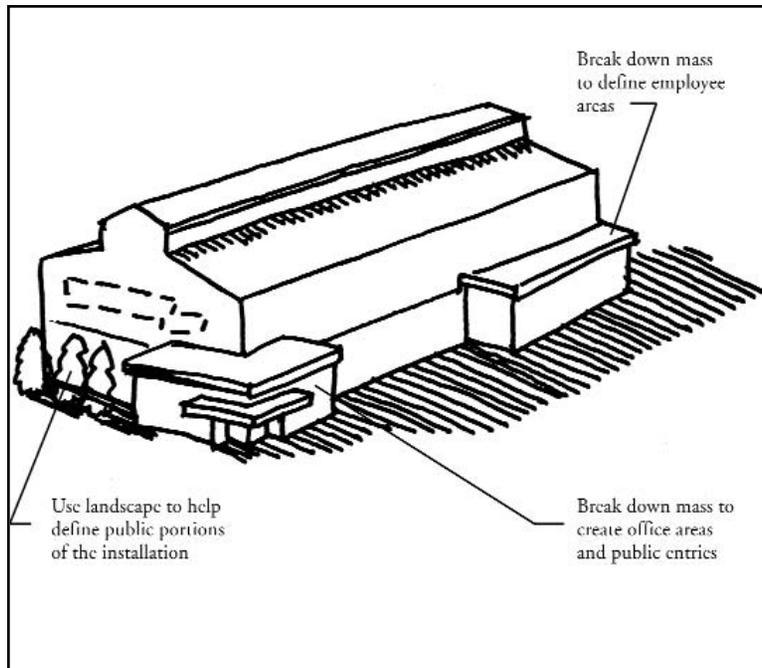


Figure 4.3: Massing of industrial buildings

IV.2. Architectural Standards

IV.2.A Mass and Scale

Intent: Building mass and height shall be designed to maintain a scale appropriate to surroundings.

- 1) The mass of the office portion of the building shall be broken down to a human scale with a strongly marked public entry and employee break or lunch areas, when provided. See Figures 4.3 and 4.4.

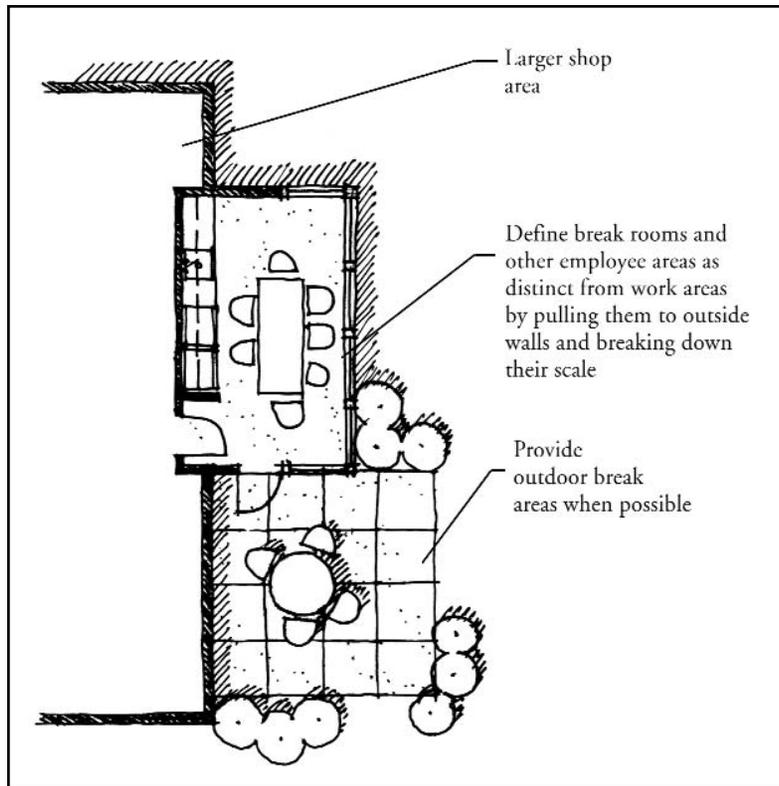


Figure 4.4: Industrial employee areas

IV.2.B. Architectural Elements and Articulation

Intent: Create an image of high quality development. Encourage a variety of architectural elements that avoid featureless design. Articulate front facades to reduce the impersonal appearance of large industrial sites and provide visual interest.

- 1) Each principal building on a site shall have clearly defined, highly visible customer entrances featuring one of the following: canopies or porticos, overhangs, recesses/projections, raised corniced parapets over the door, peaked roof forms, arches, or other unique architectural details.
- 2) Buildings shall be designed with consistent materials and details on all sides visible from public right-of-ways.
- 3) If provided, employee break areas shall include a high degree of transparency, articulation and architectural detail.

IV.2.C. Building Materials

Intent: Building materials shall present an image of high quality and permanence.

- 1) At least 30 percent of the front/office portion of the building shall be of transparent materials to differentiate the office from the remainder of the site and to create an inviting customer area.
- 2) Predominant exterior building materials shall be high quality, durable material such as, but not limited to: brick, wood, sandstone other native stone, integrally colored, textured, or glazed concrete masonry units, pre-finished metal panel systems, high quality prestressed concrete systems, and water managed Exterior Installation Finish Systems (EIFS).
- 3) Predominant exterior building materials that require substantial maintenance or are of low durability, shall not be permitted, including split shakes, rough sawn or board and batten wood, painted concrete block, field painted standard corrugated metal siding, or barrier type EIFS.

- 4) Smooth faced gray concrete block, tilt-up concrete, pre-engineered metal buildings and standard single or double tee concrete systems shall be permitted only on facades not visible from public rights-of-way, open space or residential areas.
- 5) Standards IV.2.C.2 and IV.2.C.3 can be waived if a project demonstrates the application of a material which satisfies the above intent statement.
- 6) Building materials and details used on the façade of the primary structure may be transitioned to a lesser degree of detail on service sides of the building.
- 7) To the extent possible, interior daylighting in the form of skylights or other architectural features shall be used to minimize energy use.
- 8) Applicants are required to submit a sample material board.

IV.2.D. Colors

Intent: Exterior colors shall be aesthetically pleasing and compatible with colors of nearby structures.

- 1) Intense, bright, black, or fluorescent colors shall not be used as the predominate color on any wall or roof of any primary or accessory structure. These colors may be used as building accent colors, but shall not constitute more than 10 percent of the area of each elevation of a building. All sign areas shall be excluded from this calculation.
- 2) Applicants are required to submit a color pallette board.

IV.2.E. Roofing

Intent: Roof forms shall contribute to the overall image of high quality and permanence, and shall be used to screen rooftop equipment where possible.

