

Parker Mainstreet Master Plan

Continuing to Create Great Places and Spaces

September 2015



activity adjacent adult affordable already architecture around art bar better
bike build buses businesses car care chains change character community
congestion connections continuity create daily dancing development
different district dog downtown due east end events existing
extend goods gulch hometown hours housing improvements interested
intersection kids lighting live local location
mainstreet market moves music night open options
outdoor pace park parker particularly patios peak pikes
pine places plaza programming public really related residents
restaurants retail seating service shops sidewalk skating small
smaller space special street success system themed times town
traffic transit turn vehicle venue victorian walk wide winter work



Dear Citizens, Businesses and Visitors:

As the Mayor of the Town of Parker, on behalf of the Town Council, and in the spirit of cooperation with Parker citizens, stakeholders and partners, we are proud to introduce the new **Mainstreet Master Plan – *Continuing to Create Great Places and Spaces!*** This vision and action plan is the exciting result of a community desire and collaborative effort to continue promoting a successful, economically sustainable and vibrant downtown destination where citizens, businesses and visitors come together to live, work, shop and play.

As the heart of our community, Parker citizens value Mainstreet and the downtown area for its economic, cultural and social activities. However, great places don't just happen by chance or overnight. It takes a continual commitment to thoughtful and progressive planning to achieve the visions and goals that will affect positive change to downtown. The Master Plan has identified opportunities and challenges regarding land use, urban form and circulation from Twenty Mile Road on the west to Pine Drive at Town Hall on the east. For example, a top priority identified in the Plan is to increase daytime population and activity in order to support economic revitalization of the Plan Area. The Plan's recommendations reinforce the Town's strong commitment to strengthen and grow the Mainstreet Corridor. Businesses, residents and visitors will enjoy the results of the Plan's promotion of an enhanced downtown experience.

As a policy document, the Plan will help guide future development and revitalization along Mainstreet and the immediate surrounding areas - building upon the years of previous planning efforts and Town investment in the downtown area. Economic revitalization requires a common vision, goals, partnerships, education and participation among all who have a vital interest in the downtown area. The Town encourages community leaders, property owners, businesses, residents and future development partners to actively utilize the Plan and be champions for implementing the vision and change!

On behalf of the Town, elected officials and staff, we wish to thank everyone for their participation and support in the development of this Plan. Your input has been invaluable and has helped guide the process and outcomes. Town Council and I look forward to working with you and our partners towards a revitalized Mainstreet that is an economically, culturally and socially sustainable and the vibrant heart of our community.

Sincerely,

Mike Waid
Mayor

Acknowledgments

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Public Hearings

Planning Commission, September 10, 2015
Town Council, September 21, 2015

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Executive Summary

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Project Background

Purpose

Mainstreet is the heart and soul of the Parker community, reflecting its character, culture and heritage. The Town desires to strengthen its identity and grow downtown into an economically vibrant, dense and exciting area recognized as a destination and place of opportunity.

The Mainstreet Master Plan is a policy document intended to provide long-term, 20-year, guidance for all land use, urban form, circulation and parking decisions within the Mainstreet plan area (also referred to as the Mainstreet corridor) and the immediate surrounding areas of influence. The plan will help the reader understand existing conditions and visualize future development, overall intensity, urban design - how it may look and feel - and the community desire for a strong pedestrian environment throughout the corridor. It also addresses both physical and policy issues, as well as implementation strategies essential for advancing this vision to effect positive change and revitalization of the plan area.

As a vision plan that builds upon previous planning efforts and a continually evolving downtown corridor, it provides a foundation to which citizens and stakeholders can all be champions for revitalization and the ultimate future of the Mainstreet corridor. The goals, principles and recommendations described in the Plan are intended to guide future sustainable land use development and redevelopment. This action plan was informed by Parker citizens, visitors, businesses and property owners as stakeholders and will require their cooperation and engagement to work as partners with Town government towards implementation. The ultimate community benefit will be increased downtown activity, vibrancy and growing the Plan area's businesses resulting in a regionally recognizable downtown destination and experience! Finally, the Mainstreet Master Plan will be adopted as an element of the Town's comprehensive master plan, Parker 2035 – Changes and Choices.



Figure 1 Master Plan Study Area

The Mainstreet Master Plan study area extends from 20 Mile Road on the west to Pine Drive on the east. The plan area is bisected by Parker Road, which splits the area into two distinct areas, the West End and Old Town.

Study Area

The Mainstreet Master Plan focuses on an approximately 1.15-mile segment of Mainstreet from Twenty Mile Road to Pine Drive. This key and centrally located plan area is bisected by Parker Road, a state-level highway owned and maintained by the Colorado Department of Transportation (CDOT), which divides the study area into two almost-equal pieces.

The western portion of the study area has been dubbed 'the West End' in this plan and extends from Twenty Mile Road to Parker Road. The eastern,

better-known portion of the plan area is generally referred to 'Old Town' in this plan. The northern and southern boundaries of the study area were set by the Town to generally include areas of development opportunity that do or could impact the Plan area's development, character and feel.

West End and Old Town are names for the purposes of this plan only. Town Council, later in this plan development, chose to simplify the plan area name to 'downtown' as the result of a concurrent planning process and analysis for a wayfinding signage program.

Existing Conditions

Land Use

The West End is dominated by low-rise, single-story retail and office uses, where typical tenants include banks, professional offices and franchise casual dining. Other West End uses include multi-family residential (under construction), senior assisted living, two small but underutilized parks and a community shopping center that includes a grocery store and big box spaces.

Old Town has a different mix of uses than the West End, with smaller-scale retail and office/commercial uses mixed with a variety of civic, cultural and institutional uses. Also, Old Town's retail market is under served by the existing tenant mix. O'Brien Park, the Town's flagship park and home to a long list of festivals and community events, is also within this portion of the study area. Other uses within Old Town include an older strip retail center (oriented 'sideways' to Mainstreet with minimal street frontage), the Parker Senior Center, a child day care, assisted living and the RTD Park n Ride and bus facility. Cultural and civic uses include the newly renovated Mainstreet Center, the Parker Arts Culture & Events (PACE) Center, Town Hall and a new Douglas County Library facility, which is currently under construction.

Urban Form

Within the West End, the majority of Mainstreet buildings are one-story structures built in a simple strip-type format. Buildings tend to cluster in groups on two or three sides of an internal parking lot and do not face Mainstreet. Building materials include a significant amount of masonry, brick and faux-stone veneer, providing some link to historic Old Town materials, but overall architectural form is generic and undistinguished.

Within Old Town, buildings display a different, generally smaller scale than that found west of Parker Road. Most buildings along this segment are two or three story with pitched roofs or articulated parapets, creating a higher level of pedestrian interest. The central portion of Old Town strives to create an historically-referenced western storefront architecture. Materials tend toward brick, masonry and clapboard siding.

Circulation

Pedestrians

As previously mentioned, the community desires an accessible and safe pedestrian environment for all users and abilities. Virtually all the public roadways within the study area have sidewalks, with only a few gaps in the system. However, sidewalks vary in width and cross-section with attached walks directly abutting the curb being prevalent throughout the study area.

The area's most challenging pedestrian condition is the intersection of Parker Road and Mainstreet. Pedestrians must cross 8 lanes of traffic and approximately 135' of arterial roadway, although two grade-separated alternatives to this crossing are available: Kieffer's Crossing underpass (approximately 375' north of the intersection) and the Sulphur Gulch Trail underpass (approximately a quarter-mile south of the intersection).



Bicycles

The Sulphur Gulch Trail is the study area's most prominent bicycle facility. This off-street, multi-use trail parallels Mainstreet through an open space corridor approximately a quarter-mile south of Mainstreet. The trail also connects users with other off-street trail links.

The study area features only one on-street bike lane, on Pine Drive, and throughout the plan area cyclists must blend with traffic. Limited bicycle racks are located sporadically within the right-of-way and occasionally on private property.

Transit

The study area is served by three Regional Transportation District (RTD) bus routes, Route 410, Route P and Route 153. Schedules are calibrated to serve weekday commuters with no weekend service on any of the routes. RTD also provides a Call-n-Ride service in portions of Town including the Plan area. The Call-n-Ride is a curb to curb shuttle service that must be booked a minimum of 2 hours in advance. A 2005 study recommended future bus

rapid transit (BRT) service to connect the Plan area with the Southeast Light Rail Transit (LRT) extension; this bus would follow Mainstreet west of Parker Road. No timeframe or funding for BRT implementation was identified, but the Town wishes to preserve necessary right-of-way for this future potential.

Parking

The Mainstreet corridor offers approximately 264 free, on-street parking spaces within Old Town; these spaces have no time limits. On-street parking is not provided in the West End or its intersecting roadways west of Parker Road.

In addition to on-street parking, the Old Town area currently offers approximately over 1050 public parking spaces on-street and in lots ranging from 18 to 182 spaces in size - available at no charge and with no time limits. The largest parking reservoirs are at O'Brien Park, the Mainstreet Center, the PACE Center and Town Hall. Smaller lots exist off Pilgrims Place adjacent to St. Matthew's Church and near old Town Hall. Not included in this count

are the 173 spaces at the RTD Park-n-Ride which are often treated as public parking during events.

There are also two large private, off-street parking areas in Old Town. The approximately 265-space lot behind Parker Station was thought by many residents and visitors to be a public access lot but is located on private property, privately maintained and was gated off in 2015 to limit use to tenants, guests and other permitted users.

The second large private parking area is located internal to the strip mall behind Baskin Robbins ice cream shop and has approximately 230 spaces. Smaller amounts of private parking, also often treated as 'public' but technically reserved for tenants of the adjacent offices and businesses, are located behind the buildings that front Mainstreet in Old Town as well as north of Pilgrims Place.

All off-street parking in the West End is private and associated with adjacent buildings.



Recommendations

Land Use

The study area is challenged by three primary land-use related conditions and the Plan's recommendations focus on addressing these issues to support and grow a vibrant and sustainable downtown:

- **Appropriate Use:** An absence of active pedestrian-oriented ground-floor uses
- **Gaps:** A large number of physical gaps between existing buildings
- **Intensity:** A lack of development intensity and critical mass

The Plan's most prominent recommendation is to change Town policy—through zoning, an overlay district or other appropriate mechanism—to encourage active pedestrian-oriented ground floor uses for buildings and parcels adjacent to Mainstreet while directing office and residential uses to upper floors. Examples of appropriate first floor uses include restaurants, boutiques, coffee shops, art galleries, personal services and other similar uses. Directing office and residential uses to upper floors allows further economic development opportunities that will attract more workers, residents and visitors - enhancing the Plan area's appeal as a sought after business location and destination. The Plan promotes long-term transitioning of vacant parcels and non-pedestrian friendly uses to higher intensity, downtown-type development. Uses which bring more daytime use—such as employment nodes or higher education—are specifically encouraged.

Finally, in addition to promoting the appropriate uses, the Plan also recommends a greater number of uses. Throughout the Plan area, new development must be a minimum of two stories, with up to five stories permitted in most areas. The goal of these last two recommendations, higher intensity development and multi story buildings, are to create more activity and a more vibrant street life, which promote and support a larger customer base for existing and future businesses.

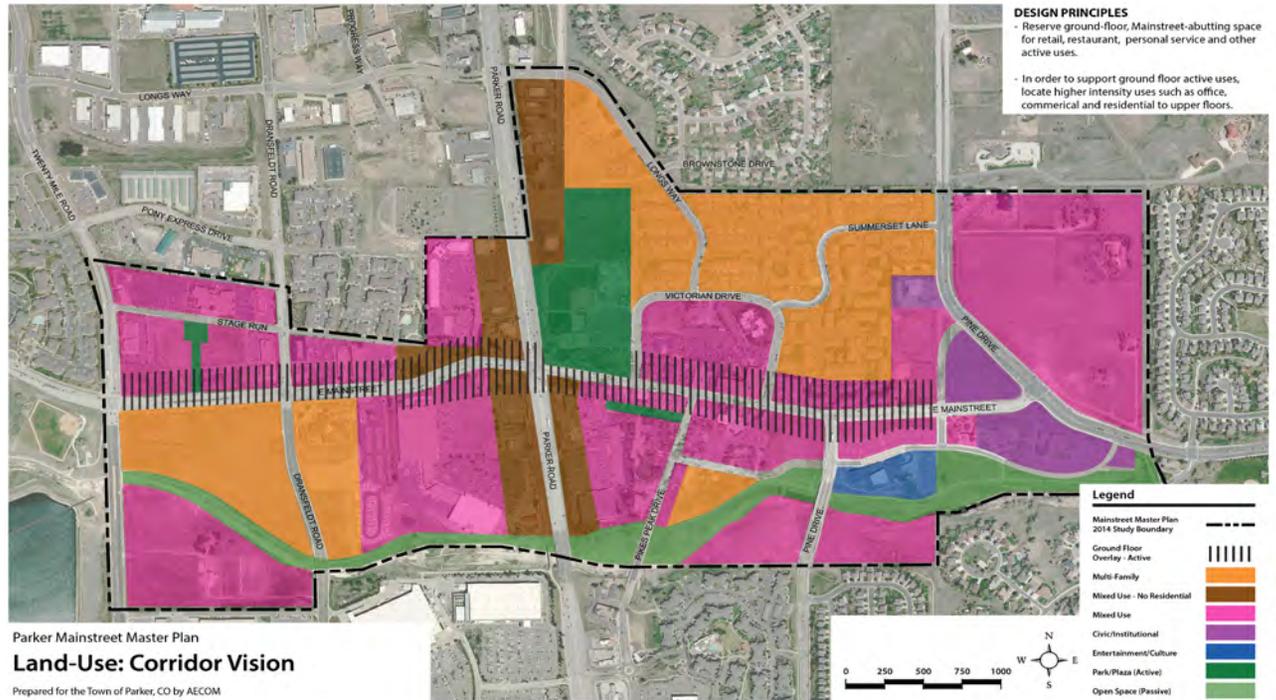


Figure 2 Land-Use: Corridor Vision

Future development and tenancing should focus on active, ground-floor uses that promote an interesting, pedestrian-focused downtown. Office and residential uses should be directed to upper floors (on Mainstreet) and side streets.

Top 3 Land Use Recommendations

ACTIVE USES: Promote active ground-floor active uses such as retail, restaurants, galleries and personal services with interactive storefronts for Mainstreet abutting buildings and parcels.

MORE HOUSING: Encourage increased residential housing throughout the plan area, with a focus on higher-density multi-family development as a part of vertical mixed use development.

MORE DAYTIME ACTIVITY: Actively pursue employers to create a higher intensity of office jobs within the Plan area and higher education uses that support a daytime economy.

Urban Form

Creation of unique character feel and appropriately scaled development, buildings and public spaces in the West End and preservation of the unique character of Old Town are the central themes of the Plan's urban form recommendations. Architectural style and material recommendations focus on creating a high-quality, diverse human scaled environment that is interesting to residents and visitors alike, but not limited to a particular style or period. Public art and streetscape (street trees, lighting, furnishings) are both emphasized as 'common threads' to tie together the downtown's two distinct districts, the West End and Old Town.

The Plan also outlines urban form guidance for priority redevelopment parcels, such as the Crossroads Shopping Centre area and PACE Center block. These design recommendations focus on the Plan's urban form design principles and desired outcomes, recognizing that there are many possible development approaches to achieve the desired character goals.

Note: Design illustrations are conceptual only and meant to emphasize the Plan's design principles.

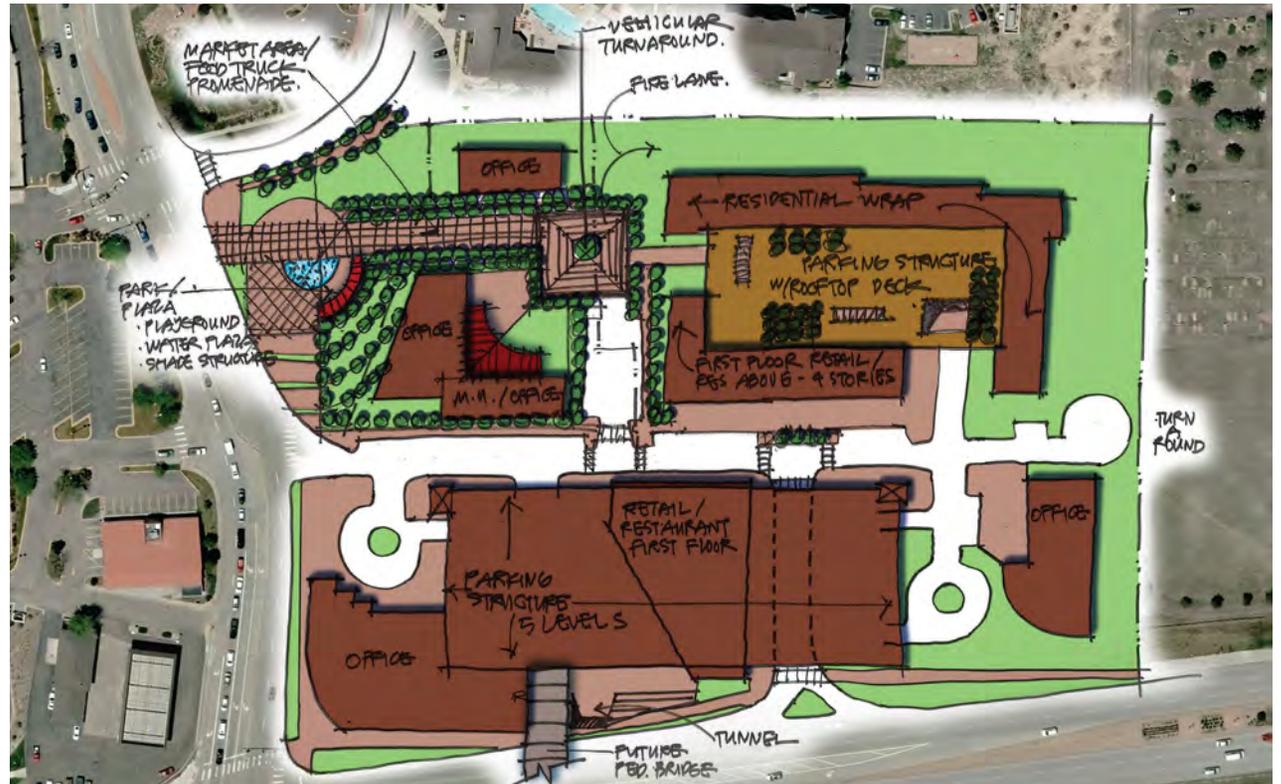


Figure 3 Potential Crossroads Mall Redevelopment

New development should pay special attention to street edges and community integration. This sketch for potential Crossroads Mall redevelopment demonstrates design principles that should be used in future development.

Top 3 Urban Form Recommendations

STREET TREES: Install a continuous line of street trees spaced 30 feet apart, as well as art, street furnishings and enhanced hardscapes along both sides of Mainstreet to create character, shade, visual interest and a healthy environment.

BUILDING HEIGHTS: To provide continuity with existing buildings and character, as well as increase housing and employment opportunities, buildings in the Plan area should be a minimum of 2 floors minimum and a maximum of four to five floors.

ARCHITECTURE: Emphasize high quality materials as well as authentic and diverse architecture that promote and enhance the downtown character instead of following a prescribed style.

Circulation and Parking

Taking a cue from the Town's 2014 *Transportation Master Plan* (TMP), the Mainstreet Master Plan emphasizes multimodal network with a focus on options. The Plan outlines a complete network of bicycle facilities, including both north-south and east-west links into both the West End and Old Town. Bicycle parking is also discussed, with specific rack and location suggestions.

Traffic Calming

For pedestrians, the plan recommends a policy shift to give greater priority for pedestrians and bicyclists, particularly at intersections. The plan recommends across-the-board pedestrian improvements, as well as further study of alternatives to crossing Parker Road on foot. While the Mainstreet corridor relies on adequate traffic volume and flow to maintain its vitality, traffic should not overwhelm the pedestrian experience. Enhanced traffic calming techniques should consider slowing the speed of vehicles to support non-vehicular modes of travel and provide the desired safe and strong pedestrian/bicycle environment. Treatments such as consistent use of vehicle stop lines at all plan area crosswalk intersections and consideration of in-street pedestrian crossing signs are recommended. Doing so will reinforce Mainstreet, through the Plan area, as more of a destination versus a through route. The plan also proposes enhancements to the Kieffer's Crossing underpass and outlines criteria for evaluating the potential of a future pedestrian bridge.

Alternate Routes

Foremost among the Plan's vehicular recommendations is the promotion and creation of parallel routes to Mainstreet. This recommendation is intended to ease vehicular congestion and, in doing so, promote a pedestrian-priority downtown environment. The Plan recommends strategies such as time limits and peak-time valet parking to manage and maximize existing parking supply, with an ultimate goal of constructing a public or shared public/private parking structure when needed to support the ongoing growth and economic sustainability of downtown Parker.

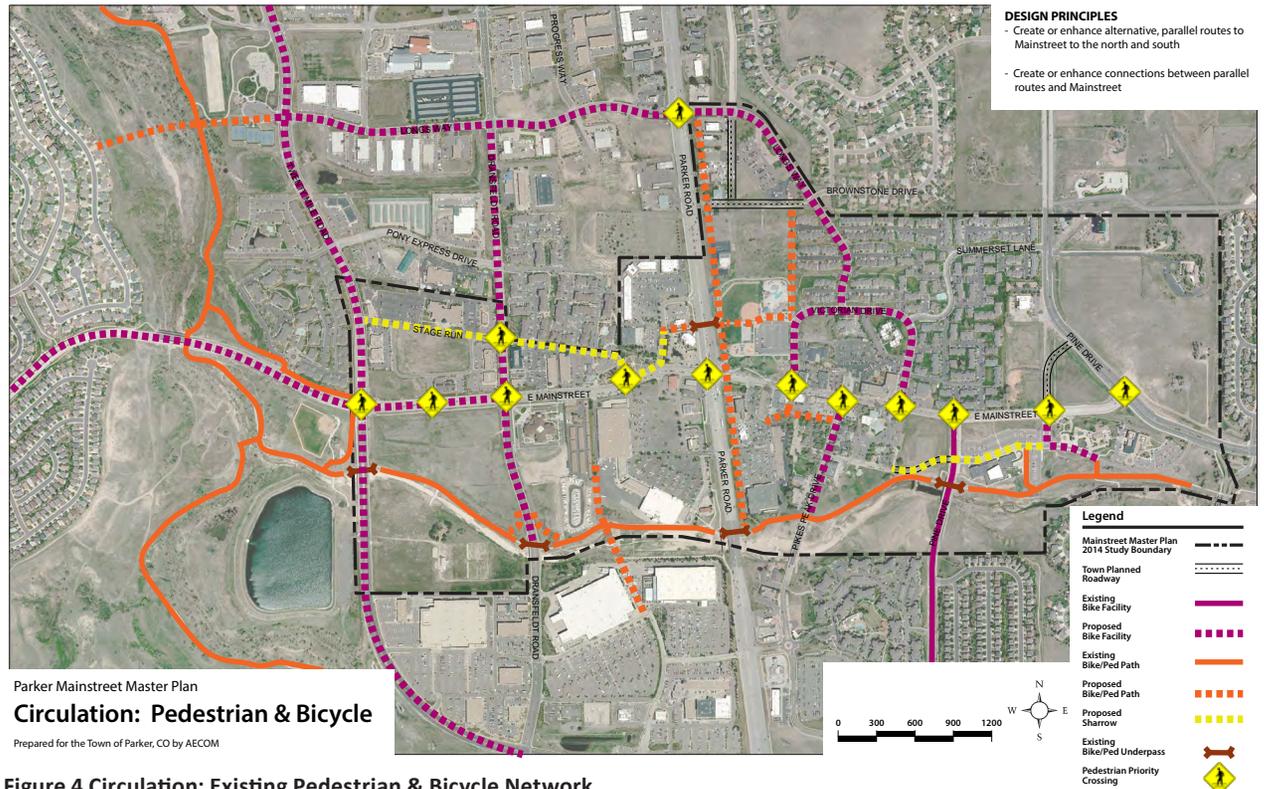


Figure 4 Circulation: Existing Pedestrian & Bicycle Network

A new bicycle network, including north-south and east-west facilities, promotes multi-modal travel to and within the downtown.

Top 3 Circulation Recommendations

ALTERNATE ROUTES: Extend existing roadways to promote alternate routes to Mainstreet creating a better connected grid system throughout the downtown.

ADD BIKE LANES AND ROUTES. Introduce bike lanes and routes providing connectivity to and within downtown.

ENHANCED PEDESTRIAN CROSSINGS: Introduce additional crosswalk enhancements to increase safety, comfort and visibility at all Mainstreet intersections within the Plan area, especially Parker Road and Mainstreet.

Parks & Public Spaces

Parker loves festivals, and most of these events are held in O'Brien Park. Stakeholder and public feedback indicate a desire for more and varied events. Event organizers expressed that existing events could expand if additional space were available.

Twenty Mile Historic Park and Living Wheel Park: These two highly underutilized parks, comprising almost two acres and 430 feet of prime Mainstreet frontage, create a significant 'dead zone' in what should be one of the West End's most vibrant areas.