- 1) Visible roof surfaces shall be made of durable materials such as concrete tile, metal, other prefinished architectural metals or architectural grade asphalt shingles. Membrane systems are prohibited on any visible portion of the roof.

V. MULTIFAMILY RESIDENTIAL

V. MULTIFAMILY RESIDENTIAL



V. MULTIFAMILY RESIDENTIAL

Goals and Objectives

The primary goals for this area are to:

- Plan for a variety of housing types which establish and maintain a quality community appearance.
- Provide flexibility in adjusting to changes in housing trends and patterns, while maintaining compatibility with surrounding development.
- Ensure that high standards of siting and the optimum use of available land are applied throughout developments.
- Organize residential buildings around central amenities such as courtyards, plazas, open space and other recreational features.
- Ensure multifamily residential uses complement adjacent uses.

V.1. Site Design Standards

V.1.A. Adjacent Sensitivity

Intent: Protect significant natural features and resources in order to minimize the impacts of development on the environment and create open space amenities, when possible.

- 1) Buildings on sites adjacent to stream corridors and wetland areas shall be set back a minimum of 20 feet from the edge of the 100 year floodplain as shown on the most current FEMA map or from the edge of wetlands as shown on the most current Corps of Engineers map.
- 2) The design of site improvements shall minimize cut-and-fill in order to preserve the general character of the existing terrain and to minimize drastic differences between adjacent developed and undeveloped sites.
- 3) Development sites adjacent to open space and/or recreational areas shall limit the height of retaining walls to 4'-0". Terraced walls

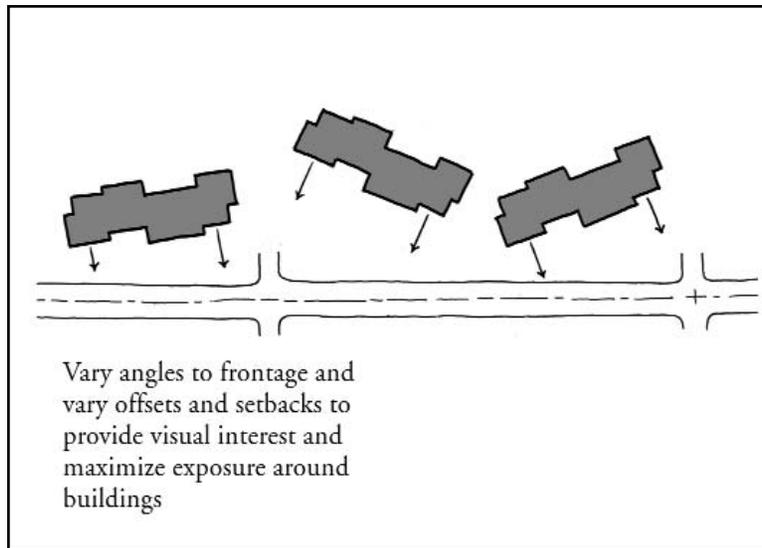


Figure 5.1: Multifamily buildings sited at varying angles

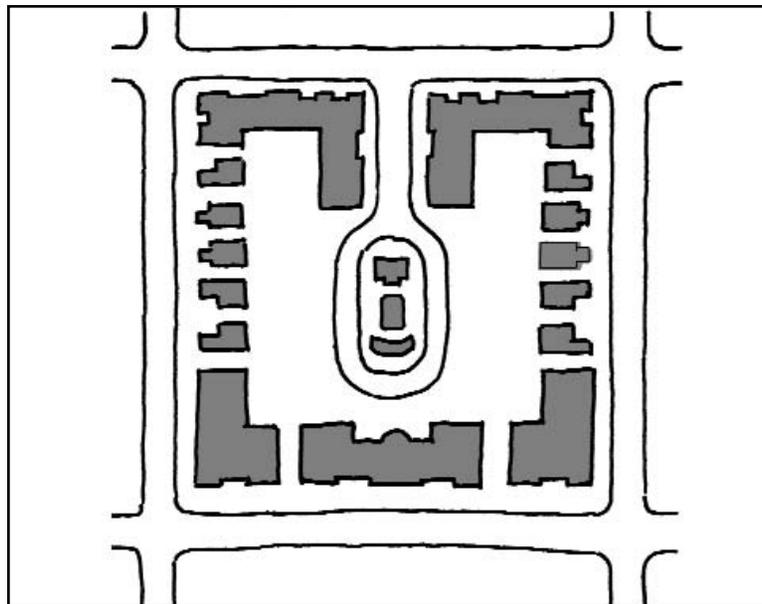


Figure 5.2: Alternative or "new urbanist" site development

- landscaped with live plantings between walls shall be permitted.
- 4) Development sites that include visually significant ridgelines shall limit the extent of building on top of the ridgeline, or rooflines that protrude above the ridgeline to 20 percent of the total length of the ridgeline within the property boundaries. Visually significant ridgelines are the top 25% of a natural topographic ridge that rises at least 50' above the adjacent land over a distance of 150' or less.

V.1.B. Site Development

Intent: Organize multifamily residential developments to create a sense of place and neighborhood.

- 1) Structures, open space, and parking areas shall be grouped or clustered to avoid the monotony of continuous rows of building walls, and to allow visual access into the development.

V.1.C. Building Location and Orientation

Intent: Orientation of buildings in multifamily developments shall reduce and minimize their apparent visual mass from adjacent single-family neighborhoods and minimize exposure to adjacent commercial or industrial areas.

- 1) In order to provide visual interest within projects, buildings shall be sited at slightly varying angles, and not lined up on minimum setbacks and/or parallel to one another or to roadways or parcel boundaries. Nor shall they be sited end-to-end, creating visual "alleys" within a project. See Figure 5.1.
- 2) Standards V.I.B.1 and V.I.C.1 can be waived if a project demonstrates the key principles of a new urbanist development, including a compact, pedestrian-oriented neighborhood, with clearly defined and centrally located useable public spaces, a grid or modified grid street pattern, and building entrances fronting streets. See Figure 5.2.
- 3) Buildings shall be oriented in a more perpendicular than parallel fashion to adjacent single family homes and commercial or industrial areas.

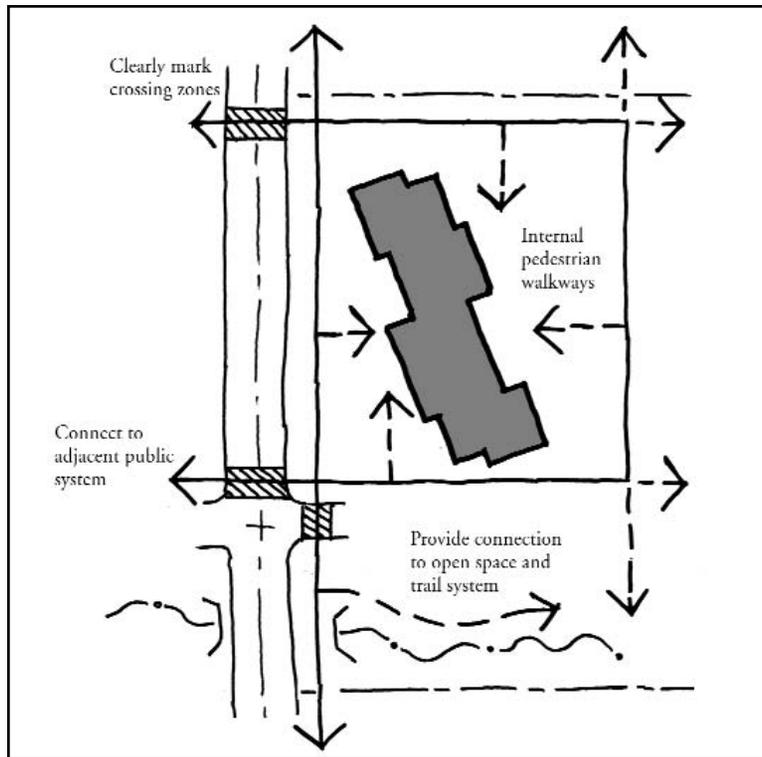


Figure 5.3: Pedestrian access

- 4) Where parallel orientation is necessary, building setbacks shall be increased.

V.1.D. Pedestrian Access

Intent: A safe, continuous pedestrian network shall be integrated into the development to minimize conflict with automobile movement while promoting inviting and convenient access within and between developments.

- 1) Internal pedestrian walkways shall be provided and connect individual developments with adjacent residential neighborhoods, community-wide open space, and trail systems. See Figure 5.3.
- 2) Sidewalks shall be provided along all public streets.

V.1.E. Vehicular Access

Intent: Create an automobile circulation system that provides for safe and efficient movement within and between multifamily properties. See Figure 5.4.

- 1) All multifamily development shall provide internal automobile connections to adjoining local or collector streets.
- 2) Multifamily development sites between 5 and 15 acres shall include a minimum of one public street or private drive, with detached sidewalks and tree lawns, that is continuous through the site, and connects to a public street on both ends (referred to as a through-access drive).
- 3) Multifamily development sites greater than 15 acres shall include a minimum of two through-access drives.
- 4) Through access drives shall be consistent with, and aligned with, local streets in adjacent existing development areas.
- 5) Landscaped parking courts, loop streets, and innovative street designs shall be encouraged to ensure privacy, safety, and visual diversity.

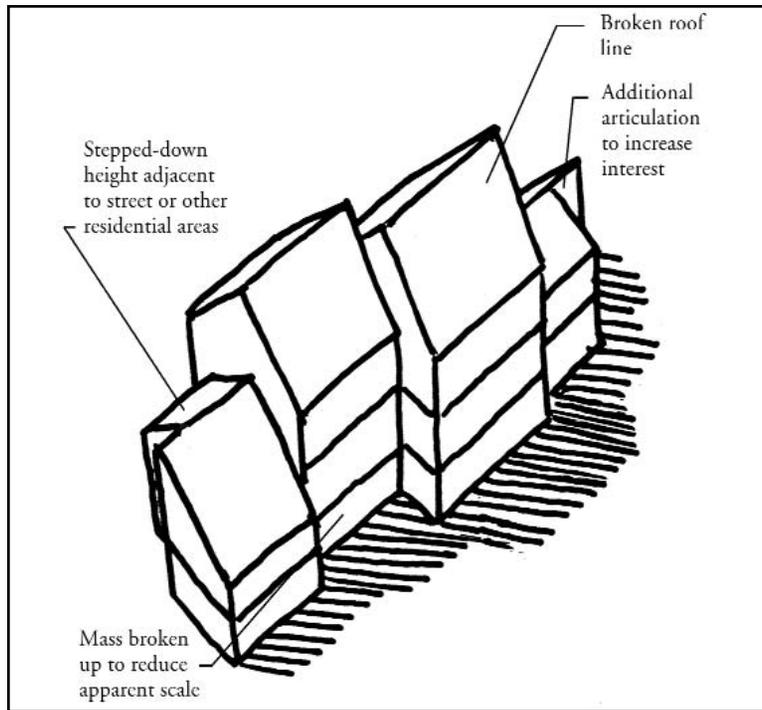


Figure 5.7: Scale of multifamily buildings

V.2. Architectural Standards

V.2.A. Mass and Scale

Intent: Multifamily buildings shall maintain a scale appropriate to surroundings. See Figure 5.7.

- 1) Building mass shall be suitable relative to both lot size and setbacks, and may require "stepped-back" designs that place greater height and mass away from the street. Height may also be stepped-down adjacent to single-family homes to provide a visual transition.
- 2) The mass of buildings shall be broken up to reduce the apparent scale, provide visual interest and depth, and achieve a more articulated form.
- 3) Building designs should incorporate visually heavier and more massive elements at their base, and lighter elements above these components. A second story, for example, should not appear heavier or demonstrate greater mass than that portion of the building supporting it.

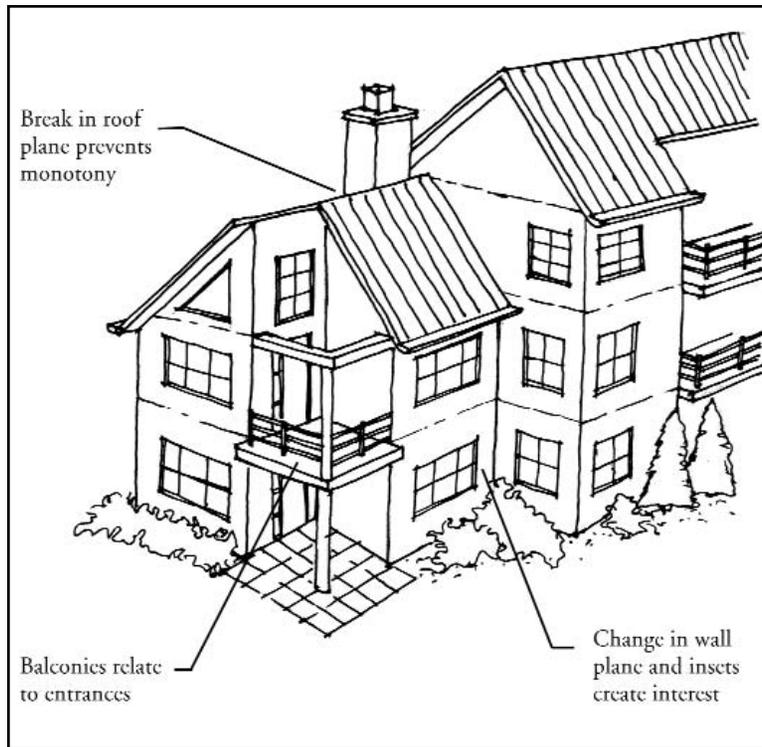


Figure 5.8: Building articulation

V.2.B. Architectural Elements and Articulation

Intent: Create an image of high quality multifamily residential developments with a sense of neighborhood.