Both parks need to be redesigned to focus on creating an interesting street frontage that continues the energy of Mainstreet and encourages visitors to continue along the corridor. A new park design will create interesting active and passive areas with shade that could be used for casual dining, reading, play or other activities.

Stage Run: A new park/plaza located between the movie theater and Mainstreet will be configured to create a more formalized public open space that draws residents of nearby neighborhoods to this West End amenity. The plaza will be easily accessible, visually interesting and allow for restaurant outdoor dining and other pedestrian-friendly uses at its perimeter. The space should be flexible and designed to allow for events and festivals in the West End.

Pilgrims Place: Pilgrims Place will be reimaged as a flexible, multi use space intended to accommodate events, pedestrians, cyclists and vehicles. The new space would be identified through enhanced paving materials that would extend to a new plaza in the adjacent park and the pedestrian passage to Mainstreet between the restaurants. This treatment will create a plaza like feel along the roadway.

Removable bollards would make it possible to close the street for use as a stand-alone public space or extended, special-event spillover space from O'Brien Park.

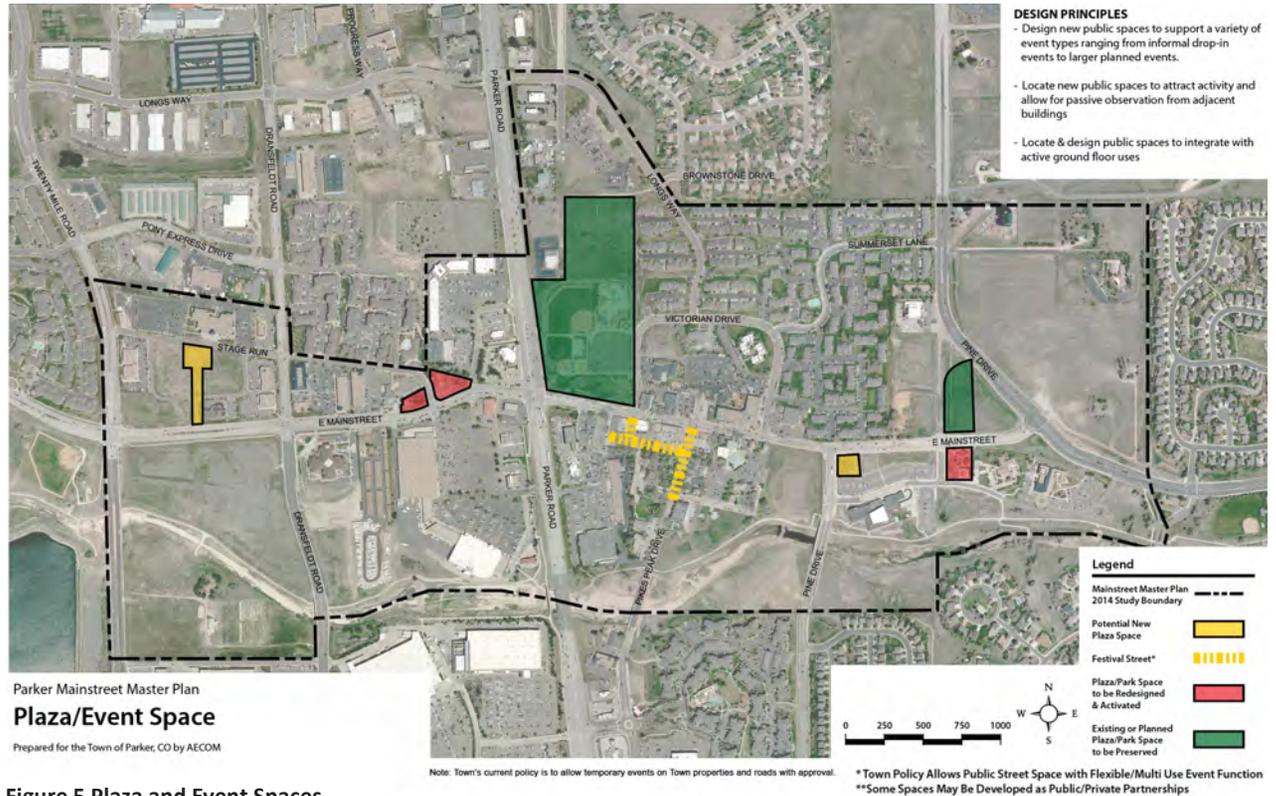


Figure 5 Plaza and Event Spaces

Top 3 Parks and Public Spaces Recommendations

20 MILE HISTORIC PARK AND LIVING WHEEL PARK: Redesign the existing 20 Mile Historic Park and Living Wheel parks to promote activity in the parks and create a pedestrian friendly street edge.

STAGE RUN PUBLIC PLAZA: Construct a new public plaza space abutting Stage Run, in tandem with development.

PILGRIMS PLACE: Redesign Pilgrims Place as a shared multimodal flexible space that can be closed and used as a special event space.

Implementation

The recommendations outlined in the Mainstreet Master Plan will be carried out over the next 20 years by a variety of public and private entities. Some recommendations are clear and straight-forward, can be implemented in a fairly short amount of time and administered by a single department or entity. Other recommendations will require cooperation and collaboration between single Town departments or a larger number of parties—as well as an extended period of time—to implement. Actions are grouped into five categories:

- 'Groundwork' (Immediately)
- Phase 1 (0-5 years)
- Phase 2 (6-10 years)
- Phase 3 (11-20 years)
- 'As Appropriate' (AA)

The Master Plan also identifies a specific lead department or responsible party to each action, to ensure a 'champion' to carry the Plan's recommendations through to execution.

The Town and the community will need to make a coordinated effort to align our annual and day to day decisions with the visions and goals of this Plan to achieve a more vibrant and economically sustainable downtown. These important decisions will be made during capital projects prioritization, budgeting, events planning, policy amendments and other projects/programs.

Successful advancement of the vision and the desired long-term outcomes of this Mainstreet Master Plan will depend on acknowledgment of and partnered efforts to overcome several potential barriers discussed in this document. Potential challenges to implementing the Plan's recommendations include:

- **Physical** - Vacant/underutilized parcels and no critical mass; inconsistent pedestrian environment
- **Market** - Lack of critical mass of rooftops and businesses to create a daytime economy as well as retail and retail diversity; this results in limited sales volumes and limited private investment
- **Regulatory** - Inconsistencies among regulatory documents that may not support this master plan vision (urban renewal plan, zoning, design standards)
- **Financial** - Limited investment and/or reinvestment in properties; the need for appropriate cost effective incentive tools to attract business and investors
- **Political (leadership & community-driven)** - Consistency among elected officials willing to advance the vision; attitudes (fears) about density and affordability, complacency or unwillingness to change; understanding of public and private sector roles and responsibilities within the Mainstreet Master Plan corridor
- **Organizational** - Lack of a formal business organization working together on management issues and facilitating district-wide improvements.





1 Introduction, Vision + Goals

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Introduction

Purpose of the Master Plan

Mainstreet is the heart and soul of the Parker community, reflecting its character, culture and heritage. As such, the Town desires to reaffirm the Mainstreet corridor's role as the economic, cultural and social center of the community. This Master Plan recognizes the issues and opportunities to growing and strengthening the Plan area on both sides of Parker Road and provides recommendations for revitalization of the downtown area by continuing to create a healthy downtown core and sustainable place of opportunity.

The Mainstreet Master Plan is intended to provide long-term, 20-year guidance for all land use, urban form, circulation and parking decisions within the Mainstreet corridor and the immediately surrounding areas of influence. The plan addresses both physical and policy issues, as well as implementation strategies, and is informed by a current market analysis of downtown Parker and the larger trade area.

This document seeks to complement, refine and advance the recommendations contained in existing Town plans and documents. In some cases, this Master Plan provides more detailed direction as to how broad, Town-wide policies and recommendations should be applied within the specific context of the Mainstreet corridor. In other cases, this document provides direction on addressing significant community changes that have taken place since existing policies were adopted.

Some maps throughout this document include recommended urban design principles that may include both form and function, and should serve as a community guide for future sustainable development in the Plan Area.

Relationship to Prior Documents

The Mainstreet Master Plan area is currently part of the Downtown Core, East Downtown Gateway and the Central Commercial Character Areas of the Town's 2035 Master Plan. The Downtown Core and East Downtown Gateway areas encourage, and are currently made up of, mixed-use development that includes higher density residential, office, government, arts and cultural venues, restaurants, retail, personal services, as well as, parks and other gathering places for community events. The Master Plan further describes future development and redevelopment in both areas as compact, walkable and embracing the urban downtown experience through land use, urban form and a multi-modal transportation network. Also stated in the Town's Master Plan, is that the importance of a healthy, vibrant economy to Parker's high quality lifestyle cannot be over emphasized. This includes the critical downtown core - the Mainstreet Master Plan study area - which is the focus of this Plan. The Central Commercial Character Area contemplates a similar mix of uses

and encourages mixed use developments, but allows for a development style that is more suburban style, automobile focused and horizontal in the mix of uses.

Documents most relevant to this planning effort include the following:

- Downtown Strategic Action Plan (2002) and Report Card (2013)
- Greater Downtown District Circulation Network Visionary Plan (2003)
- Parker Bike Lane Plan (2005)
- Parker Central Area Reinvestment Plan (2009)
- Greater Downtown District: Standards and Guidelines for Development (multiple documents, 2002-2004, revised 2010)
- Open Space, Trails and Greenway Master Plan (2010)
- Parker 2035: Changes and Choices (2012)
- Old Town Creative District Plan (2013)
- Town of Parker Transportation Master Plan (2014)



The Town of Parker supports a strong commitment to progressive planning that contributes to the community's high quality of life. The graphic above shows a selection of the Town's recent plans and implemented projects from those plans.

Figure 6 Relevant Plans and Policies

Study Area

The Mainstreet Master Plan focuses on an approximately 1.15-mile segment of Mainstreet from Twenty Mile Road on the west to Pine Drive (Mainstreet) on the east. The area is bisected by Parker Road, a state-level facility owned and maintained by the Colorado Department of Transportation (CDOT) which divides the study area into two almost-equal pieces.

The western portion of the study area has been dubbed 'the West End' in this plan and extends from Twenty Mile Road to Parker Road. This area, originally planned as a vertically mixed-use neighborhood, has in recent years developed into a more horizontal, auto-centric and stand-alone mix of higher density multi-family residential, senior housing and big-box and strip retail uses.

The better known portion of the Plan area is referred to in this Plan as Old Town. This area includes the historic crossroads from which Parker has grown, and has deep meaning and value to the community.

The northern and southern boundaries of the study area were set by the Town to generally include areas of development opportunity that could impact Old Town development, character and feel. The overall study area and its two sub-areas are shown on map on the following page. Towards the end of this planning process, the Town completed an analysis to determine how the community referred to the Plan area. This analysis was conducted as a part of another project. The results of the analysis guided Town Council direction to refer to the entire Plan area as Downtown.

Public + Stakeholder Involvement

Event Outreach

In order to reach out to the community 'where they already are', the project team set up and staffed

project information tables at a series of public events in the summer and fall of 2014. The team attended two Sunday farmers' markets, two Friday-night Wine Walk and a Movies-in-the-Park event at O'Brien Park. Community members were very talkative at these events, and were encouraged to spend as much or as little time as they wished chatting with Town staff and consultants. Input from these events was vital in establishing the Plan vision and goals; materials and comments can be found in Appendix A.

Small-Group Interviews

Town staff and the design team worked together to identify stakeholder focus groups that would be representative of specific interests within the Plan Area. The team conducted a series of eight small-group meetings, including property owners, event operators, downtown businesses, the Chamber of Commerce, residential boards, and the library and Parker Senior Center. As with other outreach materials, notes from this series of discussions are included in Appendix A.

In May 2015, meetings with major property owners and interested businesses were also conducted to review the draft key recommendations.

Technical Advisory Committee

The Technical Advisory Committee (TAC) provided an important forum for cross-discipline input on the Master Plan, and included Town staff from Public Works, Parks and Recreation, Cultural Department, Community Development, Economic Development and Communications. CDOT also participated in these meetings. The TAC met at four critical milestones over the course of the project, to review and refine draft recommendations.

Public Meeting

The planning process included one open house in spring 2015 to review draft key recommendations with the public. The meeting was held at the PACE Center, and attracted approximately 20 community members and stakeholders. Input from this event was incorporated

in the final plan. Materials and summary comments from this meeting can be found in Appendix A.

Town Council and Planning Commission

The project team provided in-person briefings to joint meetings of the Town Council and Planning. These briefings included a presentation as well as question-and-answer period.

Lots of Questions

Stakeholders had lots of questions and suggestions during the initial goal-setting portion of the project. Distilled into essential issues, this plan will provide directions on many of these issues:

Outdoor Dining

How can we redesign existing sidewalks to accommodate more outdoor dining?

More Shops

How can we retrofit existing buildings to promote first floor retail, display windows and unique local shops?

Pedestrian Focus

How do we better balance and promote transportation alternatives (bikes, pedestrians, transit and cars)?

Parking Management

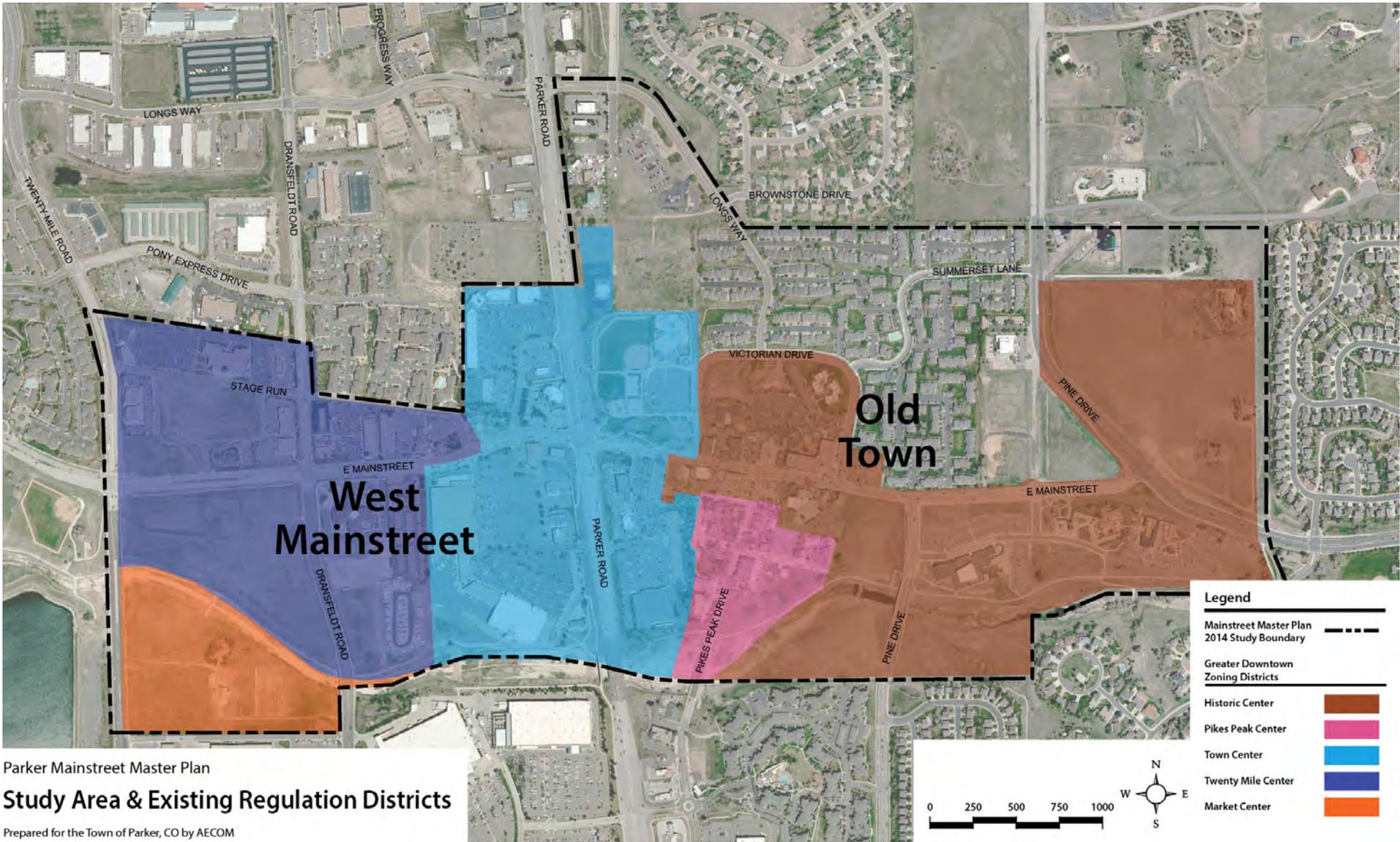
If we do drive, how do we reduce 'the hunt' for parking?

More Activities

What kind of events and venues would promote a 'full day' of activities?

Extended Downtown

How do we get visitors to 'turn the corner' down side streets?



The dashed line above shows the boundary of the Mainstreet Master Plan; colored areas illustrate the existing Greater Downtown Zoning and Design Districts that overlap with the study area.

Vision and Goals

Plan Area

Vision

The Mainstreet Plan area is the heart of Parker and the foundation of our community identity. Citizens value and describe this corridor as the cultural, economic and social center of our Town.

Centrally located, the corridor is comprised of two unique and distinct areas - West End and Old Town - that will be integrated into a cohesive and successful destination where the community comes together to live, work, shop and play.

Building on the past, Parker's future land uses, connectivity and the design of the corridor will be carefully planned to create a series of great places and spaces that provide a greater variety of activities and experiences including easy, safe and comfortable movement whether walking, biking, driving or taking transit.

Goals

Goal P-1

Attract desirable land uses and businesses that activate and further the vision of Mainstreet as the cultural, economic and social hub of our Town and transition the misplaced land uses to more compatible areas to create a vibrant, mixed use, walkable arts and entertainment center infused with community gathering spaces, specialty retail and dining options, diverse creative businesses and life-cycle housing choices.

Goal P-2

Solicit public support and participation for changing the way we think about the economic sustainability and vibrancy of the corridor.

Goal P-3

Foster partnerships with those that have a vital interest in the corridor and work together to achieve common goals of revitalization and development/redevelopment of the corridor.

Goal P-4

Identify and capitalize on existing assets such as unique character, density, buildings and human scale that give people a sense of belonging and use these as the foundations for revitalization and sustainability.

Goal P-5

Create a cohesive yet distinctive character for the West End and Old Town Parker through the development of district design features and physical improvements that creates a sense of arrival and place.

Goal P-6

Expand the role of the Mainstreet corridor into a year round all-day destination through development/redevelopment, active land uses, employment, events and activities creating a solid daytime economy.

Goal P-7

Create a 'front door' gateway to the Mainstreet Corridor at the Mainstreet and Parker Road intersection.

Goal P-8

Emphasize quality building and community

design through enhanced development standards and design guidelines.

Goal P-9

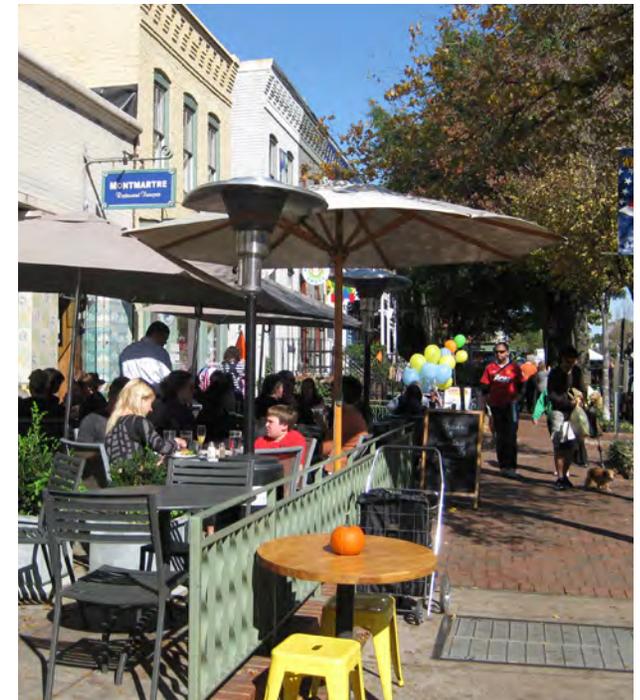
Improve roadways throughout the Mainstreet corridor by enhancing bicycle and pedestrian safety, mobility and comfortability to create a multimodal environment as recommended in the Parker Transportation Master Plan.

Goal P-10

Upgrade pedestrian crossings of all streets within the Plan area with special consideration of pedestrian crossings of Parker Road and crossings of Mainstreet.

Goal P-11

Create bicycle and pedestrian connectivity between Mainstreet and the Sulphur Gulch Trail.



West End

Vision

The West End will be a distinguishable and desirable downtown neighborhood, a shift from the Old Town district character, and will be embraced for its walkability, pedestrian scale, amenities and hometown feel through a more residential character and a carefully sized and detailed streetscape. West End's success will encompass a distinct image and feel that capitalizes on residential density and will be supported by larger retail and office space, dining, entertainment and shopping opportunities. The district's mix of uses along with its density of housing will create a competitive and convenient area adding to its vitality and daytime activity. Feature gateways, future redevelopment, parks, open space and signature gathering places at the West End will contribute to a sense of place, promote an exciting synergy and lifestyle while growing the population of residents, workers and visitors to the neighborhood.

Goals

Goal W-1

Develop the West End as the gateway into a unique downtown neighborhood through urban form and design to create a distinct and recognizable community character.

Goal W-2

Foster partnerships with stakeholders that have a vital interest in the district and work together to achieve common goals of revitalization and development/redevelopment of the district.

Goal W-3

Create a pedestrian-friendly streetscape with enhanced sidewalk, crosswalk and roadway improvements, landscaping, lighting, furnishings and public art.

Goal W-4

Create a series of different parks, plazas and gathering spaces that are available for events and public use.

Goal W-5

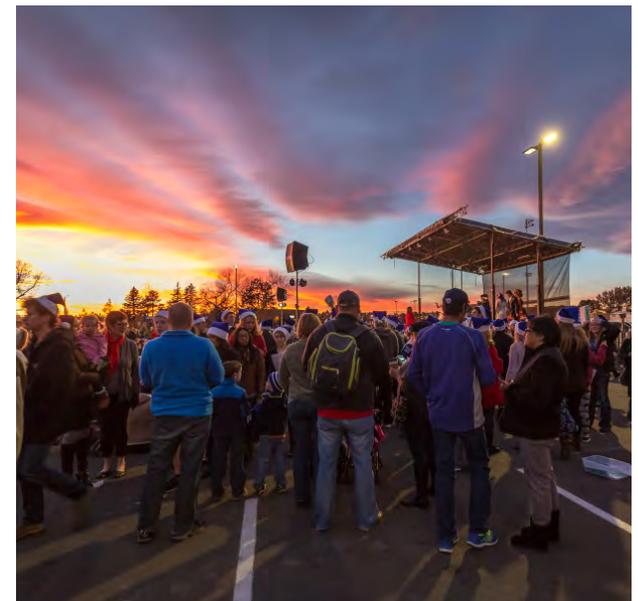
Preserve potential for future transit improvements including Bus Rapid Transit (BRT) and a community shuttle along Mainstreet.

Goal W-6

Strengthen the mix of land uses to activate Mainstreet, support the existing housing density, increase employment and increase patronage of surrounding businesses in the area.

Goal W-7

Support the West End with active first-floor and high intensity land uses to enhance the visitor's experience and strengthen the economic base of this central neighborhood.



Old Town

Vision

Old Town is the quaint, historic and walkable downtown core that captures the authentic spirit of our community. Rich with opportunities in art, entertainment, dining, business and learning, Old Town celebrates its unique character and appeal as an inviting and exciting Creative District destination with year round activities and amenities. Parker's vision is to continue shaping an exceptional and vital downtown that enriches the business community, residents and visitors alike.

Goals

Goal O-1

Embrace economic restructuring in Old Town by expanding and diversifying the mix of commercial uses and business types in order to create an improved commerce district and a multi-use destination.

Goal O-2

Support Old Town with active first-floor and high intensity land uses to enhance the visitor's experience and strengthen the economic base of the district.

Goal O-3

Preserve the district's unique historic character but complement it with distinct urban design treatments - visual elements - to create a more inviting environment for shoppers, visitors and workers.

Goal O-4

Foster collaboration and partnerships with stakeholders in the district and work together to achieve common goals of revitalization or development/redevelopment.

Goal O-5

Redefine Old Town streets as public spaces for cars, pedestrians, bicycles and events.

Goal O-6

Identify and capitalize on existing assets such as unique

buildings, pedestrian scale and the historic character of the district that give people a sense of place and use these as the foundations for revitalization.

Goal O-7

Create an interconnected roadway network that provides parallel routes in Old Town.

Goal O-8

Manage and locate potential new parking to service existing and future new uses enhancing the visitors experience.





2 Existing Conditions

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Land Use

Land use consists of permissible activities and uses that occur within a piece of land. It plays an integral role in determining the overall functioning of a neighbourhood and the type of development that can occur on any given parcel. Various land uses come with associated building forms, densities, surface and edge treatments and parking provisions. Changes to the current land uses in turn transforms the overall character of the built environment and associated activities occurring within a given area.