- 1) Building fronts are encouraged to include articulations such as bays, insets, or porches or stoops related to entrances and windows. See Figure 5.8.
- 2) Façade articulation and articulation elements such as building breaks, changes in wall planes, gables, balconies, and varied architectural treatment shall be used to avoid long, monotonous walls.

V.2.C. Building Materials

Intent: Building materials shall present an image of high quality and permanence.

- 1) All multifamily developments shall be constructed with exterior building materials and finishes that are of high quality, permanence and durability such as, but not limited to: natural wood, masonite and shingle siding, other types of wood siding, stucco, brick, stone and water managed EIFS.
- 2) Exterior building materials shall not include the following: rough sawn or board and batten wood, smooth-faced or gray concrete block, painted concrete block, tilt-up concrete panels, field painted or pre-finished standard corrugated metal siding, or standard single or double tee concrete systems.
- 3) Predominant roof materials shall be high quality, durable material such as, but not limited to: wood shake shingles and clay or concrete tiles. Other materials, such as composition wood and asphalt shingles and standing-seam metal roofs, will be considered on a case-by-case basis.
- 4) Applicants are required to submit a sample material board.

V.2.D. Colors

Intent: Exterior colors shall be aesthetically pleasing and compatible with colors of nearby structures.

- 1) Intense, bright, black, or fluorescent colors shall not be used as the predominate color on any wall or roof of any primary or accessory structure.
- 2) Applicants are required to submit a sample color palette board.

V.2.E. Roofing

Intent: Roof forms shall contribute to the overall image of high quality and permanence, and shall be used to screen rooftop equipment where possible.

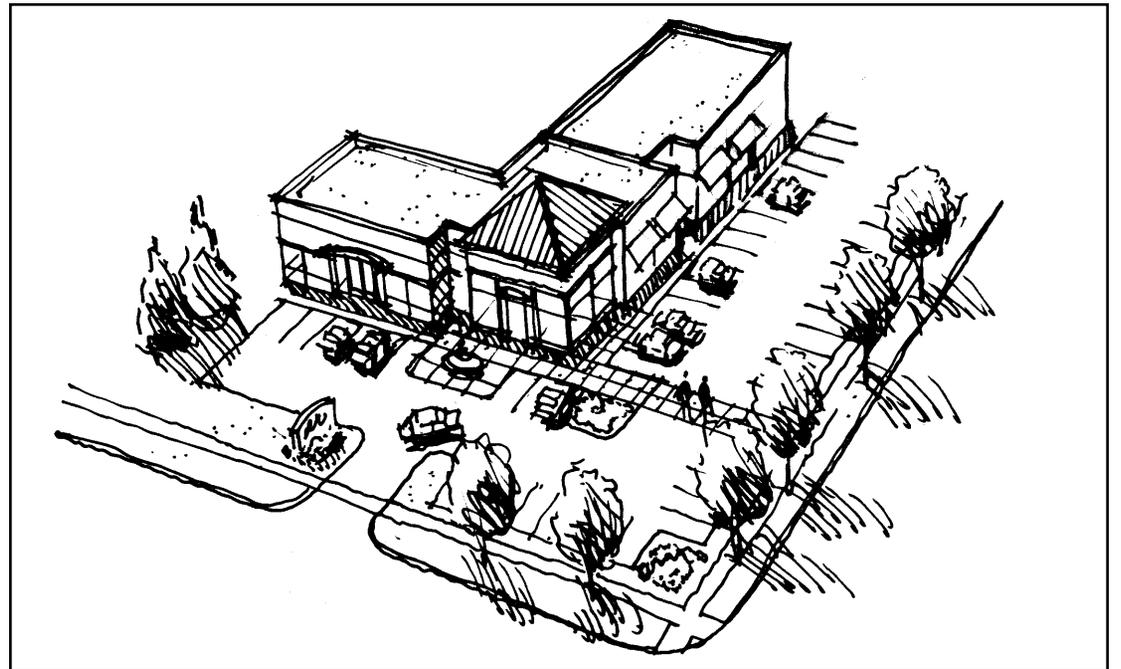
- 1) A gable or hip configuration should be used with complimentary sheds, dormers, and other minor elements. Other roof forms will be considered on a case-by-case basis.
- 2) Roof forms shall be designed to correspond and denote building elements and functions such as entrances and arcades.

V.2.F. Accessory Structures

Intent: Accessory structures shall be integrated and compatible with the primary buildings they serve.

- 1) Accessory structures, including but not limited to grouped mailboxes, storage, and maintenance facilities, recreational facilities, picnic shelters, and gazebos shall be architecturally compatible with the buildings they serve.

VI. NEIGHBORHOOD CENTERS



VI. NEIGHBORHOOD CENTERS

A Neighborhood Center is a concentration of commercial and service uses that serve adjacent neighborhoods. Neighborhood Centers are located at key intersections throughout Town and are conveniently located to serve pedestrians and bicyclists, as shown in Figure 6.1. Therefore, the Neighborhood Center standards have more stringent pedestrian requirements, with more street-oriented building provisions than the auto-oriented Community and Regional Centers. These sites are not appropriate for businesses serving regional users. Neighborhood Centers are located at the following intersections:

Mainstreet at Jordan Road
Mainstreet at Dransfeldt
Lincoln Avenue at Jordan Road
Hess Road at Parker Road
Hess Road at Jordan Road

Goals and Objectives

The primary goals for these areas are to:

- Encourage the concentration of appropriately pedestrian-scaled developments that conveniently serve shopping and service needs for adjacent neighborhoods.
- Discourage regionally scaled developments whose traffic volumes and services will not fit well in these locales.
- Promote safe and efficient pedestrian access for surrounding neighbors to and between sites.
- Encourage a lower density and building height than is allowed in Community Centers.