West End

Existing Uses

On its northern face, the West End is dominated by low-rise, single-story retail and office uses. Multi-family apartments are just outside the study area but abut both the north and west perimeters. Most structures have significant setbacks from the street and tend to be oriented internally toward a centralized off-street parking lot rather than toward the adjacent roadway. Typical tenants include banks, professional offices, and franchise casual dining. One block north of Mainstreet but still within the study area is a movie theater.

The southern portion of the West End is occupied by multi-family residential (under construction), senior assisted living and community shopping center that includes a grocery store. Other smaller uses include a self-storage center (oriented north/south with only a small portion of the facility abutting Mainstreet), a small fenced utility area with two natural gas pressure regulation stations also adjacent to Mainstreet, and potential small pad lots on the outside edges of the shopping center.

The northeastern portion of the West End also includes two small parks on either side of the 'T' intersection of Stage Run and Mainstreet. The western park includes the historic Twenty Mile House which was moved to the site in the 1860's from its original location about a mile south. Also located on

this site are interpretive signage and ornamental plantings. The east park, known as Living Wheel Park, features a ring of seven flags arrayed within a circular planting and sidewalk area, however, does not have visible parking. Neither park appears to be used and are suitable only for passive activities such as reading or quiet contemplation which can be disturbed by passing vehicular noise.

This area's sole civic use is a local branch of the Douglas County Libraries. However, the library is currently constructing a new facility in Old Town at the east end of Mainstreet and will vacate its current facility when the new one is completed.

The Sulphur Gulch Trail is a major recreational amenity and runs east-west through the southern portion of the study area. The trail features underpasses at all major intersecting north-south roadways, and has an open space buffer on both sides of the paved path. Additional trail access to the West End is under construction at Dransfeldt Road.

Vacant/Undeveloped Parcels

North of Mainstreet near the movie theater one significantly sized parcel within the 'U' between Mainstreet and Stage Run remains undeveloped with no public or approved plans at this time. A small parcel adjacent to Mainstreet is also vacant with a willing-to-sell owner but the site has significant issues related to an underground gas easement and related above-ground controls.

A large parcel between the Sulphur Gulch corridor and the existing Target to the south remains vacant. Immediately east across Dransfeldt, the parcel south of the senior housing is under construction with the final Phase III of the senior facility campus.

Critical/Catalytic Parcels

The two parcels occupying the northwest and southwest corners of the Mainstreet/Parker Rd intersection are critical for creating a gateway into West End district of the downtown area, both from a wayfinding and an image point of view. Both citizens

and Town staff have indicated a desire for different land uses and urban structure on these parcels that can portray an attractive image, invite visitors into the West End and furthers the vision and goals of this plan.

The community shopping center has also been identified as a parcel that does not conform with a more pedestrian-friendly vision of the Mainstreet corridor. The lack of an active street edge and the resulting 'missing teeth' in this Mainstreet frontage are chief among concerns for this parcel.

Policy Guidance

The West End, as defined by this study, is within the Town's Greater Downtown Zone District. This district is divided into five sub-districts, each with their own 'Standards and Guidelines for Development' providing specific guidance for land uses and



parameters for urban form, architecture and site design. The West End area includes the following three zoning sub-districts; Twenty Mile Center, Town Center and a small section within the Market Center.

These sub-districts permit a variety of residential, office, retail, dining, and civic uses. However, in all three, single-family and duplex residential are not permitted and multi-family requires a special review process. Some vehicular-oriented uses, such as auto repair and tire sales, are permitted in these sub-districts with special review. Similarly, drive-through facilities are also permitted within these areas however, in the Town Center and Market Center sub-districts, properties adjacent to Mainstreet require a special review process. Parking structures are allowed in all three sub-districts.

Public and Stakeholder Input

Most stakeholders had very little to say about the West End area, whether positive or negative; rather, people seemed not to associate this part of town with the downtown experience. As one commenter noted, "Downtown stops at Parker Road." A handful of people did note that it would be nice if this west area felt more like Old Town, had smaller shops and felt more walkable.

Summary

Overall, West End's current land uses are not conducive to a walkable downtown precinct. Its lack of street oriented uses and active edges, with under utilized parcels and inactive park spaces diminishes its role within the wider area.

Old Town Existing Uses

Old Town has a different mix of uses than the West End, with smaller-scale retail and office/commercial uses mixed with a variety of civic, cultural and institutional uses. O'Brien Park, the Town's flagship park and home to a long list of festivals and community events, is also within this portion of the study area;

the park occupies the northeast quadrant of the Parker Rd/Mainstreet intersection. First floor active uses such as retail and dining are not contiguous, are scattered throughout the Old Town area and therefore contribute to the lack of active pedestrian-oriented ground floor uses. Office uses occupy storefront spaces within the Chamber of Commerce building (southwest corner of Mainstreet and Pikes Peak Drive).

Other uses within the Old Town plan area include an older strip retail center (oriented 'sideways' to Mainstreet with minimal street frontage), the Parker Senior Center located near the northern plan area boundary (offering lunch and daytime programming), a child day care, an assisted living facility and the RTD Park-n-Ride and bus facility. A significant portion of the study area is occupied by townhomes with a small portion of this multi-family development fronting Mainstreet itself. Cultural and civic uses include the newly renovated Mainstreet Center flexible community gathering and event space and the Parker Arts Culture & Events (PACE) Center. Town Hall occupies a large site at the eastern end of the Mainstreet Corridor. As noted previously, the Parker branch of the Douglas County Library will be moving to a new facility across from Town Hall within the short term.

Also, as noted previously, the Sulphur Gulch Trail runs through the entirety of the study area, passing through both the West End and Old Town districts.

Vacant/Undeveloped and Underutilized Parcels

Old Town has a number of undeveloped parcels of varying sizes. The parcel between O'Brien Park and the Parker Senior Center was purchased by the Town with Town and Douglas County open space funds and is slated for an eventual expansion of the park. Land between Sulphur Gulch Trail and Pikes Peak Avenue has been approved for higher density residential which currently is under construction as a multi-family housing project. The vacant parcel adjacent to Mainstreet at the southwest corner of Mainstreet and Pine Drive (west of the PACE Center)

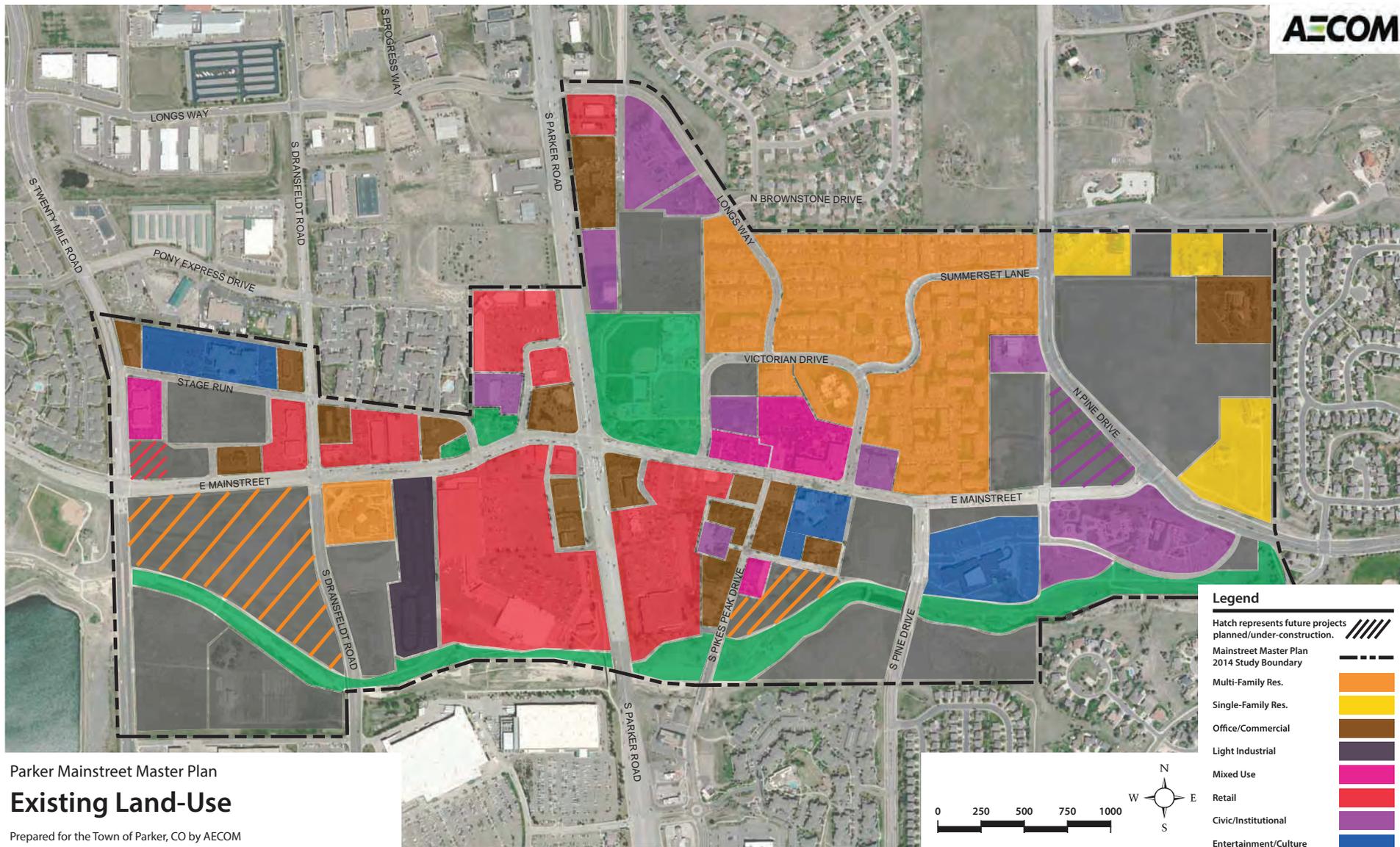
has no approved plans at this time. The large parcel north of the Mainstreet/PACE Center Drive intersection will be bisected by the PACE Center Dr extension to Pine Drive; the eastern half of the parcel will house the new Parker Library and the new Town park/plaza, while the western half will be privately developed. The Town would like to see vertical mixed-use development on the western portion of this property. The portion of land between the PACE Center parking lot and Mainstreet (referred to as Lot 2) has been discussed as a potentially developable parcel, but would need to preserve some views to the PACE's iconic architecture.

The area's largest undeveloped parcel at the eastern end of Mainstreet fronts Pine Drive and is commonly referred to as the Pine Curve property. The Town conducted a public Master Planning process in 2009 that resulted in the Pine Curve Master Plan which included three conceptual development scenarios for this parcel. The scenarios included medium box retail and pedestrian-oriented mixed-use alternatives. Also, in 2009 the Town released an Request for Proposals (RFP) seeking development partners for the site. The Town received two responses from developers both of which included a big box (125,000 s.f.) anchor - no plan has been formally approved at the time of this plan.

Critical/Catalytic Parcels

Although the Town has successfully created a downtown experience and destination, the area's land use remains patch-worked with an extremely limited inventory of active ground floor uses. In order to strengthen and expand the Old Town character and its economic vitality, all parcels abutting Mainstreet should be considered critical development/redevelopment parcels. Of particular interest are vacant parcels which create a gap in the street frontage, or uses which do not contribute to the desired retail/dining/arts/culture character of the district.

Old Town expands south from Mainstreet down Pikes Peak Drive and for this reason, the parcels abutting this intersection are of particular importance. The



Parker Mainstreet Master Plan
Existing Land-Use

Prepared for the Town of Parker, CO by AECOM

Figure 8 Existing Land Use

Existing land uses within the study area are widely varied, including residential, retail, commercial and civic uses.



'gateway' parcel at the southeast corner of the Parker Road and Mainstreet intersection, currently occupied by a bank, is also important for the same reason.

Policy Guidance

As noted previously, the Town's Greater Downtown Zoning District's five sub-districts each provide development and design standards and guidelines for land use and urban form within the Downtown Core character area. Old Town, as defined in this study, is within the Town Center district, Pikes Peak Center district and Historic Center district.



The Town Center sub-district accommodates a broad mix of retail and service uses on both the immediate east and immediate west sides of Parker Road. The Historic Center and Pikes Peak Center sub-districts permit a variety of residential, office, dining and civic uses. Single-family and duplex residential are not permitted in the Historic Center and require a special review process in Pikes Peak. Multi-family is permitted within the Historic Center, but requires special review in Pikes Peak Center. Vehicular-oriented uses, such as auto repair and car washes are not allowed in either district. Parking structures are not permitted within the Pikes Peak district and require special review in the Historic Center district. Common goals of all three zoning sub-districts are:

- Create a built environment that is in scale and character with pedestrian oriented activities
- Encourage alternative transportation choices
- Provide opportunities for new retail development and sales tax generation.



Old Town includes a variety of uses ranging from the beloved O'Brien park to strip retail, mixed-use office and even residential.

Public and Stakeholder Input

In general, residents and visitors alike seem to love Old Town, and would like more of it; specifically, input highlighted interest in more shopping and dining opportunities. The idea of outdoor patios and marketplaces were particularly popular and most people were very quick to emphasize that they wanted local, boutique, non-chain stores and restaurants. One individual wryly noted that downtown and the immediate area has more than enough banks.

Several teens provided input that they would like somewhere to gather downtown where they wouldn't be harassed for loitering; they were also interested in a second smaller skate park in the downtown area, noting that while the existing park near the near the Fieldhouse (a recreation center) is good, it's too far away.

There is also interest from stakeholders in an expanded art program: art displayed and for sale within Old Town, as well as a place for artists to work, exhibit and perform.

Summary

While this area's land use includes smaller scale, diverse retail uses that are more conducive to a vibrant, walkable vibrant downtown experience, there remains significant gaps in the retail frontage. Vacant lots and fragmented retail frontage detract from the overall experience and require changes in the land use to enhance the overall experience.

Events

Outdoor Venues and Events

Parker has a wide variety of community events and festivals held at both indoor and outdoor venues throughout the year. Within the downtown area, O'Brien Park is the most used space for outdoor festivals and events, including Oktoberfest, Movies in the Park, Art in the Park and Parker Days to name just a few. Other events such as Trick or Treat on Mainstreet, the holiday carriage parade and the Farmers' Market occupy Mainstreet and parts of Pikes Peak Drive requiring temporary street closures. Adjacent roadways typically remain open during these events. Events such as the monthly Chamber of Commerce Wine Walk occupy the sidewalks and create notable additional pedestrian traffic in the downtown area.

The Town's largest annual event by far is Parker Days, held over three consecutive days in June and drawing over 145,000 attendees in 2014. In addition to O'Brien Park, this festival uses the vacant parcel in the northwest corner of Victorian Drive (for carnival rides), as well as other smaller sites along Main Street. Festival organizers note that they did evaluate the vacant parcel at the southwest corner of Mainstreet and South Pine Drive as an alternate or expansion site for the carnival, but the site's topography and grade below street level made it difficult for public event use.

The PACE Center has its own small outdoor amphitheater however, it is underutilized due to its size and poor location hidden behind the building. PACE has also expressed interest in using its parking lot for outdoor events but is still exploring life-safety requirements related to closure of Pikes Peak Avenue (road in front of PACE Center entrance). A new grass area with a band shell area as well as a paved public plaza are planned by the Town adjacent to the new Douglas County Library site currently under design at the northeast corner of Mainstreet and Pace Center Dr and planned for construction in 2015.

Indoor Venues and Events

The Town owns two indoor cultural and event venues, the Mainstreet Center and the PACE Center. For indoor events, the Mainstreet Center, currently undergoing a name change to The Schoolhouse, is in high demand for its smaller and more affordable spaces. The PACE Center hosts its own programming and serves as home base for a number of independent community performances, such as dance recitals and youth theater as well as attracting regional and national acts. The new Parker library will have additional meeting space for events. Some indoor events branded or associated with the Town such as the Taste of Parker and Business Expo, take place at the Wildlife Experience, located approximately 5 miles northwest of the downtown area just outside the Town limits. This may change now that the University of Colorado owns the Wildlife Experience.

Policy Guidance

The *Parker 2035 Master Plan* provides general direction towards supporting and expanding cultural and scientific programming as well as community events, noting the economic development opportunities associated with promoting the Town as a premier arts and culture destination. Also, *The Old Town Creative District Plan (2013)*, a community-endorsed plan, identifies the creation of outdoor spaces to exhibit and perform art as a primary goal, paired with the need to create outdoor gathering spaces.

Public and Stakeholder Input

The community strongly supports event and festival programming and would like to see the cultural calendar expanded to include activities and events during all four seasons. A number of patrons noted that the PACE amphitheater, while nice, is too small and oddly located. Supporting this comment, a number of individuals would like to see additional public spaces, whether pocket parks or plazas. Local dance and theater schools and programs also noted that they could hold more classes and events if space were more readily available - youth theater in particular is limited by competing for rehearsal space and times in the Mainstreet Center. Professional event planners involved in organizing some of the downtown events felt the local demographics could support indoor 'market' events – such as antique shows or special-interest swap meets – if an appropriately sized facility were available.



Urban Form

Urban form refers to the overall physical, three dimensional layout and design of an urban environment. It is constituted through street widths, building heights, massing and set backs and the relationship between individual buildings. Buildings frame the open spaces between them, defining the public realm. Therefore, the urban form shapes the overall quality and character of the urban environment, particularly as experienced through the public realm.



West End

Height, Form and Massing

The majority of buildings in this segment of the Mainstreet corridor are one-story structures built in a simple strip-type format. Buildings tend to cluster in groups on two or three sides of an internal parking lot and do not face Mainstreet. The new two-story senior living facility somewhat departs from this model with the front door oriented on a diagonal to the adjacent intersection and place parking adjacent to the streets between the main structure and the sidewalk. The final phase of this continuum care complex is currently under construction with a maximum allowed four-story building.

Building height and massing should emphasize important corners and designate points of entry (gateways) in this corridor. Also under construction on the south side of Mainstreet west of Dransfeldt is a new three story single use apartment complex with 306 units. The complex will have nine apartment buildings and a clubhouse parallel to Mainstreet.

Architecture and Materials

The buildings in this area do utilize a significant amount of masonry, brick and faux-stone veneer. These treatments provide some link to materials used in Old Town. Architectural form, however, is generic and undistinguished and as such does not portray a particular architectural style.

Streetscape, Furnishings and Amenities

With the exception of the sidewalks on the south side of Mainstreet in front of the apartment complex that is under construction and the existing assisted living facility, the sidewalks along this segment of Mainstreet are attached. Widths vary but are generally no less than five feet. Most parcels display a fairly suburban aesthetic, with a manicured lawn and interspersed deciduous and coniferous trees separating the sidewalk from the building face or parking lot.

This portion of the corridor also features planted center medians. The medians include a small number of Parker's trademark 5-globe pedestrian lights along with a combination of trees, perennials and ornamental grasses. The five-globe lights are also included on the medians; the absence of trees in these medians does make the lights more obvious.

Intersections have street lights, typically placed at a height of approximately 30-feet. There are neither pedestrian lights nor other associated design elements, such as banners, hanging baskets, at-grade baskets or similar. The area does not have benches, trash cans or other street furnishings promoting walkability.

Signage and Wayfinding

All signage in the area is private and located behind the public right-of-way; multi-tenant signs for shopping areas are examples of this type of signage. There is no directional/wayfinding signage to help orient and guide residents and visitors to destinations, landmarks, services or trails in this corridor.

Policy Guidance

Referring again to the urban form parameters set forth in the Town's Greater Downtown District: Standards and Guidelines for Development within the Twenty Mile Center, this portion of the Mainstreet corridor is intended to act as the gateway into Parker's Central Commercial character area. Mainstreet building setbacks within the Twenty Mile Center district are determined at site plan review and within the Town Center design district, the Mainstreet setback is zero feet. Both districts impose a 60' or four-story height limit and require ground floor materials to be predominantly storefront windows or masonry.

Public and Stakeholder Input

Few people had comments regarding the West End area, although some respondents did note that they would like to see the West End look and feel more like Old Town—it is too spread out, too auto-oriented, and they'd like to see it have the same pedestrian feel and 'look' of Old Town. They would also like more continuity between the two 'sides' of Mainstreet, east and west of Parker Road.

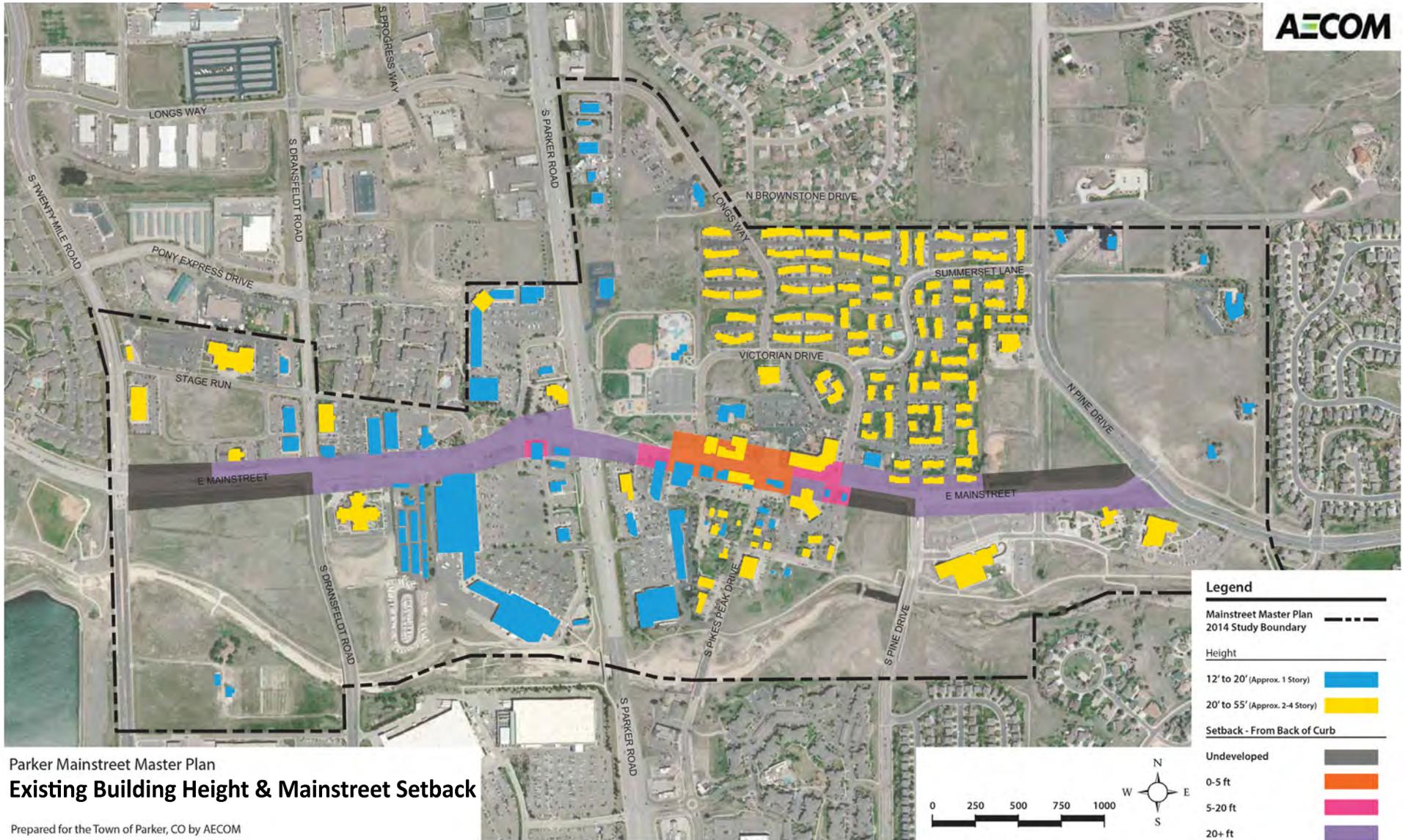


Figure 9 Existing Building Height & Mainstreet Setback (as of August 2014)

The study area includes a large number of one-story buildings. The majority of the study area has significant setbacks from the roadway, a condition not aligned with the pedestrian-oriented goals of the downtown area.

Old Town

Height, Form and Massing

Within Old Town, buildings display a different, generally smaller scale than that found west of Parker Road. Most buildings along this segment are two or three story with pitched roofs or articulated parapets, creating a higher level of pedestrian interest. The buildings are assembled in shorter groupings and almost without exception face the street. Buildings approach the back of sidewalk, with parking provided on-street and/or behind the buildings. All of these elements of urban form help create pedestrian friendly streets which are often the most active and memorable places in our community. People are naturally attracted to these visually and physically 'comfortable' and interesting places!

Old Town does include an older strip retail center, but the center is oriented 'sideways' to Mainstreet with minimal street frontage. The strip center also includes a bank and a drive-through restaurant, somewhat mitigating the visual impact of the center's large central parking lot from Mainstreet and Parker Road.

Architecture and Materials

The central portion of Old Town strives to create a historically-referenced western storefront architecture. This style is expressed in the newer Vines and the Parker Station multi-tenant buildings; smaller stand-alone structures such as the office spaces along Pikes Peak Drive and Pikes Peak Avenue provide



examples of more residential Victorian-style buildings with porches set back from the street. Materials tend toward brick, masonry and clapboard siding.