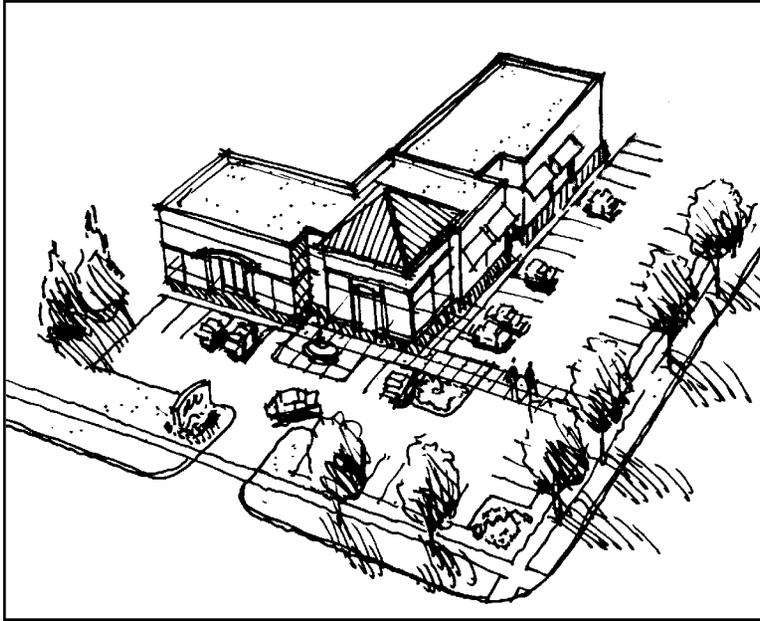
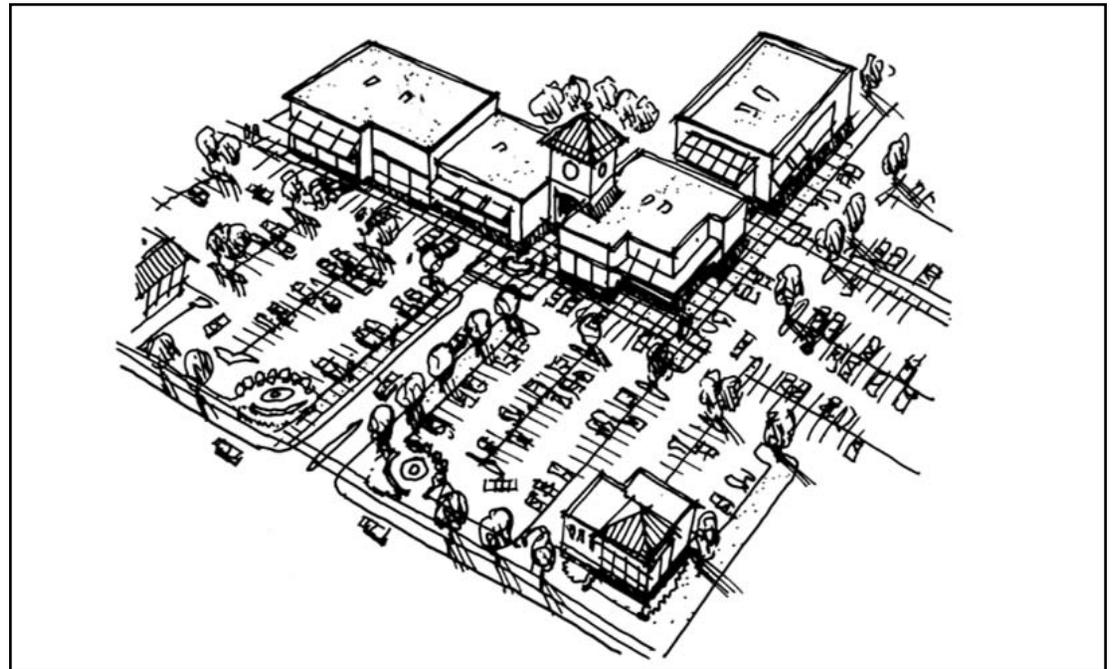


Figure 6.1: Neighborhood Center

General Provisions

- 1) Convenient pedestrian and bicycle access shall be provided to all Neighborhood Centers.
- 2) A maximum of 25 percent of parking shall be located between the primary structure and the primary access road to create a pedestrian-oriented environment.
- 3) Developments shall be encouraged to provide a patio or seating area, or a pedestrian plaza with benches and flowerbeds. All developments are required to provide trash receptacles and bicycle racks.
- 4) Significant entry features shall be provided, including but not limited to, landscaping and signage.
- 5) All internal pedestrian walkways shall be distinguished from driving surfaces through the use of durable, low maintenance surface materials such as pavers, bricks, or scored, colored concrete to enhance pedestrian safety and comfort, as well as the attractiveness of the walkways.
- 6) Each point at which the system of sidewalks crosses a parking lot or street shall be clearly marked through a change in paving materials, height, texture, or distinctive colors.
- 7) Each site shall provide at least two of the following: a water feature, clock tower, public art, natural feature or other deliberately shaped focal point.

VII. COMMUNITY CENTERS



VII. COMMUNITY CENTERS

A Community Center is a concentration of multi-neighborhood scale commercial uses that serves the needs of several neighborhoods. The intensity of uses are generally higher than those found in a Neighborhood Center. Community Centers are located at major crossroads within Town and are visual focal points which provide ties between various distinct districts within Town, as shown in Figure 7.1. Community Centers are located at the following intersections:

Mainstreet at Parker Road
Lincoln Avenue at Parker Road
Stroh Road at Parker Road
Hilltop at Parker Road

Goals and Objectives

The primary goals for these areas are to:

- Emphasize important focal points between neighborhoods.
- Encourage the concentration of appropriately scaled developments that serve multiple neighborhoods while avoiding strip development patterns.
- Encourage the development of businesses that provide a variety of products and services that will benefit the local community and neighborhoods.
- Promote safe and efficient pedestrian and bicycle access for surrounding neighbors to and between sites.
- Encourage a lower density and building height than is allowed in Regional Centers, but a greater density and building height than is allowed in Neighborhood Centers.

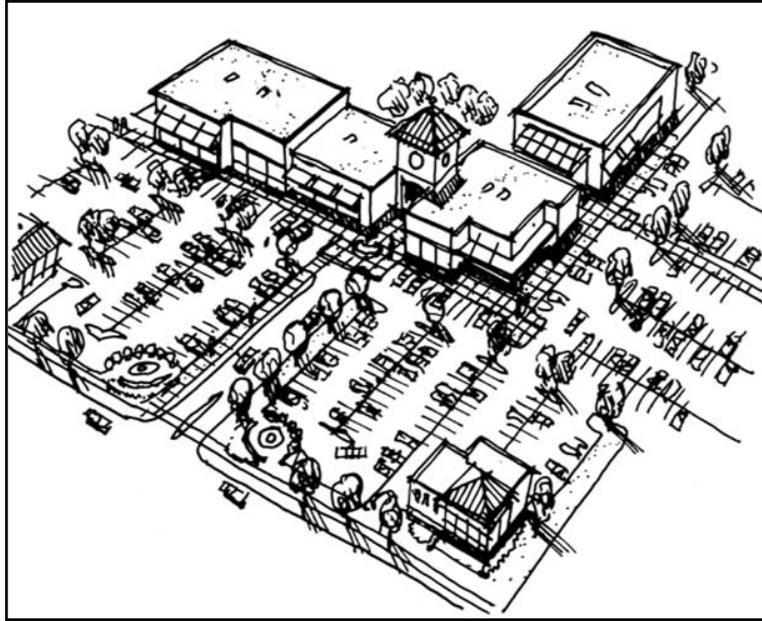


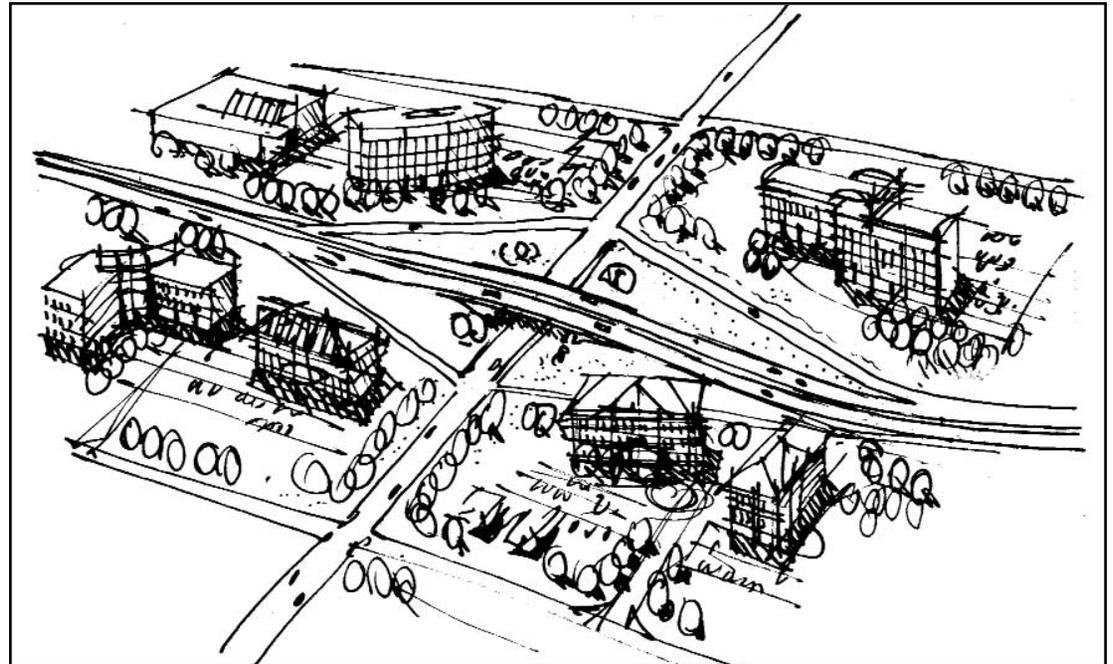
Figure 7.1: Community Center

General Provisions

- 1) Building heights shall relate to preserving views of the mountains to the west.
- 2) Significant entry features shall be provided, including but not limited to, landscaping and signage.
- 3) Distinctive landscaped areas shall be provided at project entries and at intersections of public streets adjacent to the project. Landscaping in these areas shall consist of plant specimens having a high degree of visual interest during all seasons. A mixture of shrubs, ornamental trees, flowers, and/or ground cover around sign bases, and at curb returns near site entrances shall be planted.
- 4) Each site shall provide at least two of the following: a water feature, clock tower, public art, natural feature or other deliberately shaped focal point.

VIII. REGIONAL CENTERS

VIII. REGIONAL CENTERS



VIII. REGIONAL CENTERS

A Regional Center is a concentration of commercial uses that serve the wider regional community and attracts regional interest to Parker. Regional Centers are located at key entryways into the Town and are visual focal points which provide ties between the Town and the surrounding region, as shown in Figure 8.1. Regional Centers are located at the following intersections:

E-470 at Parker Road

E-470 at Jordan Road

Goals and Objectives

The primary goals for these areas are to:

Encourage development of businesses that provide a variety of products and services that will benefit the local community and surrounding region.

Create a visible skyline to differentiate Regional Centers as more significant activity areas than surrounding areas.

Preserve views to and from Regional Centers and major thoroughfares.

Promote safe and efficient vehicular access within and between sites.

Encourage greater density and building height than is allowed in Community Centers.