Some parts of the eastern portion of Old Town give indication of an emerging contemporary aesthetic. The tone of this area is set by the PACE center which utilizes intersecting planes and more modern materials, such as blonde and black brick and Corten steel (rusted metal). The new Parker library will have a contemporary design, form and massing as well. The area's other predominant building, Town Hall, uses a similar modern form of western architecture and materials. The existing 1980's townhomes fronting Mainstreet in this section, however, reflect a residential Victorian-style aesthetics.

Streetscape, Furnishings and Amenities

The Town undertook a recent streetscape revitalization effort through Old Town; these enhancements include wide colored concrete sidewalks and crosswalks, wrap-around curb ramps with integrated iron detectable warning domes and grated street trees in select locations. The Town's distinctive 5-globe lamps are located in the roadway's planted medians and mounted on stone pedestals occupying the four corners of the Mainstreet/Pikes Peak intersection. Black Victorian-style pedestrian lights line the street along Mainstreet from Parker Road to Victorian Drive east and in front of the PACE Center. Pedestrian lighting is not located in front of vacant parcels, Old Town Hall and the townhomes along Mainstreet and are also not located along streets other than Mainstreet in Old Town. Parker Station's widened sidewalk and outdoor dining areas illustrate well planned urban design, form and the pedestrian-oriented amenities desired by many stakeholders.

Signage and Wayfinding

Old Town has introduced a few wayfinding monuments at select intersections. These signs point visitors to major attractions, such as PACE Center, Town Hall, Mainstreet Center, O'Brien Park and the Sulphur Gulch



Trail. These monuments also include 'district' markers and are constructed of brick with a stone veneer base.

Policy Guidance

Referencing standards contained in the Town's Greater Downtown District zoning policy, the Historic Center design district promotes historically-referenced storefront architecture oriented toward Mainstreet and requires a 0-foot build-to line with wide sidewalks that include pedestrian amenities. Heights are limited to 60-feet or four stories and architectural materials must be siding or masonry.

The Pikes Peak Center design district encourages a Victorian architectural style, and requires buildings to orient toward Pikes Peak Drive or Pikes Peak Avenue. Heights are limited to 45-feet and setback is directed to create a street edge consistent with adjacent and existing development with an attached sidewalk and amenity/landscaped area.

Public and Stakeholder Input

Public comment indicated an overall appreciation for the style and scale of Old Town. People also requested more trees and more places to sit and additional lighting on Pikes Peak Drive to draw visitors off Mainstreet and down Pikes Peak Drive. Few people had any comments regarding signage, although bike signage (directional and to alert motorists) as well as better signs to parking were mentioned.

Circulation

Vehicular Hierarchy

Roadways are generally classified according to their function, access and volume; the study area includes—from highest (heaviest traffic) to lowest (least traffic) — highways, arterials, collectors, local roads and Old Town streets (see Figure 10). Along with describing the roadway hierarchy, the following discussion also describes the Level of Service (LOS) for roadway segments in the Plan study area. LOS is a way to measure and analyze the quality of traffic flow and uses the letters A through F with A being the best and F being the worst.

At the ‘top’ of the hierarchy, the study area is bisected by State Highway 83, more commonly referred to as Parker Road. Parker Road currently functions at a Level of Service (LOS) D north of Mainstreet and LOS E south of Mainstreet. These two capacity descriptors are characterized in the 2014 Transportation Master Plan (TMP) as ‘speeds begin to decline with increased density’ (LOS D) and ‘roadway at capacity’ (LOS E). With no mitigating measures or enhancements, Parker Road is expected to operate at LOS F—over capacity and with significant delays—by 2035.

Next down in the hierarchy are arterials, including Twenty Mile Road, Mainstreet (between Dransfeldt Road and Twenty Mile Road) and Pine Drive. The section of Mainstreet between Twenty Mile Road and Dransfeldt, currently classified as an arterial, is recommended to be changed to an Old Town Street, which addresses an acknowledgment in the TMP that the final classification of this section of road would be determined as a part of this Mainstreet Master Plan process. According to 2012 volumes, Twenty Mile Road is operating at or near free flow (LOS C) north

of Mainstreet and at decreased speed (LOS D) south of Mainstreet. Pine Drive is operating at decreased speed (LOS D) along the entire study area portions. Collectors include Dransfeldt Road., Longs Way and Pine Drive south of Mainstreet. Only Dransfeldt is evaluated in the Parker TMP and is at LOS C. A small portion immediately south of Mainstreet is anticipated to change to LOS D with no upgrades.

Mainstreet, an Old Town Street from Dransfeldt Road to Pine Drive, is operating at decreased speed (LOS D) along the entire study area portion. The Old Town portion of Mainstreet is projected to change to LOS E by 2035. All other roadways within the study area are classified as Old Town streets and local streets that are expected to carry only local, low-speed, low-volume traffic.

Traffic Control

West of Parker Road, all named roadway intersections are signalized, with the exception of Crossroads Drive. East of Parker Road, named roadway intersections are also signalized, with the exception of both Victorian Drive intersections; both of these intersections have stop conditions for Victorian Drive.

Mainstreet Bypass

Twenty Mile Road to Hilltop Road, Pine Drive north of Mainstreet to Lincoln Avenue, and Longs Way and Pine Drive south of Mainstreet to Hilltop Road offer alternate routes to bypass the Mainstreet corridor, depending on the traveler’s origin or destination. The Twenty Mile Road/Parker Road and Mainstreet/Pine Drive intersections are signalized, while the Longs/Parker Road is currently controlled by a 2-way stop on Longs Way. A signal is proposed to be installed in the future.

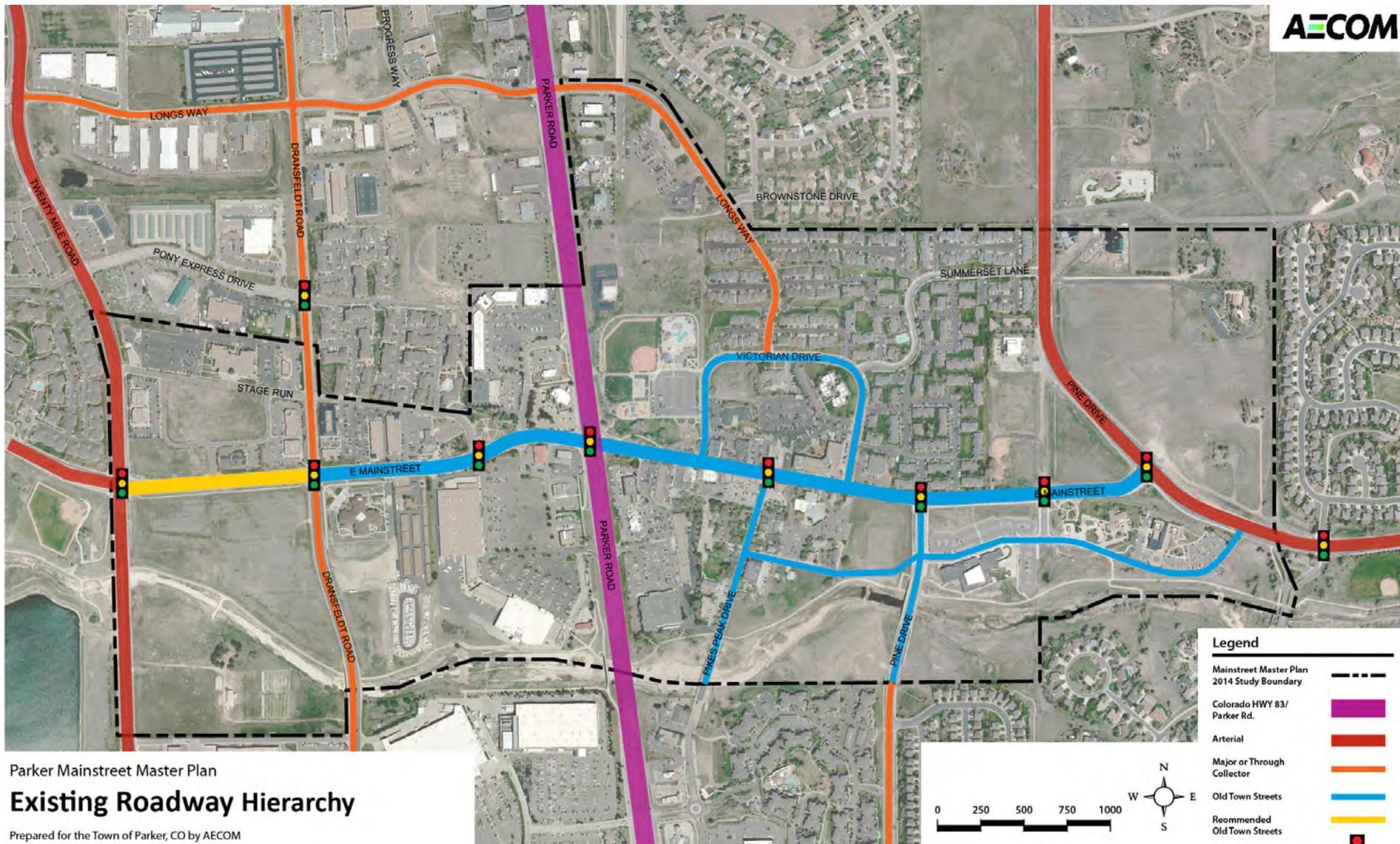
Policy Guidance

A number of adopted Town documents promote the creation of new vehicular roadways that would create a grid-like network throughout the downtown area and beyond, with a particular emphasis on east-west alternatives to Mainstreet. Specific roadway connections were identified in the *Greater Downtown District Circulation Network Visionary Plan (2003)*, the *Parker 2035 Master Plan (2012)* and the *TMP (2014)*. Future connectivity within the study area includes an east-west connection south of Mainstreet and roughly through the big box retail and strip malls east of Parker Road. These policies also emphasize the need to work with the Colorado Department of Transportation (CDOT) to mitigate the negative impacts and leverage the benefits of SH 83 (Parker Road) on the local community.

The Town’s most recent policy document, the *TMP (2014)* provides detailed guidance on roadway design, as well as current data regarding traffic volumes and design.

Public and Stakeholder Input

An overwhelming number of stakeholder comments—on any topic—referenced Mainstreet congestion. Many respondents indicated that they avoided the downtown area due to the difficulty of getting through the area. A few noted that the ‘Mainstreet Bypass’ seemed to work well enough, but the idea or use of alternate routes did not appear to be widespread. Many also voiced concern about cars traveling too fast in the downtown area especially as pedestrians cross Mainstreet mid block and not at crosswalks.



Parker Mainstreet Master Plan
Existing Roadway Hierarchy

Prepared for the Town of Parker, CO by AECOM

Figure 10 Existing Roadway Hierarchy

A lack of east-west through routes means that Mainstreet must serve both local, downtown destined traffic as well as regional pass-through traffic.

Pedestrian

Virtually all the public roadways within the study area have sidewalks, with only a few gaps in the system. Of the segments missing sidewalks, the east side of Parker Road is the most notable and lacks sidewalk throughout the entire length of the study area. An approximately 800-foot segment of Pikes Peak Ave. in front of Town Hall is also sidewalk deficient on both sides; the Sulphur Gulch Trail does provide a certain degree of connectivity in this area, but does not provide sidewalk between the Pine Drive/Mainstreet intersection and Town Hall.

Sidewalks vary in width and cross-section with attached walks directly abutting the curb being prevalent throughout the study area. Detached sidewalks with planted or rock-filled amenity zones do occur in some of the residential areas. Street trees in grates do occur intermittently throughout Old Town, in front of the PACE Center, and generally from the Victorian Drive eastern intersection to the Victorian Drive western intersection.

Most sidewalks have what appear to be fairly recently-upgraded curb ramps. West of Parker Road, most ramps are non-directional (oriented diagonally into the intersection, rather than in-line and parallel to the direction of travel) and colored to contrast with adjacent pavement; these ramps do not have detectable warning panels at street edge. Crosswalks are typically painted 'zebra' stripes, and do not include auto stop lines in front of them - a later recommendation to increase pedestrian comfort.

East of Parker Road on Mainstreet, ramps are wrap-style (still non-directional, but wrapping around the entire curve instead of a single central point) and colored to contrast. These ramps feature iron detectable warning strips at street edge. Crosswalks are colored, scored concrete. Both Victorian Drive and Mainstreet intersections are raised. Blocks with on-street parking also feature pedestrian bump-outs at intersections, to shorten crossing distance.

All signalized intersections along Mainstreet are equipped with pedestrian-call buttons to request a 'walk' phase. Pedestrian crossing signals do not default to a walk phase unless requested by the user.

The area's most challenging pedestrian condition is the intersection of Parker Road and Mainstreet, followed by the east and west intersections of Mainstreet and Victorian Drive. Pedestrians crossing Parker Road at Mainstreet must cross 8 lanes of traffic and approximately 135' of arterial roadway, a daunting proposition. The northeast corner of this intersection also uses a 'pork chop' or 'free right' turn configuration, which allows turning vehicles to continue without stopping even though 'yield' signage does exist at this location. Two grade-separated alternatives to this crossing are available but unsigned; the Kieffer's Crossing underpass is located approximately 375' north of the intersection and the Sulphur Gulch Trail underpass is approximately a quarter-mile south of the intersection. However, there is no sidewalk connection on the east side of Parker Road from Mainstreet to the Sulphur Gulch Trail.

Policy Guidance

The Transportation Master Plan (2014) provides a variety of goals and strategies regarding pedestrian and bicycle circulation and highlights the need to consider Complete Streets principles in all future infrastructure upgrades, supported by transportation policies in the Parker 2035 Master Plan. The TMP articulates that sidewalk design should use context sensitive solutions in order to be responsive to adjoining land uses and recommends the introduction of mid-block pedestrian crossings where appropriate. The Plan specifically recommends detached shared-use bike/pedestrian paths along Parker Road, as well as on-street bike facilities on all collector and arterial roadways. The Plan does recognize the special context and constraints of Mainstreet and recommends 'unique bike treatments' to balance character and mobility in this area. Also, Chapter 10 of the Parker 2035 Master Plan supports a 'pedestrian transit' bridge across Parker Road just north of the O'Brien Park baseball field.

Public and Stakeholder Input

Public input identified a handful of pedestrian 'hotspots' which were perceived to be unsafe: crossing Parker Road and the west intersection of Victorian Drive/Mainstreet. When asked, most people were unaware of the Kieffer's Crossing and Sulphur Gulch Trail underpasses as an alternative to crossing Parker Road at grade. There was some concern regarding ADA access, particularly with Old Town sandwich boards which could make it difficult to navigate the area in a wheelchair. There is also interest in improvements to Pilgrims Place, the alley directly behind the Chamber of Commerce building on the south side of Mainstreet, and the potential to create a unique, multi-use public space and street environment.



Old Town (top) offers comfortable pedestrian crossings; Parker Road (middle) is a difficult 8-lane divider between Old Town and the West End; the West End (bottom) is a challenging pedestrian environment due to roadway scale and lack of enclosure from adjacent buildings.

Bicycle Facilities

Pine Drive south of Mainstreet features the only on-street bike lane within the study area. The approximately half-mile striped lane is continuous from Rodeo Circle to Mainstreet and offers north-south connectivity between the Sulphur Gulch Trail and Mainstreet. The Sulphur Gulch Trail is an off-street, multi-use facility paralleling Mainstreet that travels through an open space corridor and is offset a tenth to a quarter-mile from Mainstreet. The Sulphur Gulch Trail also connects users with other off-street trail links including the Cherry Creek Regional Trail.

Bike Parking

Bicycle racks are located sporadically with no centralized public bicycle corral in the study area. Racks are located at public facilities including the O'Brien Park intersection of Mainstreet and Victorian Way, at H2O'Brien Pool, Mainstreet Center, Town Hall and at the PACE Center. Also, bike racks can be found at other commercial destinations.

The map on the following page shows existing bicycle facilities and missing sidewalk links within the Plan area.

Policy Guidance

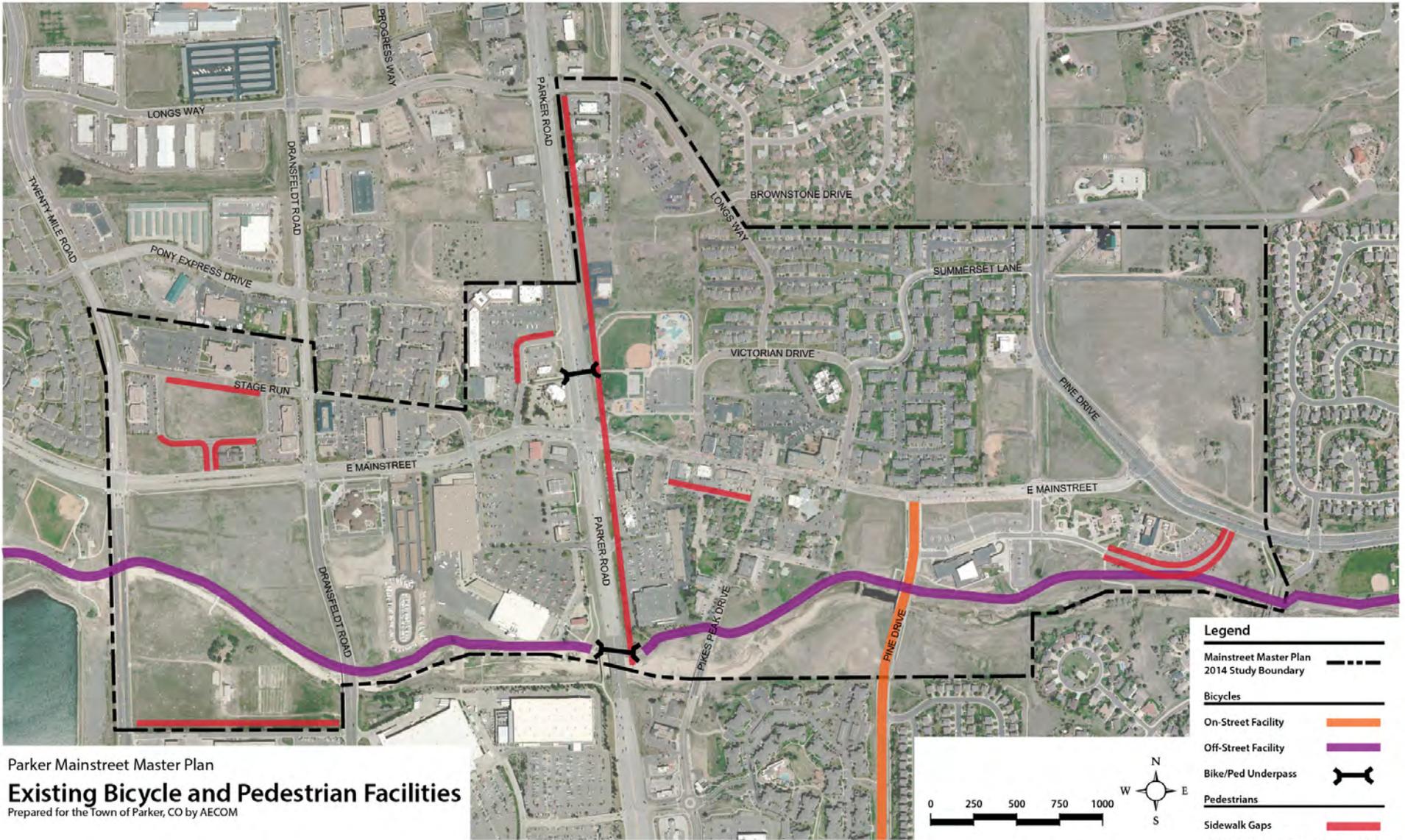
As noted in the preceding 'Pedestrian' section, the Transportation Master Plan specifically recommends inclusion of on-street bike lanes on all collector and arterial roadways with acknowledgment that Mainstreet will require special contextual treatment for bicycles. The Plan recommends detached shared-use bike/pedestrian paths along Parker Road.

The Bike Lane Plan (2005) establishes the Town's desires for community-wide bike facilities and includes proposed sections for the inclusion of on-street bike lanes; these sections and recommendations were updated by more current best practices described in the 2014 Transportation Master Plan.

Public and Stakeholder Input

Many stakeholders expressed an interest in biking to downtown but cited a lack of bicycle facilities and resulting safety concerns as the main reason they did not do so. When asked what type of facilities they would like to see, there was a definite preference for off-road facilities. Safety and family-friendliness were the most common reasons cited for this preference. Many people felt that parallel facilities rather than bike facilities on already congested Mainstreet were acceptable. There was also interest in centralized bike parking, so that people could arrive by bike, park and then walk the Mainstreet corridor.





Parker Mainstreet Master Plan
Existing Bicycle and Pedestrian Facilities
 Prepared for the Town of Parker, CO by AECOM

Figure 11 Existing Bicycle & Pedestrian Facilities

The Sulphur Gulch trail parallels Mainstreet and provides connectivity to regional trails.

Transit

Bus

The study area is currently served by three Regional Transportation District (RTD) bus routes:

- **Route 410**, a 'feeder' route connecting the downtown area with the Lincoln Station light rail stop
- **Route P**, an express route providing service along E-470 and I-25 terminating at Civic Center Station in downtown Denver
- **Route 153** with service along Chambers Road through Aurora to Montbello

The 410 and the P originate at the Pinery Park-n-Ride and serves the Stonegate Park-n-Ride and the Parker Park-n-Ride located on Longs Way, just east of Parker Road and immediately north of the Parker Senior Center. The 153 originates at the Parker Park-n-Ride. Route 410 provides service at approximately 30-minute intervals during morning and evening peak hours, weekdays only. Route P is also limited to weekday rush-hour service, with approximately 20 minute intervals. Route 153 provides hourly service during AM and PM peak periods with an approximately 1 hour and 20 minute ride time to the Montbello Park-n-Ride; There is no bus service in Parker on the weekends.

The P currently accesses the Parker Park-n-Ride via Mainstreet and Longs Way; a signal may be installed in the future at Longs Way and would provide opportunity for this route to avoid Mainstreet. The Parker Park-n-Ride offers 173 spaces, 2 bike racks and 6 bike lockers; there is no fee for parking.

The Town continually works with RTD to improve bus transit service for its citizens. At the time of this writing, RTD is considering enhanced service in Parker, that if approved by RTD's Board, would go into effect in 2016.

Light Rail

Light Rail (LRT) routes E and F serve the Lincoln Station, located in Lone Tree, from approximately 4:30 am to 2 am, at 6-9 minute intervals during peak hours and 10-15 minute interval off-peak. Both lines serve the I-25 corridor and downtown Denver, with termini at Union Station and the Central Business District; total travel time to either downtown terminus is approximately 40 minutes. When the I-225 LRT extension comes online (scheduled for 2016) patrons will be able to transfer trains and travel northeast along the I-225 corridor to Aurora and connect with the East Line commuter rail (also scheduled to open in 2016) to Denver International Airport. Travel times for this segment are not available at this time. Patrons will also be able to connect to the East Line at Union Station in downtown Denver. Lincoln Station offers 1734 parking spaces and charges a \$2 daily parking fee for non RTD residents.

Parker Park-n-Ride

The Parker Park-n-Ride has 173 parking spaces and serves all bus routes within Parker. The Park-n-Ride is integral to Parker's transit infrastructure and provides a centralized multimodal hub for the Parker area. This site has the potential to redevelop as a transit oriented development (TOD), further intensifying uses in the downtown area.

Call n Ride

The study area is also served by RTD's Call-n-Ride. This shuttle bus service must be booked in advance (a minimum of 2 hours and up to 2 weeks prior) and is subject to the same fares as regular bus service. The curb-to-curb service is open to all users regardless of ability and is frequently used by patrons with mobility limitations who cannot transport themselves to a regular stop or station.