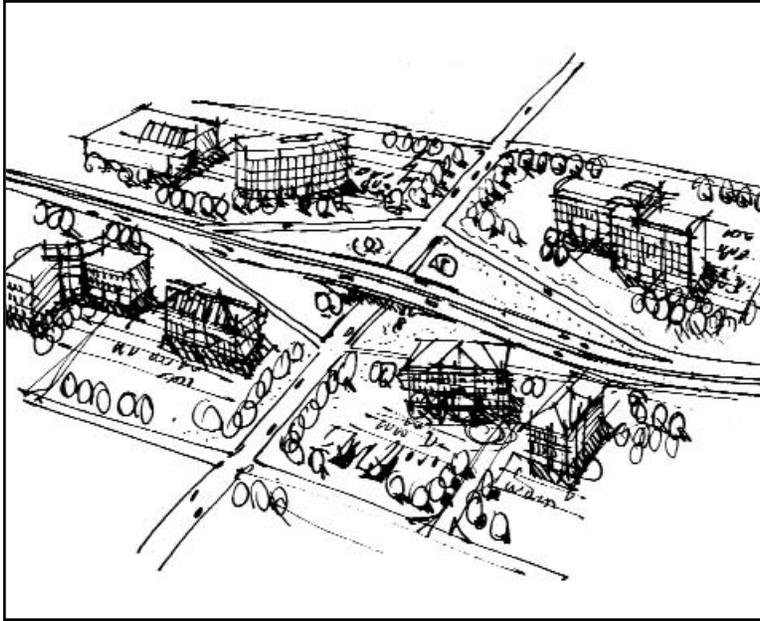


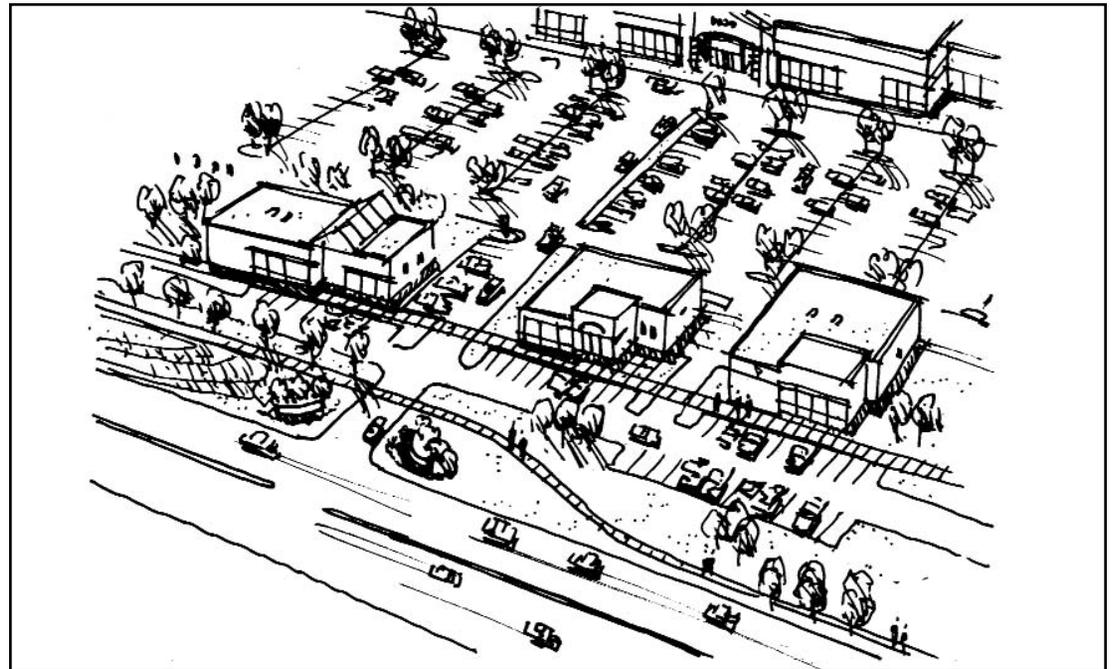
Figure 8.1: Regional Center

General Provisions

- 1) Building height or density on the fringe of the Center shall step down in order to create transitions to surrounding areas.
- 2) Buildings taller than four stories located within 100 feet of the E-470 multi-use easement and visible from E-470 shall have their tallest portion oriented so that the long axis is perpendicular to E-470.
- 3) A higher intensity of landscaping shall be provided than is required for other Centers.
- 4) Pedestrian and bicycle access to adjacent public right-of-way shall be provided to connect pedestrians and cyclists to the regional trail corridor within the E-470 multi-use easement.
- 5) Significant entry features shall be included. Examples include but are not limited to, landscaping and signage, fountains, decorative walls, public art, etc.
- 6) Set-aside areas for public use or public amenities shall be provided to create a central focal point.
- 7) Each site shall provide at least two of the following: a water feature, clock tower, public art, natural feature or other deliberately shaped focal point.

IX. PARKER ROAD CORRIDOR

IX. PARKER ROAD CORRIDOR



IX. PARKER ROAD CORRIDOR

The Parker Road Corridor is a stretch of commercial uses that serves the Town of Parker and surrounding unincorporated residential areas. The planning and design of developments along Parker Road dictates much of the character along this major thoroughfare. In an effort to reduce corporate designs that are indifferent to local identity and interests, these standards shall encourage development that contributes to the Town of Parker's unique sense of place by reflecting its physical character and adding to the visual interest of this streetscape as shown in Figure 9.1.

Parker Road is a State Highway that connects Downtown Denver to Colorado Springs. This primary corridor is the major organizing spine of the Town. An essential step in improving Parker's image is integrating quality development into the Town's primary roadway. The area between E-470 and Mainstreet may have significant redevelopment potential, while the stretch between Mainstreet and Stroh Road is currently less developed and has significant potential for new development.

Goals and Objectives

The primary goals for this area are to:

- Encourage development that contributes to a cohesive and visually interesting streetscape.

- Encourage development that contributes to continuity along Parker Road and presents an image of high quality and value.

- Promote efficient vehicular, pedestrian and bicycle access that links all street edges, parking systems and developments to each other.

- Encourage a lower density and building height than is allowed in both Regional Centers and Community Centers.

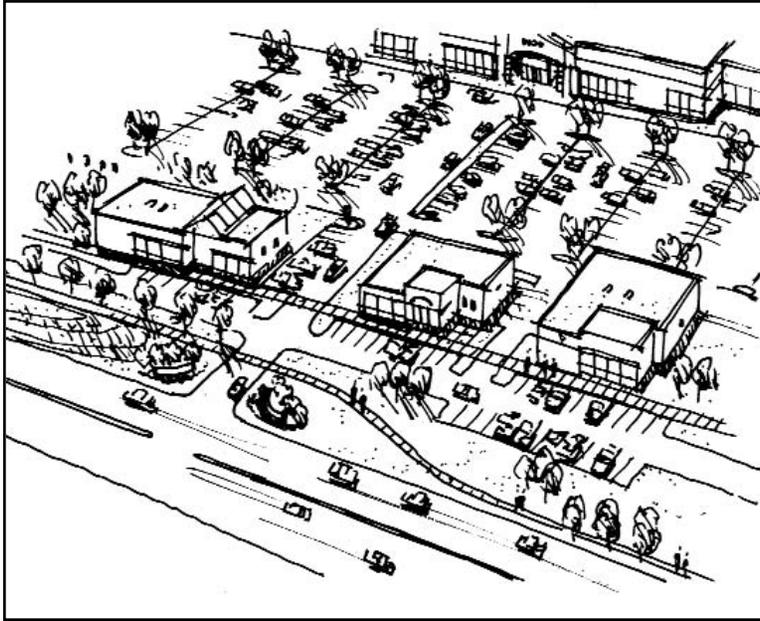


Figure 9.1: Parker Road Corridor

General Provisions

- 1) Development shall utilize an overall site organization that functionally relates to the street and development on adjacent property.
- 2) Vehicular and pedestrian access between adjacent properties shall be provided as practical.
- 3) Site plan submittals shall show proposed access connections to adjacent developed properties and future provision for access to undeveloped properties.
- 4) Landscaping which creates a unified streetscape along Parker Road shall be provided.
- 5) Detached sidewalks shall be provided and shall meet Parker Road at all intersections.

APPENDIX A: BACKGROUND AND CONTEXT ANALYSIS

A. Parker in the Region:

In recent years, the Town of Parker has experienced rapid growth fueled by the expanding Denver economy and the nationwide trend of urban flight. Expanding residential development has stirred retail and commercial development, placing heavy demands on roadways and altering traditional land use patterns. Residents are concerned that contemporary suburban, auto-oriented, "big box" retail development has the potential to engulf Parker Road and its surrounding environs, negatively influencing a visitor's impression of the Town.

A major challenge facing the Town of Parker today is maintaining the small town feel which makes Parker a unique community within the Denver region, while at the same time ensuring that growth in Parker provides a positive contribution to this rapidly growing region. This balance promotes development which recognizes such special community features as the Cherry Creek corridor, the Black Forest, rolling land forms and the pastoral history of this community. It also recognizes the development potential of Parker as a community located at the cusp of the rural Douglas and Elbert Counties and the urban Denver-metro area. Design standards balance these goals by encouraging development that contributes to the Town of Parker as a unique place by both reflecting its physical character and its role in the Denver region.

B. Natural features:

The Town of Parker's natural features, as depicted in Figure 1, are major defining elements of this community. A key natural amenity is Cherry Creek corridor and its tributaries, which run through Town and provide a system of bicycle and pedestrian trails, parks and water resources for active and passive recreational enjoyment.

The Black Forest and related terrain along the eastern edge of Parker Road forms another major component of Parker's natural character. While much of the Black Forest is outside the Town limits, a stand of trees on elevated ground nears Parker Road to the south side of Hilltop and creates a highly visible natural feature.

While not physically within the Town, the panoramic view of the Front Range west of town is another valuable natural asset. Limiting intrusion into these mountain views is a key consideration for designing projects that are sensitive to view preservation.

C. Transportation:

The Town of Parker's transportation network forms a hierarchy of roads of which Parker Road is the most significant. Because Parker Road serves as the major spine through town and interchanges with E-470, this corridor is expected to serve much of the projected traffic through Town. Jordan Road has and will continue to gain importance in the future as traffic volumes to and from E-470 increase along this corridor. Providing a link between the Town and I-25, Lincoln Road is currently the second most significant component of Parker's transportation network.

The third tier of transportation connections includes Main Street and Hilltop Road. While these corridors also provide access points into Town, they serve

more local rather than regional traffic. Dransfeldt and 20-Mile Road, and the remaining internal collector streets vary in their level of significance. What is clear from the diagram in Figure 2 is that road extensions and the formation of higher levels of connections in non-residential areas is crucial to the safe and efficient movement of vehicles and pedestrians through the commercial and industrial areas of Town.

D. Land Use:

Land use patterns within the Town of Parker illustrate a clear trend toward service and retail commercial uses along Parker Road and light industrial uses adjacent to Dransfeldt and 20-Mile Roads. Much of the surrounding land to the east and west is characterized by residential development. Figure 3 is an illustrative overview of major land use categories within the Town of Parker. Land uses are conceptually identified as residential, commercial, or light industrial. The diagram reveals a clear pattern of commercial concentration along Parker Road from roughly Hilltop Road north, with the most intense development occurring between Main Street and Lincoln Road.

E. Potentials for Development:

In order to address potential conflicts or opportunities for development in the Town of Parker, an assessment of the quantity and location of undeveloped commercial land remains is necessary. Figure 4 identifies those areas which are not developed or "fully utilized," as compared to surrounding land uses. Three categories of potential sites are identified: (1) vacant sites within the Town of Parker, (2) sites which are expected to redevelop or transition within the Town of Parker, and (3) sites which are either vacant or in transition located outside or adjacent to the Town limits. Some, or all, of the sites in this last group may be annexed into the Town at some point in the future.

APPENDIX B: SIGNIFICANCE OF CENTERS AND CORRIDORS

The following is a description of the significance of designated neighborhood, community, and regional centers and Parker Road corridor, as well as existing and potential land uses. This information was prepared as a baseline analysis used to develop standards applicable to each classification.

NEIGHBORHOOD CENTERS

A. Mainstreet at Jordan Road / Mainstreet at Dransfeldt: These primary intersections along Mainstreet, west of Parker Road, have the potential to extend the general commercial and high density residential core of the traditional Downtown uses that exist on the east side of Parker Road. The opportunity for pedestrian-oriented development mirroring the scale, density and character of Downtown is one that should not be missed. Parker residents often have expressed their preference for Downtown's pedestrian character over the auto-oriented development of Parker Road.

B. Lincoln Avenue at Jordan Road: This area is a potential center as the northwestern point of entry into Parker. It is currently being commercially developed and surrounded by single family detached residential developments. Because both Jordan and Lincoln interchange with regional expressways, (E-470 and I-25 respectively), this intersection's significance as a center and entryway is enhanced.

C. Hess Road at Parker Road: Surrounded by a number of residential developments, the intersection of Hess at Parker has the potential to provide pedestrian-scale services and uses to a number of neighborhoods.

D. Hess Road at Jordan Road: With the planned extension of Hess to Jordan, this intersection is a potential neighborhood center. This center will ultimately serve several neighborhoods throughout southwest Parker.

COMMUNITY CENTERS

A. Mainstreet at Parker Road: The Mainstreet area east of Parker Road is seen by Parker residents as the heart of the Town. Recently, design standards have been adopted to protect the distinct traditional character areas within the Downtown. This center is a point of entry into the Mainstreet district and a heavily developed commercial node. At the time of the creation of this document, this intersection does little to announce the importance of this area for motorists on Parker Road. Improved signage, landscaping, monumentation, crosswalks, etc. could be added to give this intersection the emphasis it deserves. And, while this area is most likely at full buildout, the potential exists for redevelopment at some time in the future.

B. Lincoln Avenue at Parker Road: This area is a potential center as the northeastern point of entry into Parker. Because both Parker and Lincoln interchange with regional expressways, (E-470 and I-25 respectively), its significance as a center and entryway is enhanced.

C. Stroh Road at Parker Road: This center is the southern point of entry into Parker and is currently partially developed with one corner inside the Parker town boundary.

D. Hilltop Road at Parker Road: Located just south of the Downtown area, Hilltop Road is currently being extended to the west side of Parker Road to make way for a substantial new retail development, Parker Pavilions, which is anchored by a super-size Walmart. Design of this development occurred prior to the completion and adoption of these design standards.

REGIONAL CENTERS

A. E-470 at Parker Road: This center will serve as a regional node of commercial development and may contain the largest concentration of commercial uses in Parker. Ultimately, the potential exists for large-scale development on all sides of E-470.

B. E-470 at Jordan Road: This center will also provide a regional point of entry into Parker. This area shares all of E-470's potential for regional scaled development, with possibilities including highway oriented commercial or light industrial development adjacent to the interchange.

PARKER ROAD CORRIDOR

Parker Road is a State Highway that connects Downtown Denver to Colorado Springs. This primary corridor is the major organizing spine of the Town. An essential step in improving Parker's image is integrating quality development into the Town's primary roadway. The area between E-470 and Mainstreet may have significant redevelopment potential, while the stretch between Mainstreet and Stroh Road is currently less developed and has significant potential for new development.