Future BRT

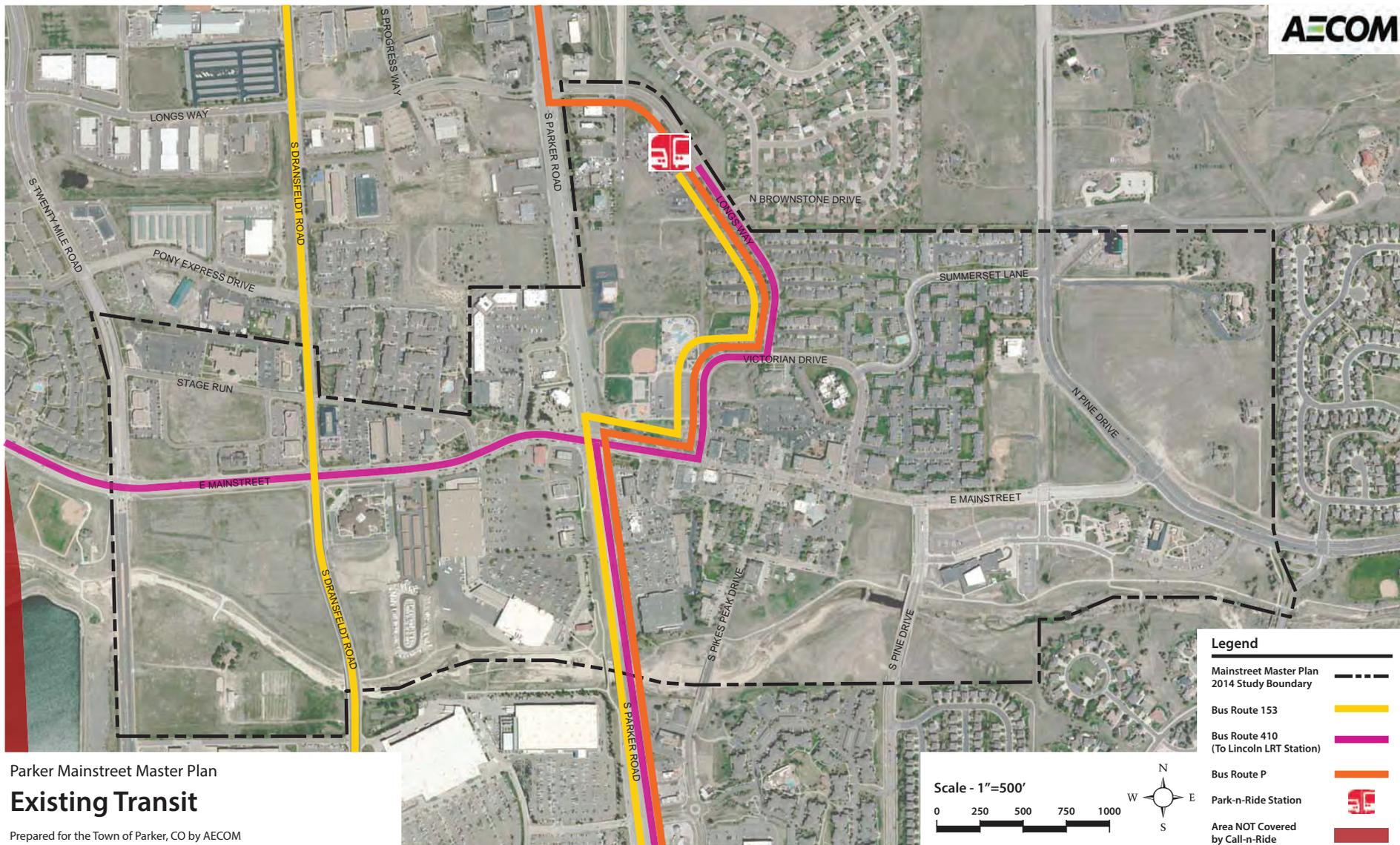
In cooperation with RTD, the Town also studied the feasibility of an LRT extension or bus rapid transit (BRT) to connect Parker with the proposed extension of the Southeast LRT to the new RidgeGate Parkway Interchange at I-25. Completed in 2005, the study recommended BRT running in a dedicated lane on Mainstreet beginning at Parker Rd and heading west as the preferred alternative. No timeframe or funding for implementation was identified.

Policy Guidance

The Transportation Master Plan (2014) recommends that the Town work with RTD to expand service hours and frequency of both regular bus service and Call-n-Ride services. This document also recommends route modifications to provide service to the 9-Mile LRT station. The Parker 2035 Master Plan echoes the desire for expanded transit service and a continued interest in the future BRT line.

Public and Stakeholder Input

Stakeholders exhibited a strong desire for transportation alternatives. Most-cited issues with current bus service were a lack of mid-day/evening and weekend hours. People were also dissatisfied with current commuter service, which provides valuable connection to light rail but does not provide connectivity within Parker itself. A number of people inquired as to whether a local trolley/shuttle type service—similar to Denver's 16th Street Mall Shuttle—could be possible for Parker's core Mainstreet area.



Parker Mainstreet Master Plan

Existing Transit

Prepared for the Town of Parker, CO by AECOM

Figure 12 Existing Transit

The study area is served by three RTD bus routes, as well as Call-n-Ride service; bus service is commuter-focused and does not offer weekend service.

Parking

Daily Parking

On-Street Parking

The Mainstreet corridor offers on-street parking throughout most of Old Town; the approximately 94 spaces are free and have no time limits. Local merchants have observed that this lack of restrictions has led to a lack of turnover with employees and second-floor business workers occupying spaces for the bulk of business hours versus customer parking. On-street parking is also available on major intersecting roadways in Old Town including Victorian (50 spaces), Pikes Peak Drive and Avenue (100 spaces) and Pine Drive south of Mainstreet (20 spaces). On-street parking is not permitted on Mainstreet or Pikes Peak Drive during snow events.

On-street parking is not provided in the West End or its intersecting roadways west of Parker Road. The Old Town area has large supply over 1050 public parking spaces that are free of charge and have no time limits.

Off-Street Parking

There are approximately 763 public spaces scattered throughout Old Town, in lots ranging from 18 to 182 spaces in size. The largest parking reservoirs are at O'Brien Park (169 spaces), the Main Street Center (119 spaces), the PACE Center (257 spaces) and Town Hall (135 spaces). Smaller lots exist off Pilgrim's way adjacent to St. Matthew's Church and near old Town Hall. Not included in this count are the 173 spaces at the RTD Park-n-Ride which are often treated as public parking during events.

There are also two large private, off-street parking areas in Old Town. The approximately 265-space lot behind the Parker Station is building is privately owned and operated as paid parking. The second large private parking area is located internal to the strip mall behind Baskin Robbins and has approximately 230 spaces. Smaller amounts of private parking, also often treated as 'public' but technically reserved for tenants of the adjacent offices and businesses, are located behind and to the east of the Sagecare building and behind the Chamber of Commerce on the north side of Pilgrims Place.

All parking in the West End is private parking.

Event Parking

Parker has a wide variety of community events and festivals held at both indoor and outdoor venues throughout the year. While indoor events tend to be associated with some sort of facility-related parking, it is the larger outdoor events that challenge the Town's roadway and parking capacity.

The largest event by far is Parker Days which is held over three consecutive days in June drawing over 145,000 attendees in 2014. The Town does not and cannot hope to have the capacity to park this many people in public facilities. This type of event has a significant impact and is highly dependent on private parking lots.

Smaller events also challenge the Town's parking supply. During all of these events residents and visitors compete for the Town's limited parking supply adding to the vehicular congestion as they circle for spaces.

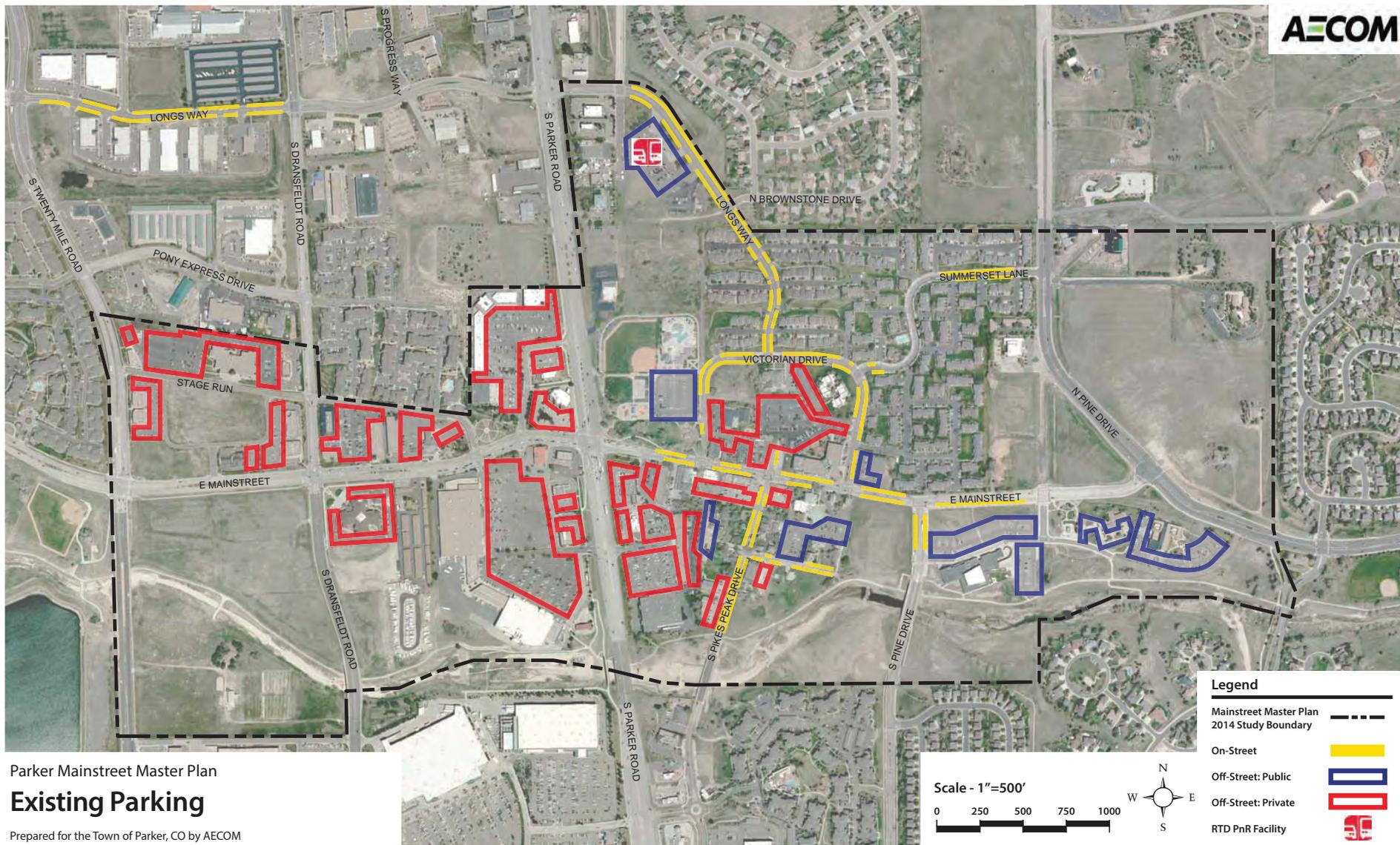
Policy Guidance

The *Transportation Master Plan (2014)* recommends a full professional parking study for Old Town to evaluate current and future needs; the plan also notes a need to maximize on-street and public parking in Old Town. This document also recommends review and updating of the *Land Development Ordinance's* parking standards and notes that areas within the Pikes Peak Center and Historic Center Districts are currently exempt from minimum off-street parking requirements in the Land Development Ordinance. Shared parking is permitted under existing policy.

Public and Stakeholder Input

Parking placed as one of the top concerns of residents and visitors alike. While most people were open to parking being a 'block or two' back from Mainstreet, there was concern that families with young children and seniors not have to walk too far. Overall, the prime complaint was that there simply wasn't enough parking during events. There were also suggestions of better signage indicating the location of parking. Some respondents wondered if a public parking garage would be possible and one individual suggested that the area north of O'Brien park be used as overflow parking.





Parker Mainstreet Master Plan
Existing Parking

Prepared for the Town of Parker, CO by AECOM

Figure 13 Existing Parking

The study area offers approximately 260 on-street and 763 off-street public parking spaces. There is also a significant amount of private, off-street parking associated with specific businesses and properties.

General Parker Market Overview and Industry Trends

The Town of Parker is located in northern Douglas County approximately 20 miles southeast of downtown Denver. Parker is 21.2 square miles with a current population of approximately 49,000 within the incorporated Town boundaries and a service/trade area population of 126,000. The Town is located five miles east of I-25 and is bisected by State Highway 83 (Parker Road). Businesses and citizens enjoy Parker's natural beauty and view of the Rocky Mountains, as well as exceptional amenities such as an extensive trail system and open space, excellent recreational amenities, superior schools, diverse retail, an efficient transportation system, generous housing and availability of commercial real estate stock. The Mainstreet Master Plan study area – also referred to as the 'downtown' area – is centrally located within the Town and includes both sides of Parker Road from Twenty Mile Road on the west to Pine Drive by Town Hall on the east.

The Parker market is full of excellent opportunities for investors, developers, businesses and educators. The Town boasts a highly educated and talented workforce and a very desirable high quality of life. The median household income is \$104,877 and the median age is 34 years. On the ground, Parker is a well-planned, progressive community that balances a hometown feel with the conveniences and amenities of a larger city. Economic development is a priority for the Town and there are various initiatives and incentives to support business recruitment and retention including urban renewal. The Study Area

(Plan Area) is located within the Parker Central Area Reinvestment Plan area which provides businesses, developers and property owners with access to urban renewal tools to support redevelopment. The Town also desires to increase the daytime economy and primary employment, both within the Study Area and throughout Town and should continue to seek additional programs and incentives to further this goal.

Study Area Existing Conditions

The Study Area consists of approximately 380 acres, 0.6 square miles, located along Mainstreet on the east and west sides of Parker Road. Currently, the Study Area is occupied by a mix of retail, restaurant, office, residential and public uses. The area has generally seen low business turnover and low commercial vacancy rates reflecting the popularity of the corridor and a stable business environment. However, this stable business environment has not translated into new construction of commercial and office spaces and business growth which is needed to ensure the critical mass necessary to be more economically sustainable.

Physical and locational attributes are an important determinant in gauging a site's market readiness. Several prime infill sites and redevelopment opportunities in the downtown area offer the critical characteristics of location, access, visibility, trade area demographics, psychographics and a competitive edge as market ready sites. Psychographics is the grouping of people according to their attitudes and preferences that is typically used for market research. Table 1 provides an evaluation of key site attributes, both in their current and expected future condition, for the Study Area. Additional data and maps can be found in Appendix B: Market Analysis that summarizes the existing conditions that will affect new development/redevelopment.

Site Analysis Indicator	Current	Future
Access	Excellent	Excellent
Visibility	Excellent	Excellent
Transportation Network	Good	Good
Surrounding Land Uses	Fair	Good
Scenic and Topographic Attributes	Good	Good
Community Perception	Good	Good

Table 1 Mainstreet Study Area Site Analysis
Source: Ricker/Cunningham

Land and Improvement Values

The Study Area is roughly 380 acres, 0.6 square miles and 734 parcels.

- According to Douglas County Assessor data, 537, or 74% of the parcels (17% of the land area) are residential, 68, or 9% of the parcels (30% of the land area) are commercial, and 19 or 2% of the parcels (7% of the land area) are vacant or agricultural in use. The remaining parcels are exempt or utilities.
- Total residential land values exceed \$8.3 million, while total commercial land values exceed \$32.6 million.
- Total residential improved values exceed \$53.4 million, while total commercial improved values exceed \$74.8 million.
- Average land value - \$133,300 per acre, which amounts to \$3.06 per square foot
- Average improved value - approximately \$90 per square foot.

Ownership

In any revitalization area, strong presence of local property ownership and consolidated larger ownerships are desirable. Local ownership allows for more attention to investment rather than being managed as a large portfolio. Consolidated larger ownership simplifies the redevelopment process.

- Local ownership (Parker residents) comprises 82% of Study Area parcels, 72% of land area, and 54% of assessed value.
- Only 5% of properties are owned by out-of-state interests, representing 10% of the Area's total land area.
- The Town is the largest property owner in the Study Area with 25% of the land area, not including right of way.
- The next four largest property owners in the Study Area own approximately 60 acres or 16% of the Study Area, not including right of way.

Site utilization, which measures economic utilization of property, is perhaps the most effective measure of an area's "ripeness" for redevelopment. When the value of improvements relative to land value is disproportionate, this is an indication that sites can support new investment and/or redevelopment. A utilization ratio of improvement to land value of less than 1.0 is an indicator of underutilization and potential economic stagnation. The Study Area shows a relatively high level of vacant or underutilized property which indicates the area could support new investment and redevelopment activities. The existing developed properties also show a wide range of utilization meaning that not all of the parcels in the Study Area would support new investment or redevelopment and therefore the Town will need to focus efforts on the parcels with the greatest opportunity.

In addition to the conditions described above, urban form or 'placemaking' elements such as plazas, trails and public spaces have a profound impact on the marketability of the Study Area. Old Town has some of these placemaking elements, while West End does not. Not surprisingly, this difference is reflected in the level of economic activity or commercial intensity in these areas.

Commercial and Office

Within the Mainstreet Master Plan area, there is approximately 747,000 square feet of variable commercial and office space which equates to approximately 15% of the commercial and office space in the Town located on 3% of the overall land area. The Study Area includes 363, or 19% of the 1885 licensed businesses in Town. The Study Area has consistently generated 5.5 – 6% of the Town's sales tax revenue over the last 5 years. Overall the sales tax generation is low compared to the percentage of commercial and office space that is within the Study Area.

At the time of adoption of this Plan, there are no new commercial or office projects under construction or in the development review process within the Study Area. Outside of the Study Area there are retail and quick service restaurant projects in different stages of development. At the time of this Plan, there are no office projects under construction or in the development review process outside of the Study Area. There are several available infill and redevelopment opportunities within the Study Area to fulfill the market demands and fulfill the goals of this Plan.

Residential

Housing prospects for almost all residential property types have shown strong improvement in the last two years. The current trend in residential real estate is driven by the relationship of supply to demand. Low supply and high buyer demand has resulted in rising home values in the last two years and increased new home construction in the Town. The population group known as Gen Yers also referred to as Millennials (born 1981-2000), are considered to be the least

likely among all age cohorts to buy their own homes and make up a large percentage of renters. Trends among Baby Boomers (born 1946 to 1964) include selling their larger homes and seeking to either rent or buy smaller units within walking distance of downtown areas or other commercial concentrations, or relocate into developments for active seniors.

After the recent great recession, Parker saw a decrease in the vacancy rate and an increase in average rents in the housing rental market. According to the Denver Metro Apartment Vacancy and Rent Survey put out by the Apartment Association of Metro Denver, for the 3rd quarter of 2014, apartments reported a vacancy rate of only 3.8% for northern Douglas County, including Parker, and an average rent of \$1390.81. Overall the vacancy rate in northern Douglas County is about the same as the metro area average of 3.9% and rents are approximately 18% higher than the metro area average of \$1145.49. The higher rents and the attraction of revitalized downtown areas continue to keep apartment development as an emerging trend in 2015. The strong apartment market is transforming downtowns throughout the country and metro Denver, including Parker's downtown. Adding more housing, retail, dining, entertainment and offices in the downtown core spurs investment, development and raises the quality of life for the community.

The improving apartment market is reflected in a significant increase in apartment construction in the Study Area and the Town. At the time of this Plan, two new apartment projects are currently under construction within the Mainstreet Master Plan area – Watermark with 306 units and Parker Flats with 146 units – and another 270 units are currently in the development review process. The Morningstar Senior Living expansion, also under construction in the downtown, includes an additional 126 senior assisted/independent living housing units to the area. On the north side of Town, near the E-470 corridor, there are another 285 apartment units are currently under construction and a 400 unit complex is currently in the development review process. Parker

currently has 2,451 existing multi-family units in five different apartment developments. Condominium construction in Parker has been stalled for the past several years, along with the rest of the state, due to a state construction defects law. However, as an ownership option, they are a critical component to any downtown housing stock, including Parker.

Summary

Mid- to long-term trends which will continue to impact demand for non-residential real estate both in the Study area and regionally include:

- Pressure from the Gen Xers (born 1965 to 1981) and Millennials for smaller energy efficient work spaces (forecasted to drop from 180 square feet per employee today to 100 square feet per employee in the foreseeable future);
- Flexible work schedules which allow employees to divide their time between home and work;
- Space sharing;
- On-line retail purchases that result in less need for in-store inventories (and therefore smaller commercial spaces); and
- Larger warehouse and distribution centers to serve the on-line retail industry.

Mid- to long-term trends which will continue to impact demand for residential real estate both in the Study Area and regionally include:

- A continuing increase in the Denver metro area population due to both in migration because of the strong economy and continued household formation (traditional, non-traditional);
- Greater emphasis on quality over quantity in home

space, lot sizes and neighborhood amenities;

- Demand for affordable housing and live/work spaces and;
- The desire for greater “connectedness” including access to gathering places, both public and commercial.

Overall, the combination of the market and the number of underutilized parcels in the Study Area creates a significant opportunity for residential and commercial development within the Study Area. The retail, restaurants and personal services market is less strong, but still growing. The downtown format lends itself to local and distinct retailers and restaurants that reinforce and benefit from the unique walkable character of downtown. Though there is a low vacancy rate in the area, office uses are more challenging in the Study Area and the current market demand in Parker and the Study Area for office is not growing as quickly. In order to maintain a daytime economy and ensure a sustainable economy in the Study Area, it is important that locations for office uses be preserved. Further economic analysis should be completed to inform programs, incentive and investments that will further the desired outcomes of this Plan.

3 Land Use Recommendations

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Land Use Overview

Land use is the critical foundation that serves as the building blocks of any district. Encouraging a mix of land uses in the Mainstreet Master Plan study area plays a significant role in achieving a balance and variety of residential, commercial, recreational and entertainment opportunities - critical components of any vibrant community. This downtown type mix also fosters an interrelated and interconnected area that is easily walkable, convenient and attractive creating a strong sense of place. Cities and towns are multi-faceted, living creatures and many additional layers of urban design, architectural style, events and programming will contribute to the ultimate character and sense of place, but land use is where it all begins. For this reason, land use decisions require careful planning and thoughtful consideration with an eye to long-term, even 50-year impact. Uses that do not contribute to the Town's vision will detract from it, and those decisions can endure even beyond the lifespan of a single building.

The plan area is challenged by three primary land-use related issues:

- **Appropriate Use:** An absence of active pedestrian-oriented commercial, ground-floor uses--potentially described as 'the wrong use in the right place';
- **Gaps:** A large number of physical gaps between existing buildings which disrupts the pedestrian experience — 'no use in any place'; and
- **Intensity:** A lack of development intensity - 'not enough people and jobs'

Appropriate Use

In the case of Mainstreet, appropriate use is as much a question of the floor on which a business is located as it is the actual nature of the business. In order to determine what constitutes 'the right use in the right place', it is important to understand the Town's downtown vision, as articulated in the Parker 2035 Master Plan, of a pedestrian-focused, walkable downtown. Restaurants, boutique retail and art galleries are typical examples of active pedestrian-oriented commercial first floor uses where people come and go, window-shop and observe and participate.

Borderline uses that may or may not be appropriate on the first floor include studio-type spaces (such as artist, dance or yoga studios), banking, and certain personal services. In these cases, the key determinants are the amount of frontage occupied and whether passersby can observe interior activities, hence the importance of avoiding a lengthy 'dead zone' of uninteresting, non-participatory street frontages. These uses should be pedestrian-oriented walk-up spaces that are incorporated in a larger mixed-use structure, rather than a stand-alone building. Studio spaces may have a small exterior-facing boutique within their reception or check-in area, while the active studios (visible or screened) occupy the rear of the space.

Uses that should not occupy street-level space along Mainstreet include office, professional services and residential. These uses are part of an active pedestrian downtown, but should occupy upper floors. Stairwell entries onto the main street are acceptable and can provide an opportunity for small interest displays, such as an 'available properties' board in the case of a real estate office. Although residential is a critical part of a vibrant downtown, it should also be directed to upper floors or

should face roadways that are residential in character. Ground-level residential on active pedestrian streets poses significant privacy issues for residents, who will typically keep curtains or shades drawn around the clock, creating another 'dead zone' of street frontage.

Filling In Gaps

Equally important as attracting active ground-floor uses is building an unbroken length of those uses. Mainstreet within both Old Town and the West End should offer a consistent line of buildings that leads the pedestrian along the street from business to restaurant to gallery. Street-adjacent surface parking and undeveloped properties disrupt the rhythm of the street and instead lead the pedestrian to wonder if there are additional shops and attractions further down the street or around the corner or if he or she has reached the end of 'downtown'.

Increasing Intensity

The third piece of creating an active downtown and a natural corollary of the preceding two steps is creating critical mass. Promoting multi-story development and vertical mixed use within that development is critical for creating a sustainable downtown that includes jobs and residents. This two-pronged target—employment and residential—will increase downtown vibrancy from a numbers and an hours point of view: more people enjoying the downtown during and after business hours and on the weekend.

Future Land Use

The following map 'Land Use: Corridor Vision' shows the vision for future, use within the plan area. More comprehensive detail regarding permitted, special review and prohibited uses within each of the map's categories can be found in the Town's Greater Downtown District Zoning, but the following broad definitions are used in this Plan.

Land Use Categories

• Ground Floor Overlay - Active Uses

This overlay represents one of the Master Plan's core principals, which is to reserve ground-floor, Mainstreet-abutting spaces for active uses. For the purposes of this plan, 'active uses' are narrowly defined as retail and dining establishments and certain personal services with a high level of pedestrian traffic and interest. Examples include restaurants, gift shops, coffee shops, specialty boutiques and the like. Professional and low-pedestrian-generating uses such as medical or law offices, realtor services or corporate headquarters are not included in this active ground-floor. Parking is usually provided on the parcel and may be surface or structured. These types of small-scale uses may include dedicated parking on their parcel, but often have reduced parking requirements and may rely heavily on public parking, whether on-street or in public lots and structures.

• Multi-Family

This category covers a broad variety of dwelling units with shared architectural elements. Although duplexes or townhomes can be considered multi-family, this plan recommends denser products such as apartment, condominiums, stacked flats, assisted and senior living facilities, lofts and similar products. This type of development typically requires some amount of aggregate, communal open space, and parking may be surface or structured. Hotels, motels and other lodging facilities are not included in this category.

• Mixed-Use - No Residential

This category covers a wide range of uses, including but not limited to retail, office, commercial, civic, educational, institutional and entertainment uses. In keeping with the *Parker 2015 Master Plan* central tenet of creating a pedestrian-friendly downtown, however, these uses should be scaled to the urban context. Retail uses are acceptable, but large-format 'big box' stores are not. Due to the context-specific conditions of the areas designated in this category, residential uses are specifically excluded from these areas for visual and noise reasons. Parking may be surface or structured within the parcel, or off-parcel in a public structure. For parcels that are assembled into a larger, multi-building development, on-site structured parking is preferred and a shared public/private parking facility is an option.

• Mixed-Use

This is a broad category that allows many of the same uses found in other categories. Retail, office, commercial, civic, educational, institutional, entertainment and even residential are all acceptable in this category. In the broadest discussion of mixed use, uses may be mixed horizontally (occupying different spaces on the same floor) or vertically (such as retail below and office above). Within the Plan Area, residential uses (multi family/high density) are restricted to upper floors along Mainstreet, but may be on any floor on non-Mainstreet properties. Parking may be contained on the parcel or within a public or shared facility.

• Civic/Institutional

These types of uses represent some sort of public service or interest, and are often exempt from property taxes. Examples of this type of use include churches, libraries, government offices, hospitals and cemeteries. Parking may be on-parcel or provided off parcel in a public facility.

• Entertainment/Culture

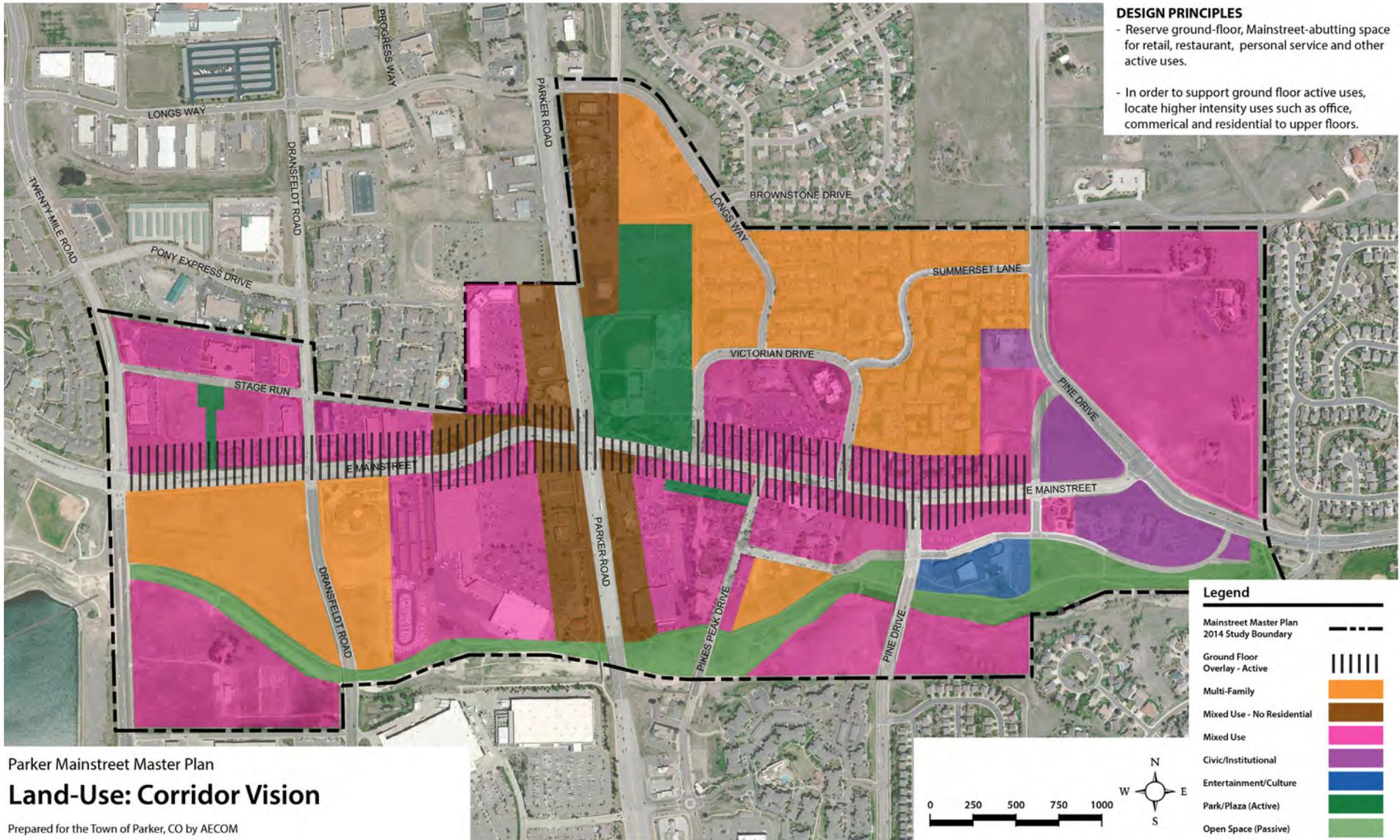
This category of uses includes public and private facilities dedicated to arts and entertainment. Examples include theaters (live or movies), museums and bowling alleys. Parking may be on-parcel or provided off parcel in a public facility.

• Park/Plaza (Active)

Areas within this category may be a grassed 'softscape' space such as a park or soccer fields, or may be a 'hardscape' area such as a public plaza. Parks and plazas may be publicly or privately owned, but should be open to the public regardless of ownership. These uses may or may not provide parking; on-site parking is more common with spaces with formal, active programming such as athletic fields or facilities.

• Open Space (Passive)

This type of space describes areas that are typically more naturalized in appearance, are often conservation areas, and have few formally programmed activities. Open space may include trails, pathways, interpretive signage or even playgrounds, but will not include intensive uses such as athletic fields or concert/gathering spaces.



Parker Mainstreet Master Plan
Land-Use: Corridor Vision

Prepared for the Town of Parker, CO by AECOM
Figure 14 Land-Use: Corridor Vision

Future development and tenancing should focus on active, ground-floor uses that promote an interesting, pedestrian-focused downtown. Office and residential uses should be directed to upper floors (on Mainstreet) and side streets.



'Active uses' such as boutique shops, bakeries, cafes and restaurants invite window shopping and lingering, and add motion and vitality to the downtown streetscape.

Active Uses

To achieve a vibrant and walkable downtown destination, future development and tenancing should promote retail, restaurant, certain personal services and other active ground floor uses in the 'strip' facing both sides of Mainstreet, with a particular emphasis on creative-industry uses such as art galleries, music stores and unique shops. The 'strip' is essentially the one-parcel-or-one-building-deep area directly abutting Mainstreet. Within this area office, professional, residential and non-active uses should be directed to upper floors. The rear of parcels or parcels not directly abutting Mainstreet can be more flexible and may be occupied by any uses allowed by zoning.

A very specific type of active use that is gaining in popularity across the nation is the indoor farmers' markets and multi-tenant artisan markets. Examples include the West Side Market in Cleveland, the Ferry Building Marketplace in San Francisco and locally, Willow - An Artisan Market located in the heart of historic downtown Littleton since 2004 and The Source in Denver. Tenancing models vary, with some markets having permanent tenants, temporary kiosk-type tenants, or a combination of both. Market studies

of these types of enterprises suggest that Parker does not yet have the population density or demographic that forms the target market, but may become a viable opportunity as downtown development intensity increases and demographics shift.

Recommendation 3.1: *Promote active ground-floor uses of Mainstreet-abutting building to only retail, restaurants and other active uses.*

Recommendation 3.2: *Encourage creative-industry uses such as cafes, art galleries, music stores and unique shops.*

Recommendation 3.3: *(aspirational): Promote an opportunity for an artisan/'makers' market in Old Town.*

Recommendation 3.4: *Seek opportunities for pulling active ground floor uses 'around the corner' at Mainstreet intersections, especially at Pikes Peak Drive.*

Residential

A central goal of the Master Plan is to increase residential density within the downtown area: more people means more activity, more customers for shops and restaurants and ultimately, more shops and restaurants and a more vital, attractive, economically viable and inviting downtown for residents and visitors alike. Residential uses are encouraged as upper-level uses throughout the plan area, and can be expanded to include the ground level in any location not abutting Mainstreet.

The existing Town and Country townhomes do front Mainstreet within the Old Town portion of the Plan Area, across from the PACE Center. These uses may stay for an indefinite amount of time, but should be transitioned to denser, vertically mixed buildings with active ground floor use in the long term. In the short term, certain urban design enhancements can be used to create a more

integrated, urban edge; these options are illustrated in the Urban Form chapter of this document.

Recommendation 3.5: *Promote increased residential uses throughout the plan area, with a focus on higher-density multi-family products.*

Recommendation 3.6: *Limit development of ground-floor residential to side and secondary streets.*

Recommendation 3.7: *Redevelop existing Town and Country Townhomes adjacent to Mainstreet to higher-density, vertically mixed use development in the long-term.*

Transit-Oriented Development (TOD)

Transit oriented development, or TOD, is a type of community development that includes a mix of higher density housing, office, retail and other amenities that are integrated into a walkable neighborhood and located near transit. Today transit service in Parker is limited. This is partially due to the lack of intensity of land uses in Town, including the Plan area. The Parker Park-n-Ride on the north end of the Old Town area functions as the Town's local transit hub. The Park-n-Ride and the surrounding areas should be redeveloped to increase the residential density and the intensity of other land uses to provide future residents and visitors convenient, affordable and easy access to transit.

Recommendation 3.8: *Encourage redevelopment of the Parker Park-n-Ride with increased density and intensity of land uses to create a transit oriented development.*

Office and Educational/ Institutional

A solid daytime economy is critical to the support of any downtown, but this 9-5 user population is largely absent from the plan area. A medium-density office node is a typical example of this type of use, and a junior college or vocational school with an integrated, urban-style campus could also be a strong generator for Downtown Parker. Downtown's biggest barrier to increasing downtown use is the small size of existing office and commercial spaces; particularly abutting Mainstreet, these spaces tend to be quite limited in size, even if adjacent suites are combined, and do not provide the square footage necessary for offices with more than 10-20 staff members or institutions needing to serve a larger population. However, currently several redevelopment opportunities exist at prime infill parcels in the downtown area for additional/larger office space to fill this need and the need/demand for additional ground-floor retail space.

The Crossroads Shopping Center, at the northwest corner of Mainstreet and Parker Road and currently a single story retail center, offers a unique opportunity for redevelopment for a mix of uses including larger floor plate office or educational buildings. The location offers high visibility adjacent to Parker Road, and can include active ground floor uses along both its Mainstreet and Parker Road frontages.

In keeping with the Town's *Creative District Plan*, business recruitment should place a particular emphasis on and evaluate potential incentives to attract creative industry tenants such as architects, designers and small-scale publishing. These tenants expand the base of downtown businesses, and offer an additional layer of interest and creativity to local programming and events. The Greater Downtown District Zoning and the Land Development Ordinance should be evaluated and amended as needed to accommodate the more diversified and pedestrian-focused mix of uses described in this plan.

Recommendation 3.9: Continue to promote office as an upper-floor use throughout the Plan area; direct development of ground-floor office uses to side and secondary streets.

Recommendation 3.10: Diversify the inventory of office space to include larger floor-plates and more flexible configurations.

Recommendation 3.11: Actively pursue higher-density office employment areas and educational/institutional uses that support a daytime economy.

Recommendation 3.12: Encourage creative industry tenants such as architects, artists, designers, publishing, software and marketing to locate to the Plan area.

Recommendation 3.13: Update existing planning and development policies in order to attract viable office/commercial business and sustainable retail activities.

Recommendation 3.14: Develop regulatory and other incentive tools that encourage development and redevelopment consistent with recommendations in this Plan as well as the Parker Transportation Master Plan and the Parker 2035 - Master Plan.



Image: parker-station.com/leasing.php



Image: parkercolorado.net

Parks & Public Spaces

EastMain / New Library & Town Civic

Centre Park and Plaza

Parker loves festivals and most of these events are held - at least in part - in O'Brien Park. Stakeholder and public feedback indicate an appetite for more varied events, and event organizers indicate that existing events could expand if additional appropriate space were available. The Town should contemplate a more urban-style park or plaza design but retain the current historic and memorializing park themes.

The Town is planning a significant new park and plaza that will come on-line in tandem with the opening of the new Parker Library. This new space will be directly west of the library building itself, abutting the PACE Center Drive extension between Mainstreet and Pine Drive, and will be an active, four seasons park including a softscape lawn area with band shell, a hard surface plaza, splash pad water feature, ice skating, and other amenities. In order to fully capitalize on the opportunity presented by the Town's significant investment in this park, and ease the time and energy involved in set-up and management of future events, the Town should carefully consider the intended programming, configuration, and amenities of this park and plaza.

The park and plaza spaces should include readily available power, audio-visual hookup, and most importantly a flexible configuration. Consideration should be given to acoustics as they relate to the band shell at the northern end of the park.

Recommendation 3.15: Design the Civic Centre new park and plaza as a special event-ready venue.

PACE Center Plaza

The Town should also consider the design and special event potential of a small public plaza space on the PACE Center property. This plaza should be integrated into the future development to be located on PACE lot 2 between the PACE Center and Mainstreet. This smaller space should be designed to accommodate PACE-related programming and should also include a flexible layout and full audio-visual hookup. This space can be a plaza or a small planted park, but in either case should not appear to be 'empty' when not in use for an event. Depending upon adjacent development, this space could double as flexible outdoor art display or dining space for a neighboring retail or restaurant use.

Recommendation 3.16: Integrate a plaza space into future development of PACE Lot 2 and display art, as an intimate outdoor programming venue.

20 Mile Historic Park and Living Wheel Park Redesign

These two highly underutilized parks, comprising almost two acres and 430 feet of prime Mainstreet frontage, create a significant 'dead zone' in what should be one of the West End's most vibrant areas. Although the shallow depth and noise from Mainstreet would make 20 Mile Historic Park a difficult programming venue, Living Wheel Park does offer a larger size and greater depth. These characteristics could be used for small-scale event programming if a more open and flexible design were introduced. Both parks should focus on creating a more active, engaging space as well as an interesting street frontage that continues the energy of Mainstreet and encourages visitors to continue along the corridor. A new park design should create a sufficiently interesting interior (and shade) that could also be used for casual dining, reading or other passive activities such as a veterans memorial or monument. This space can be a plaza or small urban style park and should include outdoor public art; this space should not appear to be 'empty' when not in use for an event.

Recommendation 3.17: Redesign the existing 20 Mile and Living Wheel Parks to promote activity within the parks and create a pedestrian-friendly street edge.



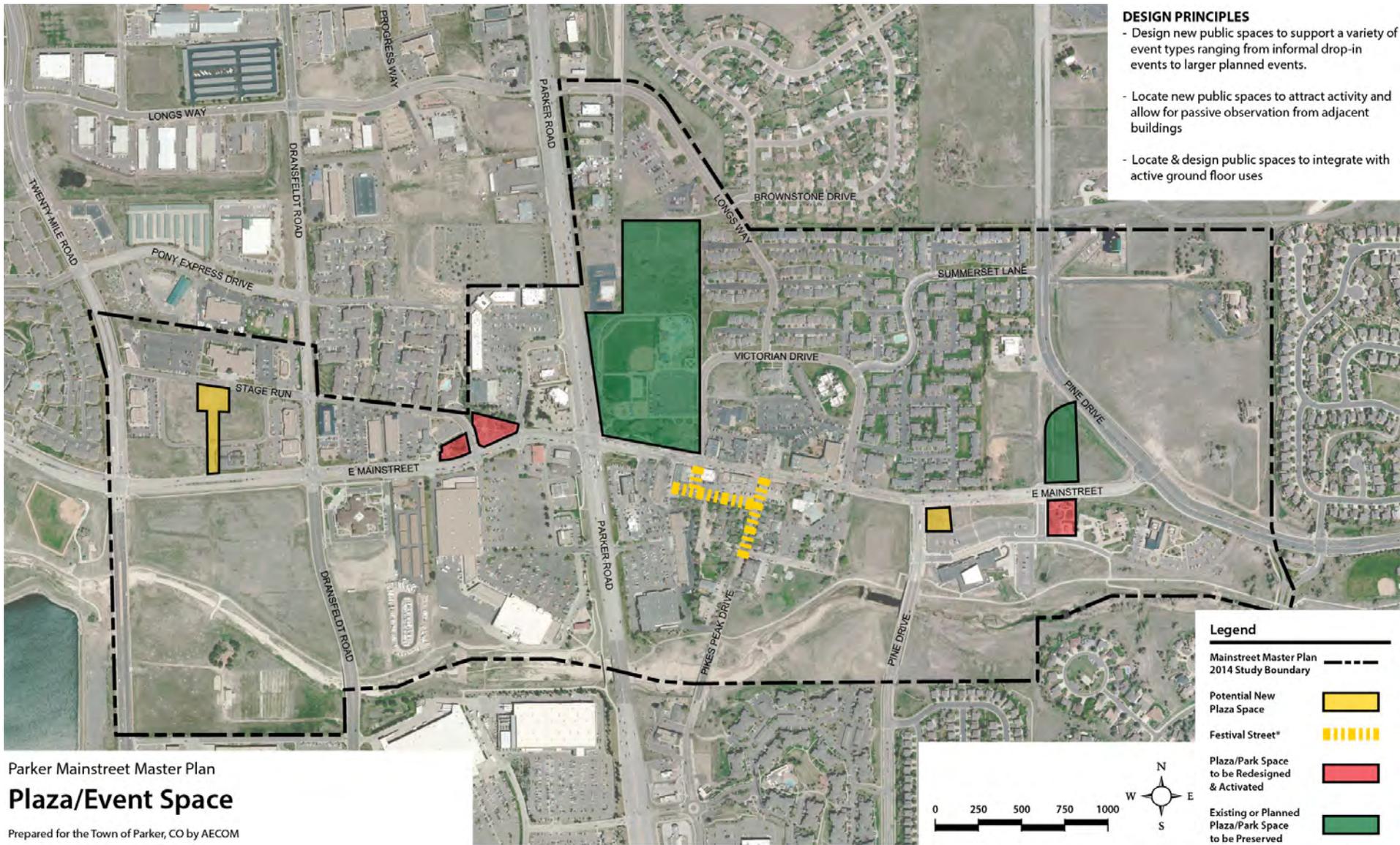


Figure 15 Plaza/Event Space

Parker loves festivals, and therefore, future investments in parks and public spaces should focus on enhancing and expanding the corridor's public event spaces.

Stage Run Plaza

Proposed land use also recommends a plaza or combination plaza/park on the west side of Parker Road, in the 'U' parcel abutting Stage Run. The park may be publicly or privately funded, or a combination of both, but is likely to be constructed in tandem with development (or redevelopment) of adjacent parcels. Regardless of funding and timing, this important new space should be carefully designed as both a central 'node' and a north-south pedestrian connection between Mainstreet and the existing movie theater. The plaza should be designed to cater to a diverse set of active and passive activities as well as designed as an aesthetically pleasing community amenity that serves a variety of users including building tenants, visitors and the public.

The new park/plaza should be configured to allow double-faced development to the east, west and south, with direct frontage on Stage Run. This space will create a more formalized public open space to draw residents of nearby neighborhoods to the Mainstreet Corridor. The space should be designed with particular attention to a permeable edge, in order to promote outdoor dining and other pedestrian-friendly uses at its perimeter. Similar to the new park and plaza adjacent to the library, the space should be flexible and configured to allow events and festivals on this side of the plan area. Further design guidance on configuration of this space is included in the Urban Form section of this document.



Recommendation 3.18: *Construct a new public open space abutting Stage Run, in tandem with development.*

Bar CCC Park

Bar CCC Park occupies a prime location at the downtown's western gateway and adjacent to the Cherry Creek open space corridor. Currently dominated by a baseball diamond, the park should be considered for redesign in the future when currently-planned expansion of the Town's athletic fields can remove the need for this park's field. Redesigned as an active or passive park, Bar CCC could provide much-needed open space for the intensified residential development currently under construction in the West End. Lower than adjacent roadways, the park's current grading would require evaluation prior to moving forward with redesign.

Similar to the challenges Old Town experiences with special events, the West End will also need to identify parking strategies for events in the new Stage Run Plaza. Shared or temporary parking agreements with private owners, particularly the existing movie theater, are one tool that can be explored. A larger, more readily available reservoir of parking can be found immediately south of the Bar CCC Park; this gravel parking area is used for open space and baseball parking, and can park up to 180 cars (3 rows, 2 aisles) with its current



image: parkerrec.com

configuration. If widened to 120 feet or valet-parked, the area could potentially accommodate an additional 60 cars (240 cars total). Depending upon where they parked within the lot, visitors could expect a five to ten minute walk from their car to the Stage Run plaza. This parking location aligns with recommendations elsewhere in this document of focusing public parking reservoirs at the edges of the plan area - this strategy reduces the number of cars within the Old Town and West End cores, and promotes a pedestrian-focused environment.

In 2009, the Town completed a design for parking lot improvements at Bar CCC. Due to the economy and funding constraints at the time, Town Council prudently deferred the project to an undetermined future time.

Recommendation 3.19: *Improve the gravel parking area south of Bar CCC Park, on the west side of 20 Mile Road to act as a public parking reservoir for the West End.*

Recommendation 3.20: *Evaluate the potential for the future conversion of Bar CCC Park into an active or passive, but unprogrammed, park.*

O'Brien Park Expansion

The Town currently owns 5 acres of vacant land north of O'Brien Park. This land was purchased in 2005 with Town and Douglas County open space funds for the purposes of expanding the existing park. The site's location and views provide a number of opportunities to expand the current amenities at O'Brien Park and create new and unique park experiences that will continue to draw residents and visitors alike to downtown.

Recommendation 3.21: *Complete an O'Brien Park expansion master plan to establish a comprehensive vision and design for improvements to the park and expansion area.*

Additional Uses Evaluated and Discarded

A number of additional potential uses and projects emerged from staff and stakeholder discussions. These opportunities were evaluated for economic and market feasibility but deemed untenable for a variety of reasons. These projects are noted here for future reference should context change and the uses be reconsidered.

Entertainment Node

The concept of an entertainment node at the west end of the study area gained some interest from stakeholders. This idea builds on the energy of the existing 10-screen movie theater and looks to add additional all-ages activities such as bowling, arcade, indoor golf, laser tag and similar amusements. Market analysis identified three significant issues with this concept.

First, this type of development is demand-driven and privately built; it is not the type of use that would be funded or run by the Town. As such, it is strictly market-driven and not controlled by the Town except through favorable zoning or incentives. Second, the described uses require a large amount of square footage, typically configured in one-story structures with few windows, and do not lend themselves to

multiple-stop visits; patrons typically select a single activity as opposed to taking in a movie, then some bowling followed by a round of golf. Form and massing combined with use characteristics make this type of entertainment node fall short of the level of pedestrian-friendliness that is desired for the plan area.

Third, entertainment as a land use is in transition and the potential for new 'fun centers' is extremely limited. Similarly, with construction of new movie theaters stagnant, and format moving towards fewer screens when it does take place, relying upon the existing AMC movie theater as a long-term anchor to a new entertainment district is a very unstable prospect.

Indoor Event Space

The popularity of Parker's outdoor festivals and events suggested potential opportunity for an indoor event space that could expand the Town's calendar to include year-round events. Conversations with event organizers indicated that the closest place capable of hosting events such as holiday gift fairs or antique shows is the Wildlife Experience, approximately 5.5 miles northwest on Lincoln Avenue. The most significant issue with this proposal, however, is the 'down time' of such a facility; when not in use, it's just a big empty building with dead street frontage. As such, it is not an appropriate use within the plan area.



Market analysis showed national trends moving away from entertainment nodes, while indoor event facilities were deemed inappropriate because of their internal focus and poor street interaction.

Opportunities to Realign Development with Land Use Vision

Considering the Land Use Vision described in the preceding pages, the following map identifies existing uses that are inconsistent with the Town's vision and goals for the plan area. Undeveloped parcels and street-abutting surface parking lots are not included in this assessment, but should also be considered as redevelopment opportunities for the reasons described previously.

In order to respect private property rights but promote future redevelopment consistent with the Mainstreet Master Plan vision as well as the Parker 2035 Master Plan, the Town should continue to grandfather existing uses and structures but prohibit redevelopment of the same nature. The Town should evaluate whether changes to existing standards and guidelines, a new overlay district, or some other policy tool are most appropriate. In addition, the Town should continue to evaluate opportunities for appropriate land holding as parcels become available for purchase and should identify a variety of public incentives that could be used to support vision-aligned redevelopment.

Recommendation 3.22: *Modify existing policy to guide redevelopment of parcels that are inconsistent with the land use vision.*

Recommendation 3.23: *Seek opportunities and utilize incentives to support property development and redevelopment that is consistent with the Plan's vision.'*

West End

Site 1, Bank with Drive-Thru (one floor)

This parcel's use and orientation is incompatible with both a pedestrian-oriented environment and the master plan's vision for active retail, restaurant and personal services on Mainstreet-abutting parcels. If the building is re-used, the drive-thru should be removed and primary and secondary entrances should be oriented to Mainstreet and the unnamed Stage Run loop road. If the site is redeveloped, a new building should have smaller setbacks from Mainstreet and the drive to the west of the site. Architecture and site planning should emphasize the entrance into the development, create active uses on the first floor and create a primary entrance from Mainstreet.

Site 2, Dental Office (one-floor)

Ground-level professional services, or the approximate 75 foot setback, do not align with a pedestrian-oriented environment. If the building is re-used, the primary and secondary entrances should be oriented to Stage Run and Dransfeldt Road. If the site is redeveloped, a new building should have smaller setbacks from Stage Run and Dransfeldt Road. Architecture and site planning should emphasize the entrance into the development and create active uses on the first floor.

Site 3, Bank with Drive-Thru (one floor)

As noted with Site 1, both stand-alone banking and drive-thru facilities are incompatible with a pedestrian environment. This parcel is also impacted by roadway re-alignments suggested elsewhere in this document.

Site 4, Twenty Mile Historic Park

This park is deed-restricted and as such, would be difficult to convert to a developable parcel. As discussed more fully in this Chapter's 'public spaces' section, however, the park should be redesigned to better support open space needs for the residents in the West end, with a particular focus on creating an active street edge.

Site 6, Mini Storage (one floor)

This storage facility is by its nature an auto-oriented use generating no foot traffic and housed in a windowless, industrial building materials with a residential style cedar fence requiring a 'dogleg' in the public sidewalk. Both the use and physical improvements of this site are unwelcoming to pedestrians. The site's narrow width, significant depth, single access and utility constraints along the Mainstreet frontage pose significant redevelopment challenges, suggesting that a flexible 'mixed-use' vision is required in order to ensure that the site converts to a more pedestrian-friendly use of any type. Regardless of use, however, any new structure should be designed with a primary façade on Mainstreet; retail use in this Mainstreet-facing portion of the site would be ideal.



Parker Mainstreet Master Plan

Opportunities to Realign Development with Master Plan Vision

Prepared for the Town of Parker, CO by AECOM

Figure 16 Opportunities to Realign Development with Master Plan Vision

The parcels identified in the map above do not align with the Town’s pedestrian-focused vision for the downtown area.

Mainstreet/Parker Road Intersection (Northwest, Southwest and Southeast corners)

Site 5, Living Wheel Park, Old Library site, Bank with Drive-Thru and Single-Story Strip Retail (Northwest Corner)

This segment of Mainstreet has a three block, 840-foot segment of inactive street frontage created by two underutilized parks (Living Wheel and 20 Mile Historic Parks), two roadway intersections and a large surface parking lot belonging to the bank on the Mainstreet/Parker Road corner. The plan recommends that all of these parcels, together with the old library site and the low-density Crossroads Shopping Center, be aggregated into a single development site anchored by a redesigned Living Wheel Park that is configured to integrate with new, adjacent mixed-use development. This aggregation offers the opportunity for the creation of a significant employment node or educational/institutional use, in order to increase daytime population and increased development intensity and activity in the Plan Area. Urban design principles for this redevelopment are illustrated in the Urban Form chapter of this document.

Site 7, Big-Box Retail (Southwest Corner)

Form and massing represent the most significant issues on this site, with a fenced storage yard and a vast surface parking area immediately abutting Mainstreet and Parker Road. Future redevelopment should exhibit a zero setback with primary entrances facing Mainstreet. Vertical architecture should replace surface parking, although a maximum of two drive access points into the site would be acceptable. All parking should be located on the interior, behind (south of) new development.

Redevelopment on the southwest corner of Mainstreet and Parker Road must provide scale, street enclosure to the public realm, and a sense of pedestrian safety in order to promote a unified Mainstreet on both sides of Parker Road. As previously noted, traffic volumes and resulting noise make this a challenging site for any land use and future development should focus on building form and massing more than use. A new building should have minimal setback from both roads, with parking under or behind (south of) the building. Primary entrances should face Mainstreet; if the building is parked behind the structure, a north and south entrance would be appropriate, as long as the north entrance is still 'primary'. In this case, the building may have two equal or 'primary' entrances.

Finally, redevelopment on the southern portion of this site should pay special attention to the adjacent Sulphur Gulch open space corridor with a focus on creating a strong relationship and pedestrian/bicycle connection between Sulphur Gulch and Mainstreet. Ground floor uses on this site are recommended as office, commercial and active uses.

Site 10, Bank with Drive-Thru (two floors), Fast Food Restaurant and Retail (Southeast Corner)

All notes regarding Sites 1 and 3 apply to this site. This site does pose additional difficulties due to permanent utility boxes in the amenity zone at this prominent and highly visible corner; particular attention should be paid to streetscape in this area in order to minimize the visual impact of these boxes. Redevelopment should locate a new building directly behind the Mainstreet sidewalk, with all parking under or behind (south of) the structure.

Old Town

Site 8, Child Care and School (one floor)

Both the use and the massing of this building are inconsistent with the desired pedestrian orientation of Old Town. Stakeholder input has highlighted difficulties created by the school's traffic pattern of concentrated morning drop-off and afternoon pickup. In addition, surface parking between the structure and the sidewalk contribute nothing to pedestrian interest. This void will become a larger liability as other recommendations in this document regarding enhanced pedestrian circulation through Kieffer's Crossing are put into place. Redevelopment should focus on a zero- to minimal-setback, with active, pedestrian-focused ground floor uses. This location should continue the pedestrian experience and include residential or office uses on the upper floors.

Site 9, 19801 E. Mainstreet – Former Water & Sanitation building (one floor)

This site's building has a pedestrian-unfriendly setback with surface parking between sidewalk and building. Redevelopment should focus on a two- to three-story structure in keeping with existing Old Town heights and a zero- to minimal-setback. New development should include active, pedestrian-focused ground floor uses and residential or office uses on the upper floors. The Town recently issued a Request for Qualification (RFQ) for redevelopment of this parcel.

Site 11, Hess Exchange building (two floors)

The building itself aligns well with the form, massing and materials desired in Old Town, even including a special corner feature that addresses the important Mainstreet/Pike Peak Drive intersection. Existing professional and office uses feature near-permanently closed window shades underlining the need for pedestrian-friendly retail and dining at street level. Existing office uses should be relocated to the second floor or to off-Mainstreet locations to make way for desired active ground floor uses.

The exception to the preceding evaluation is the rear drive-thru structure which is inconsistent with the vision for both Mainstreet and Pikes Peak Drive. Upgrading the drive-thru to create an occupiable space would require HVAC and safety improvements likely to make it just as expensive as building a new structure, with potentially questionable aesthetics. For this reason, the drive-thru should be removed and replaced with a new building extension or stand-alone building that brings Mainstreet retail around the corner and onto Pikes Peak Drive.

Site 12, Auto Repair (one floor)

While building location is acceptable, the auto-oriented use of this parcel is inconsistent with a pedestrian downtown. Future redevelopment may consider remodel of the existing structure with larger windows and potential vertical expansion. Use should transition to restaurant, retail or creative businesses. Like Site 11, it is desirable that Mainstreet retail and restaurant uses 'wrap' the corner onto Pikes Peak Drive and extend as far as Pilgrim's Place. Future redevelopment of this parcel should focus on a two- to three-story structure in keeping with existing Old Town heights and a zero- to minimal-setback.

Site 13, Old Town Hall (two floors)

Site layout and use do not contribute to a pedestrian downtown or the creative/civic orientation of the Mainstreet/PACE Center node. Site redevelopment should take care to create primary, high-quality facades on both Mainstreet and PACE Center Drive, provide consistent and unbroken sidewalk connections and crossings to and from (new) Town Hall and should seek a use or uses that complement adjacent civic or creative facilities.

Site 14, Parker Park-n-Ride

The existing Park-n-Ride is a central transit location within the Town. The site itself developed as a free standing parking lot should be redeveloped into a transit oriented development (TOD) including structured parking, multifamily residential and potentially first floor commercial. This TOD has the opportunity to include nearby properties that could be redeveloped with a higher intensity of uses.

Priority Parcels

West End

Unlike Old Town, there are no existing well-defined activity nodes west of Parker Road. There are, however, two areas that offer opportunity to begin transitioning and enhancing this segment of the corridor and downtown as a whole.

Old Library/Crossroads/Living Wheel

As discussed earlier in this chapter, increasing daytime population is a top priority to support an economic revitalization of the corridor. If aggregated into a larger unified development, this group of parcels offers opportunity for larger-scale office or higher-education uses not possible elsewhere within the Plan Area. Development assistance and incentives should be explored for this area.

Recommendation 3.24: Promote redevelopment of the NW corner of the Mainstreet/Parker Road intersection.

Stage Run

This parcel is a priority within the West End area but should not detract from the higher-priority areas within Old Town or the more comprehensive priority of creating an office node. As described in the overarching priority comments, this area should be guided toward more pedestrian-friendly development in the short-term with an eye to creating patterns for long-term redevelopment.

Recommendation 3.25: Actively promote development of the Stage Run Center parcel, south of the existing movie theater.

Old Town

Existing and Developing Strengths

The Town's first priority should be to coalesce energy at Old Town's '100-percent corner' of Mainstreet and Pikes Peak Drive. This intersection is the very center of existing retail and restaurant development and is a critical decision-making and wayfinding location. The three most critical parcels for creating an unbroken 'heart' of Old Town are:

- The surface parking lot on the northeast corner
- The undeveloped 'grassy knoll' parcel on the northeast corner and;
- The existing auto repair parcel on the southeast corner

Recommendation 3.26: Capitalize on existing strength of the Mainstreet/Pikes Peak Drive intersection.

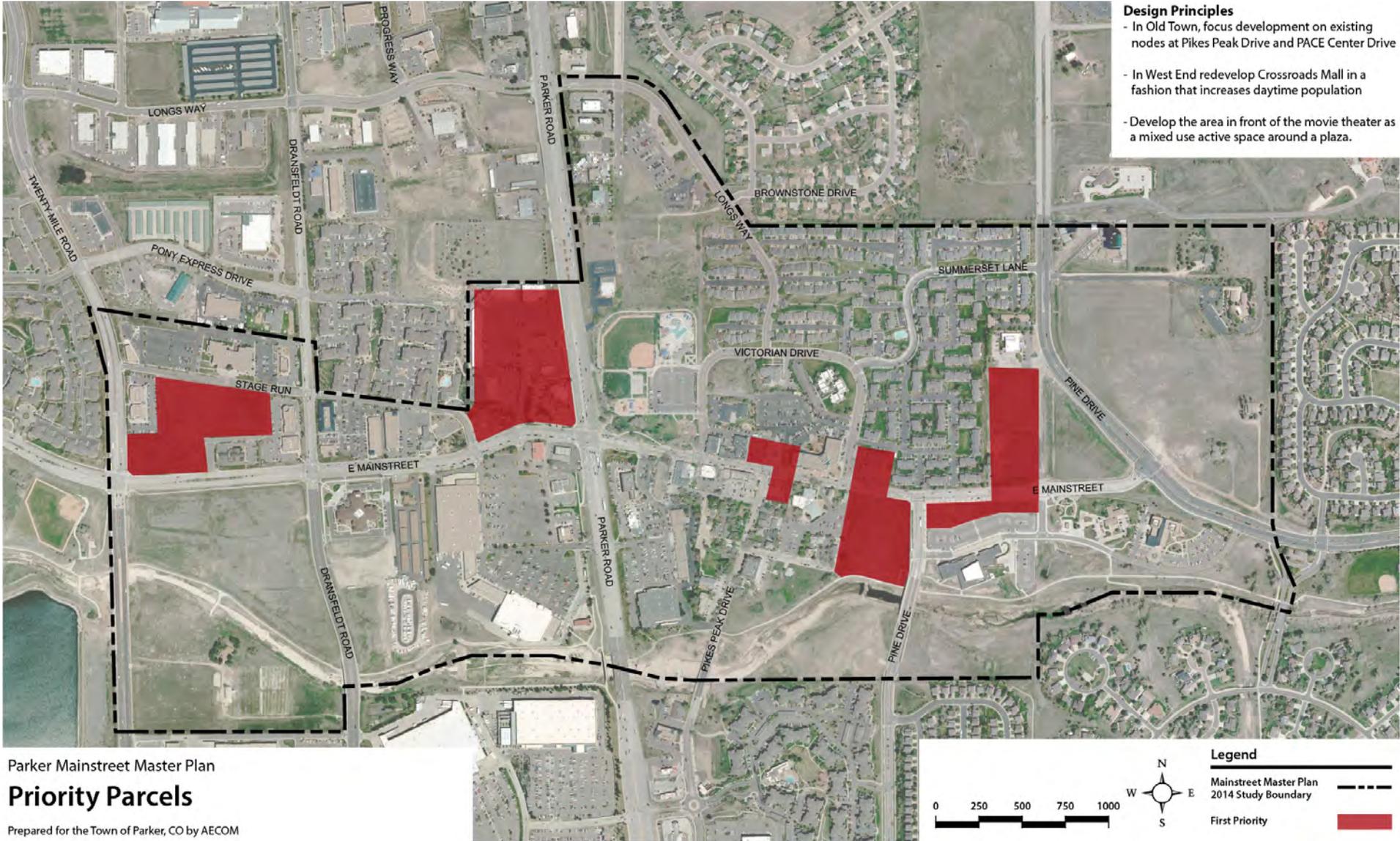
Moving east, the construction of the new Douglas County library and adjacent outdoor park/plaza offer an opportunity to solidify this part of Old Town as a true activity node. Filling in the northwest and southwest corners of the Mainstreet/PACE Center Drive intersection will be critical to connecting the energy of these two public facilities and in reducing the perceived distance between them.

Recommendation 3.27: Capitalize on current redevelopment at the Mainstreet/PACE Center Drive intersection.

Connecting Corridor

The Town's second priority should be the parcels connecting these two areas of existing and emerging strength, between Victorian Drive (east) and Pine Drive; development in this area will be important in connecting the two nodes into a continuous corridor. It is possible that properties between these two nodes may develop or redevelop before the nodes themselves are fully 'done'. In these cases, the Town should pay special attention to uses, architecture and pedestrian character that ties Old Town together.

Recommendation 3.28: Promote unity and cohesion along the entire length of Mainstreet, regardless of development phasing.



Parker Mainstreet Master Plan
Priority Parcels

Prepared for the Town of Parker, CO by AECOM

Figure 17 Priority Parcels

Redevelopment efforts should pay special attention to priority parcels that have the potential to impact Mainstreet character and/or promote further private investment.

Putting it Together

Working toward Our Goals

The chart below shows a complete list of the land use recommendations contained in this chapter, and shows how each recommendations ties with the goals established for the project.

PA: Plan Area WE: West End OT: Old Town

			Plan Area Goals										
			P-1	P-2	P-3	P-4	P-5	P-6	P-7	P-8	P-9	P-10	P-11
Plan Order	Plan Area	RECOMMENDATION	Attract Desirable Uses	Solicit Public Support	Foster partnerships.	Build on Existing Assets	Create cohesive + distinctive character	Year-Round, 24/7 Activity	Create gateways	Emphasize quality design.	Promote multi-modality	Enhance pedestrian safety	Enhance ped/bike connexns
Urban Form Recommendations													
Future Use													
3.1	PA	Promote active ground-floor uses of Mainstreet-abutting building to only retail, restaurants and other active uses.	X		X	X	X	X					
3.2	PA	Encourage creative-industry uses such as cafes, art galleries, music stores and unique shops	X		X	X	X	X					
3.3	OT	(Aspirational): Promote an opportunity for an artisan/'makers' market in Old Town	X	X			X	X					
3.4	PA	Seek opportunities for pulling active ground floor uses 'around the corner' at Mainstreet intersections.	X				X			X			
3.5	PA	Promote increased residential uses throughout the plan area, with a focus on higher-density multi-family products.	X					X					
3.6	PA	Limit development of ground-floor residential to side and secondary streets.	X										
3.7	OT	Redevelop existing Town and Country Townhomes adjacent to Mainstreet to higher-density, vertically mixed use development in the long-term.	X				X	X		X			
3.8	OT	Encourage redevelopment of the Parker Park-n-Ride with increased density and intensity of land uses to create a transit oriented development.	X		X	X					X		
3.9	PA	Continue to promote office as an upper-floor use throughout the Plan area; direct development of ground-floor office uses to side and secondary streets.	X										
3.10	PA	Diversify the inventory of office space to include larger floor-plates and more flexible configurations.	X		X			X					
3.11	PA	Actively pursue higher density office employment areas and educational/institutional uses that support a daytime economy.	X		X			X					
3.12	PA	Encourage creative industry tenants such as architects, artists, designers, publishing, software and marketing to locate to Plan area.	X	X	X		X	X					
3.13	PA	Update existing planning and development policies in order to attract viable office/commercial business and activities.	X	X									

Table 1 Land Use Recommendations

West End Goals							Old Town Goals							
W-1	W-2	W-3	W-4	W-5	W-6	W-7	O-1	O-2	O-3	O-4	O-5	O-6	O-7	O-8
Establish West End character	Foster partnership	Create Pedestrian Amenities	Provide new open space	Preserve BRT potential	Improve land use mix	Promote active ground floor uses	Improve land use mix	Promote active ground floor uses	Preserve historic character	Foster partnership	Promote multi-modality and events	Build on existing assets	Provide parallel E-W routes	Manage parking
X					X	X	X	X				X		
X					X	X	X	X				X		
							X	X						
X							X							
							X							
					X	X	X	X						
							X	X						
										X				
					X	X	X	X						
	X				X		X			X				
	X				X		X			X				
X	X				X		X			X		X		
	X				X									

Table 1 Land Use Recommendations

Plan Order	Plan Area	RECOMMENDATION	Plan Area Goals											
			P-1	P-2	P-3	P-4	P-5	P-6	P-7	P-8	P-9	P-10	P-11	
			Attract Desirable Uses	Solicit Public Support	Foster partnerships.	Build on Existing Assets	Create cohesive + distinctive character	Year-Round, 24/7 Activity	Create gateways	Emphasize quality design.	Promote multi-modality	Enhance pedestrian safety	Enhance ped/bike connexns	
3.14	PA	Develop regulatory and other incentive tools that encourage development and redevelopment consistent with recommendations in this Plan as well as the Parker Transportation Master Plan and the Parker 2035 - Master Plan	X			X	X	X	X	X	X	X	X	X
3.15	OT	Design the Civic Centre new park and plaza as a special event-ready venue.					X	X		X				
3.16	PA	Integrate a plaza space into future development and display art as an intimate outdoor programming venue.					X	X		X				
3.17	WE	Redesign the existing 20 Mile and Living Wheel Parks to promote activity within the parks and create a pedestrian-friendly street edge.				X	X	X		X				
3.18	WE	Construct a new public open space abutting Stage Run, in tandem with development.					X	X		X				
3.19	WE	Improve the gravel parking area south of Bar CCC Park, on the west side of 20 Mile Road to act as a public parking reservoir for the West End.				X								
3.20	WE	Evaluate the potential for the future conversion of Bar CCC Park into an active or passive, but unprogrammed, park.				X	X	X	X					
3.21	OT	Complete an O'Brien Park expansion master plan to establish a comprehensive vision and design for improvements to the park and expansion area.				X		X		X				
Opportunities to Realign with Land Use Vision														
3.22	PA	Modify existing policy to guide redevelopment of parcels that are inconsistent with the land use vision.	X	X	X					X				
3.23	PA	Seek opportunities and utilize incentives to support property development and redevelopment that is consistent with the Plan's vision.	X		X									
Priority Parcels														
3.24	WE	Promote redevelopment of the NW corner of the Mainstreet/Parker Road intersection.	X		X		X		X	X				
3.25	WE	Actively promote development of the Stage Run Center parcel, south of the existing movie theater.										X	X	
3.26	OT	Capitalize on existing strength of the Mainstreet/Pikes Peak Drive intersection.	X		X	X	X			X				

Table 1 Land Use Recommendations

West End Goals							Old Town Goals							
W-1	W-2	W-3	W-4	W-5	W-6	W-7	O-1	O-2	O-3	O-4	O-5	O-6	O-7	O-8
Establish West End character	Foster partnership	Create Pedestrian Amenities	Provide new open space	Preserve BRT potential	Improve land use mix	Promote active ground floor uses	Improve land use mix	Promote active ground floor uses	Preserve historic character	Foster partnership	Promote multi-modality and events	Build on existing assets	Provide parallel E-W routes	Manage parking
X		X	X	X	X	X	X	X	X	X		X	X	X
											X			
											X			
X		X	X											
X		X	X											
X			X											
												X		
X	X				X	X								
X		X	X		X									
							X	X		X	X	X		

Table 1 Land Use Recommendations

Plan Order	Plan Area	RECOMMENDATION	Plan Area Goals										
			P-1	P-2	P-3	P-4	P-5	P-6	P-7	P-8	P-9	P-10	P-11
			Attract Desirable Uses	Solicit Public Support	Foster partnerships.	Build on Existing Assets	Create cohesive + distinctive character	Year-Round, 24/7 Activity	Create gateways	Emphasize quality design.	Promote multi-modality	Enhance pedestrian safety	Enhance ped/bike connexns
3.27	OT	Capitalize on current redevelopment at the Mainstreet/PACE Center Drive intersection.	X		X	X	X			X			
3.28	PA	Promote unity and cohesion along the entire length of Mainstreet, regardless of development phasing.				X	X			X			

Table 1 Land Use Recommendations

West End Goals							Old Town Goals							
W-1	W-2	W-3	W-4	W-5	W-6	W-7	O-1	O-2	O-3	O-4	O-5	O-6	O-7	O-8
Establish West End character	Foster partnership	Create Pedestrian Amenities	Provide new open space	Preserve BRT potential	Improve land use mix	Promote active ground floor uses	Improve land use mix	Promote active ground floor uses	Preserve historic character	Foster partnership	Promote multi-modality and events	Build on existing assets	Provide parallel E-W routes	Manage parking
							X	X				X		
									X			X		

Table 1 Land Use Recommendations

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4 Urban Form Recommendations

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Urban Form Overview

Urban Form plays an integral part in the overall experience of the Town. Downtown Parker is challenged by two different urban form issues. To the east, Old Town exhibits a distinctive and clearly defined historic 'Main Street' character, but has begun to feel pressure to integrate less authentic suburban style materials, forms and massing and incorporate more contemporary architecture. The question for Old Town, then, is how to integrate a variety of urban forms into the district while still maintaining a unified feel.

The West End, in contrast, lacks a true sense of place and instead exhibits a loose collection of suburban-style strip and pad development. This district, then, must define how redevelopment and new development can begin to shape a Mainstreet character that references but does not directly imitate the feel of Old Town.

The recommendations in this chapter propose a variety of new policies and policy updates, in order to guide development in a direction that aligns with the Town's evolved and evolving, more urban vision. Recommendations also focus on creating a 'common thread' of public realm enhancements that then set up a palette for greater variety of expression in private development.

Design Districts

As noted in the Existing Conditions Chapter, the majority of the Plan area falls within the Town's Greater Downtown Zoning District and is subject to the development design standards and guidelines established for the five sub-districts within that larger District (see map on following page). Mainstreet itself travels through three of the five sub-districts.

The Town's vision for the downtown district and Mainstreet in particular has continued to evolve since these districts were established in 2002. As such, the creation of this Master Plan presents an opportunity to ensure that the sub-districts align with future desired land use and urban design. A prominent theme during public outreach was a perception of two 'sides' of Mainstreet - a mental mapping that corresponds with the 'West End' and 'Old Town' designations used in this Master Plan.

In order to align the Downtown Design Districts with demonstrated community perception and this Master Plan, it is recommended that the existing Town Center District centered on the Mainstreet/Parker Road intersection and straddling both sides of Parker Road be consolidated with the adjacent districts to the east and west. This change would redraw the Historic Center and Twenty Mile Center District boundaries to use Parker Road as a dividing line between districts.

Historic Center District

With the potential for future signalization at Longs Way, as recommended in the Circulation chapter of this document, this intersection will become an important secondary gateway into the Historic Center District. For this reason, it is recommended that the northern boundary of the district extend slightly north to Longs Way in order to encompass the RTD Park-n-Ride and any future redevelopment on that site as well as the Longs Way/Parker Road intersection.

To the south, the Historic Center boundary shrinks to allow an expansion of the Pikes Peak Center District, but remains the governing development design standards for all Mainstreet abutting structures. This change is intended to promote critical mass and a more defined identity for the Pikes Peak District, as well as a cohesive aesthetic along the newly opened Pikes Peak Avenue. Likewise, the eastern boundary of Old Town retracts to allow the creation of a new East Downtown Gateway District. This new district acknowledges the different architecture and massing established by the new Town Hall and Parker Library.

Pikes Peak Center District

The map depicted on Figure 18 expands the former Pikes Peak District east along both the north and south face of the newly opened Pikes Peak Avenue. This redefinition will create a consistent boundary along the new roadway, and with greater area, will provide the opportunity to reinforce a legible district that is unique from but complementary to the central Mainstreet corridor.

The Master Plan recommends that architectural style within the Pikes Peak district be relaxed to allow but not require Victorian style structures. While the Historic Center and Pikes Peak districts will thus have very similar guidelines, the Pikes Peak district will acknowledge current development patterns in allowing for lower floor-area-ratio, greater setbacks, and re-use of residential structures.

Town Center District

The recommended elimination of this district helps to simplify the zoning and acknowledges the need to strongly reinforce the two main areas along the Mainstreet Corridor, the West End and Old Town. Defining Parker Road as the dividing line between these two main areas aligns with how downtown is already perceived of by the community.

Twenty Mile Center District

It is recommended that the Twenty Mile Center District be expanded to Parker Road and absorb the western portion of the prior Town Center District. The southern boundary is adjusted northward to recognize recent construction of multi-family and assisted living facilities.

Market Center District

No changes are recommended to this district.

Mainstreet Residential District

This new district acknowledges existing housing density in the surrounding area, the new construction of the large apartment complex to the west of Dransfeldt Road, as well as the assisted and independent living facility to the east of Dransfeldt. These projects introduce a new aesthetic and architectural style for the southwestern portion of the West End and this new district ensures that the buildings are not seen as out-of-compliance with governing Town policy.

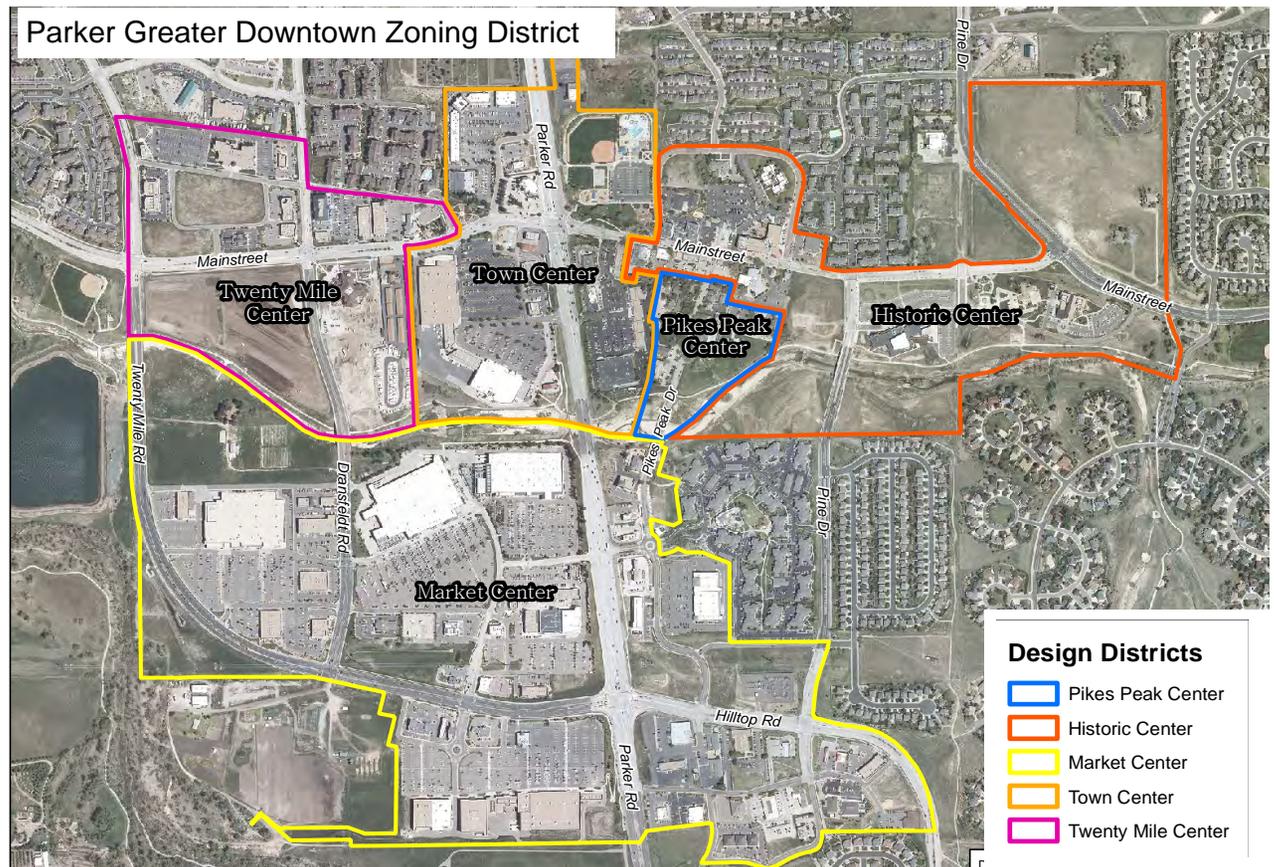


Figure 18 Design Districts

Existing Greater Downtown District: Design Districts are shown above. Recommended modifications to the districts are described on the preceding page.

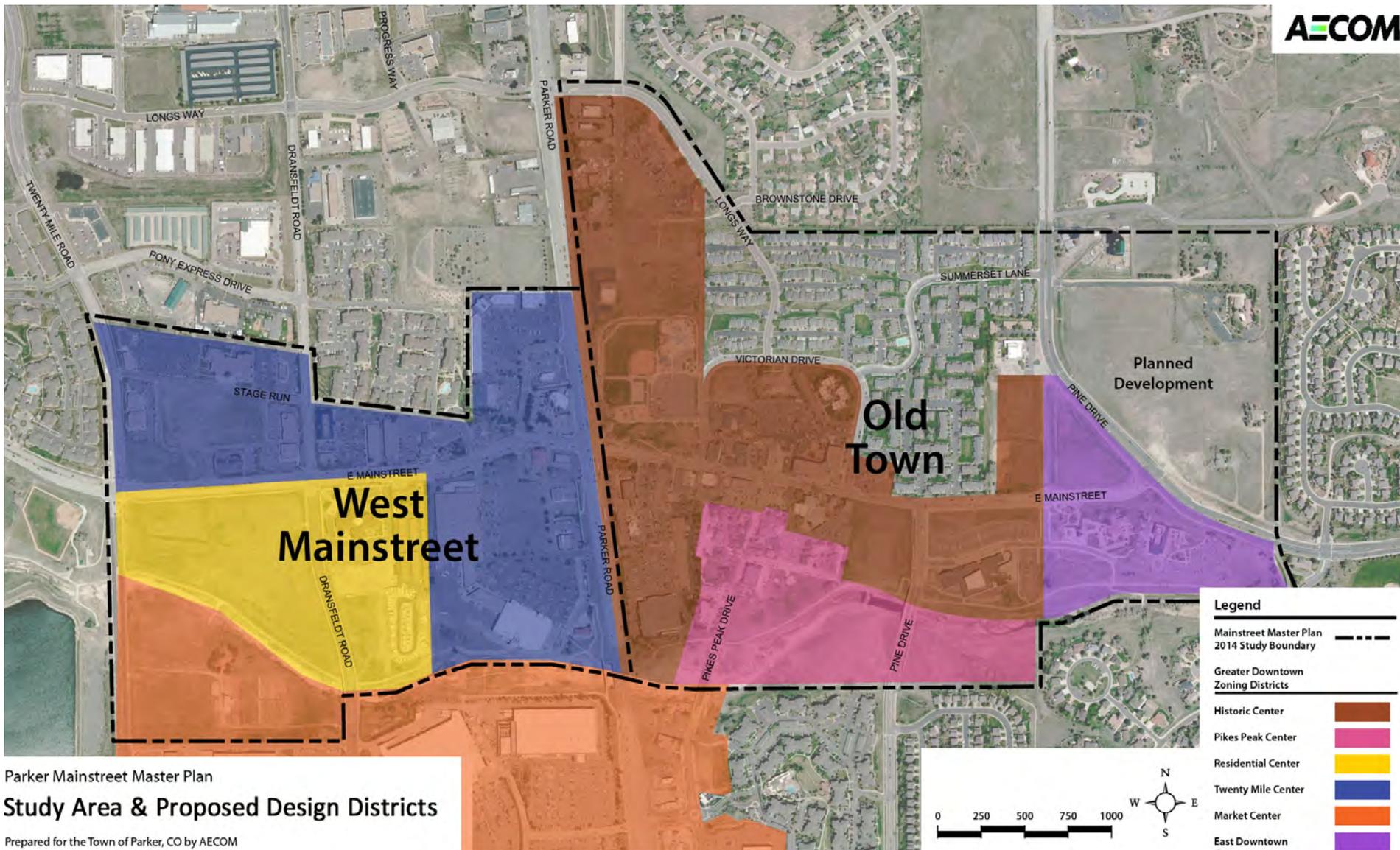
East Downtown Gateway District

The PACE center introduced a new, more contemporary style of architecture and massing in this area of Mainstreet and the new Parker Library designs exhibit similar contemporary materials and form. This new district acknowledges and provides for this variety. The district will extend from PACE Center Drive east to the eastern boundary of this Plan's study area. The Pine Curve property, immediately east of Pine Drive and within this district, is not anticipated to develop as a Planned Development (PD) with its own

development standards and guidelines and, therefore, standards, is not included in this or any other district.

Recommendation 4.1: Redraw Greater Downtown Zone Districts to more closely align with and support the vision of the Mainstreet Master Plan.

Recommendation 4.2: Amend the General Land Use Plan contained in the Parker 2035: Changes and Choices Master Plan to extend the Downtown Core character area west to Twenty Mile Road.



Recommended changes to the Greater Downtown Zoning District: Design Districts are shown above. Recommended changes include elimination of the central Town Center District (at Parker Road) and the creation of a new East Downtown Gateway at Pine Drive.

Height, Massing and Transition

Building height, massing and transition are the essential building blocks of the urban form. These elements define how buildings interrelate to each other and how they interface with the public realm. These elements impact public and private views as well as the extent of shadowing over the public realm. Careful consideration of these aspects of development is required to ensure that new developments fit within their surrounding context, and result in a human scaled development environment.

West End

Many community members noted that the West End felt too big and spread out. Future development should look to mitigate this perception in a number of ways, including narrowing building setbacks from the street, relegating surface parking to back-of-lot and increasing height to provide street enclosure.

Height

Except as noted in the preceding section regarding the Parker Road/Mainstreet intersection, all Mainstreet-abutting parcels in the west end should have a minimum height of two stories and a maximum height of four stories. This height is in keeping with the community desire for a human-scaled downtown and the greater width of Mainstreet west of Parker Road. As recommended in Old Town and in order to increase downtown density and activity, parcels that do not abut Mainstreet should be a minimum of two stories tall, with a maximum height of five stories (see Figure 21 Building Height Limits on following page).

This massing model illustrates changes at the Mainstreet/Parker Rd intersection and within the West End.

Recommendation 4.3: Required height for all new Mainstreet-facing buildings in the West End should be 2 floors minimum and 4 floors maximum. Parcels on the northwest and southwest corners of Parker Road should be a minimum of 3 stories and a maximum of 5 stories.

Recommendation 4.4: Required height for all new development that does not abut Mainstreet should be a minimum of 2 stories and a maximum of 5 stories

Massing

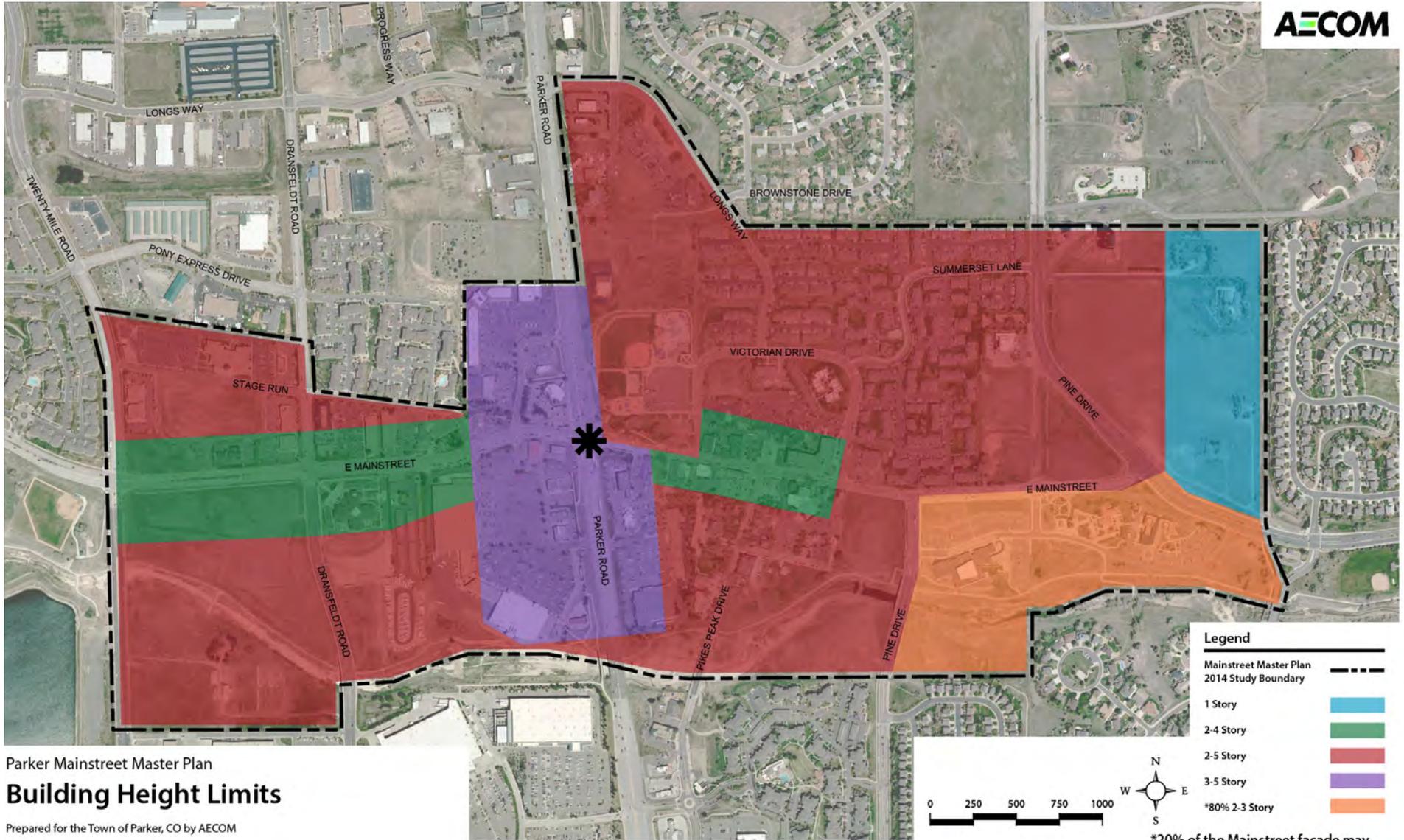
The deep setbacks characteristic of existing West End development are a significant contributor to the 'wide open' and 'too big' feel of the area. New development should approach the street, but the 4-lane width of Mainstreet suggests that a zero lot line might be too close and would have the potential to project the impression that the West End is trying (but failing) to replicate Old Town. In order to

differentiate the two areas and create a public realm in scale with the roadway, new development should be set back a minimum of 12 and a maximum of 20 feet from back of sidewalk. This range allows appropriate area for planting able to accommodate a columnar tree under correct sun exposure or two-table-deep outdoor dining while still providing street enclosure. A wider setback also maintains flexibility for future BRT, buffered bike lanes or on-street parking, depending on future needs.

Distance between buildings is as important as distance between building and sidewalk. Gaps between buildings, including outdoor dining areas, should be meaningful and functional urban spaces no greater than 20 feet wide, typically established by a maximum 10' side setback on abutting properties. In order to allow for larger outdoor dining patios, side setbacks may be increased depending upon adjacent use. If surface parking is provided, it must be at the rear of the building.



Figure 20 West End: Heights & Massing



Parker Mainstreet Master Plan
Building Height Limits

Prepared for the Town of Parker, CO by AECOM

Figure 21 Building Height Limits

*20% of the Mainstreet facade may consist of small one-story elements to emphasize height variation.

Note: 1 Story buildings acceptable in Parks.

Proposed changes to height standards include a minimum two story height throughout the plan area (with exceptions adjacent to existing single family homes), and a four to five story maximum. The intersection of Parker Road and Mainstreet (marked by *) is a critical intersection in which the buildings will serve as architectural gateways and should be 3-5 stories with signature architecture on the corner.

Recommendation 4.5: *Front setback for all new Mainstreet-facing buildings in the West End is a minimum of 12 and maximum of 20 feet from back of sidewalk.*

Recommendation 4.6: *Side setbacks for all new Mainstreet-facing buildings in the West end is a minimum of zero and a maximum of 10 feet. When the side setback area is to be used for outdoor dining, up to a 24-foot setback may be considered, depending upon adjacent use.*

Access

Front entrances of all new buildings should promote the goals of universal accessibility and should not require stairs or ramps for entry. Every effort should be made to keep each building's front door approach to the public side walk accessible.

All West End buildings shall have primary, active pedestrian entrances facing Mainstreet with vehicular and service access, including patron parking, located at the rear of all buildings. This requirement does not preclude similar exterior treatments on other elevations, and a building may have more than one primary entrance. Drive-thrus, gas stations and automobile repair shops of any configuration are not permitted.

Recommendation 4.7: *All new buildings in the West End shall have primary, active pedestrian entrances facing Mainstreet.*

Recommendation 4.8: *Ground floor elevation of all new buildings in the West End shall have universal access.*

Recommendation 4.9: *Drive-thrus, gas stations and automobile repair shops of any configuration should not be permitted on Mainstreet-abutting buildings in the West End.*

Parker Road/Mainstreet Height, Massing and Setback

This critical intersection will act as both a downtown gateway and wayfinding decision point for motorists bound for both Old Town and the West End. The three corner parcels (excluding O'Brien Park) of the Parker Road/Mainstreet intersection serve as architectural gateways and as such should require taller, three- to five-story buildings to establish a presence against the scale and volume of Parker Road. Buildings on these parcels should provide a signature, integrated architecture element on the Parker Road corner; corner signage alone is not an acceptable substitute for this requirement. Buildings should minimize setbacks from both roadways, to the greatest degree possible given sight triangle and utility requirements, and should create a significant architectural face with considerable building massing along both roadways. Sidewalk requirements are discussed later in this chapter.

Recommendation 4.10: *Buildings on the northwest, southwest and southeast corners of Parker Road should be a minimum of 3 stories and a maximum of 5 stories with integrated signature architectural elements.*

Recommendation 4.11: *Minimize setbacks from both roadways on the Parker Road/Mainstreet corners.*

Old Town Height

Stakeholder input indicated a clear preference to maintain the current 2-3 story heights already present in Old Town. However, new Mainstreet abutting development from Parker Road to the east leg of Victorian Drive should continue following the current height regulations of up to four stories with the addition of the minimum height of two stories.

In order to increase downtown density, intensity and activity, parcels that do not abut Mainstreet must be a minimum of two stories tall, with a maximum height of five stories.

The need to integrate new building heights with existing Old Town structures is reduced significantly to the east of the eastern leg of Victorian Drive, where a slight increase in height to five stories can be allowed. This increased height does come with two caveats:

1. Allow a maximum of one story on the eastern portion of the Plan area immediately adjacent to the Parker Vista residential neighborhood in order to transition building massing and mitigate impacts to viewsheds.
2. On the south side of Mainstreet, including the PACE Center and the municipal campus, require two to three stories with up to 20% of the Mainstreet façade in front of the PACE Center being allowed to consist of small one-story elements to emphasize height variation.

See Building Height Limits map on Figure 20.

Recommendation 4.12: Mainstreet-abutting properties between Parker Road and the east leg of Victorian Drive should be a minimum of two and a maximum of four stories for The parcel on the southeast corner of Mainstreet and Parker Road should be minimum of three and a maximum of five stories.



Figure 22 Old Town: Heights & Massing

This massing model illustrates how development on Lot 2 in front of the PACE Center retains views to the PACE facade. The model also shows new development across from the new Parker library, and on the vacant parcel west of the PACE Center.

Recommendation 4.13: On the south side of Mainstreet, including the PACE Center and the municipal campus, require two to three stories with up to 20% of the Mainstreet façade in front of the PACE Center being allowed to consist of small one-story elements to emphasize height variation.

Recommendation 4.14: Allow a maximum of one story on the eastern portion of the Plan area immediately adjacent to the Parker Vista neighborhood.

Recommendation 4.15: For development not abutting Mainstreet, buildings should be a minimum of two and a maximum of five stories.

Massing

In order to maintain the downtown, urban feel of the Old Town areas, all new development should continue to be subject to a zero lot line, also called a build-to line, meaning a minimum of 70% of the architectural frontage must meet the back of sidewalk. Entrances should be recessed in order to avoid door swing onto the sidewalk. New construction which includes an outdoor dining or merchant zone on the Mainstreet frontage may be set back a maximum of ten feet from back of sidewalk, a depth sufficient for a single four-person table between building façade and sidewalk. New construction which includes outdoor dining abutting Mainstreet but to the side of the building, may be a maximum of 24-feet wide, a width sufficient for four small tables. Greater setback or width than the metrics specified detracts from a consistent street wall and compromises the sense of enclosure created between the facades of facing buildings..

Recommendation 4.16: All new buildings in Old Town shall adhere to a zero lot line on their Mainstreet frontage. Outdoor dining or merchant zones may be setback a maximum of ten feet with appropriate 42" tall fencing for dining areas. Doors should be recessed.

Access

Front entrances of all new buildings should promote the goals of universal accessibility and should have a maximum slope of 2% to meet the adjacent sidewalk. Eliminating grade differences requiring stairs and ramps contributes to streamlined public realm and ensures a built environment that's welcoming to all users.

All structures shall have primary, active pedestrian entrances facing Mainstreet; this requirement does not preclude similar levels of finish on other faces, and a building may have more than one primary entrance. In contrast, vehicular and service access including patron parking, should be located at the rear of buildings in order to promote a pedestrian-focused environment and to reduce curb cuts and pedestrian-vehicular conflict.

Drive-thrus - typical of banks, fast food and other franchised and non-pedestrian focused uses - are not conducive to the pedestrian environment and pedestrian oriented businesses desired for Mainstreet and are not permitted in any configuration.

Recommendation 4.17: All new buildings in Old Town shall have primary, active pedestrian entrances facing Mainstreet.

Recommendation 4.18: Ground floor elevation of all new buildings in Old Town shall have universal access.

Recommendation 4.19: Drive-thrus, gas station and automobile repair of any configuration are not permitted on Mainstreet-abutting buildings in Old Town.

Transition

New development shall include appropriate transition in height to existing structures, with a maximum difference in height between adjacent buildings of two stories. Greater differences should use parapets, height step-downs, towers or other roof-line features to create gradual transition. Particular attention should also be paid to transitions from new development to existing single-family homes; the east boundary of the Plan area adjacent to the Parker Vista neighborhood and the south boundary adjacent to the Rowley Downs and Turtle Creek neighborhoods are of particular concern. Transition is less important when properties are separated by an arterial or collector roadway or for new development adjacent to multi-family properties, as these properties possess a more urbanized context due to density of development and traffic volumes.

Recommendation 4.20: Gradually transition new Old Town development using height and massing to existing, adjacent single-family residential development.

Architectural Character

The primary goal of downtown architecture in both Old Town and the West End is to establish a unique identity and sense of place. Architectural style should exhibit continuity in all elements of the structure and not mix character themes. It should emphasize the use of authentic materials, as described on the following page. It is possible for many different architectural styles to co-exist within the same area, and excellence in design on a building-by-building basis produces a higher quality, more interesting environment than strict adherence to a theme such as 'Victorian' or 'Contemporary'.

Additionally, care should be taken to create a sufficient level of detail for all facades in public view, including those abutting roadways, parks, plazas and parking. Development proposals for both new construction and renovation will be evaluated for adherence to context-sensitive design principles that include human scale (well proportioned to the human body), pedestrian focus and bicycle amenities, as well as 360 degree architecture.

Recommendation 4.21: New development should emphasize architectural authenticity in place of adhering to a prescribed style.

Recommendation 4.22: New development should not mix architectural character themes within a single building.

Materials

New and renovated structures should exhibit a high level of quality materials and finish on all roadway-facing facades and facades in public view. In particular, Mainstreet facades should emphasize smaller-scale and unit-type materials. Acceptable materials include but are not limited to brick and mortar, stone and masonry, as well as larger-format materials such as highly detailed, architecturally-interesting metal panel and precast concrete that contribute to the architectural style of the building. All of these finishes can be considered an authentic expression of the actual material of which they are made and are not less expensive material meant to imitate the real thing. In this vein, 'faux' veneer such as, but not limited to, cultured stone, vinyl and aluminum siding and exterior insulation finishing system (EIFS) should not be used. Wood siding, cedar shakes and other more residentially-oriented materials are permitted on non Mainstreet-abutting facades; Mainstreet facades may be accented with, but not primarily composed, of wood. In the West End Concrete Masonry Units (CMU) may be considered where they are used as secondary building material and have integrated color and burnished or split-faced finish.

Recommendation 4.23: Use high quality materials and prohibit faux veneer finishes.

Fenestration

In order to allow visual penetration into the building and promote a sense of permeability between inside and outside, a minimum of 60% of all ground floor facades and 30% of upper level facades shall be transparent glass for all buildings with Mainstreet frontage; all glass over and above the specified minimum percentages shall also be transparent. In addition, all windows must have lintels and ledges, where appropriate for the architectural style, to provide a shadow line. Like smaller-scale materials, this requirement promotes scale and interest at the pedestrian level. These fenestration requirements apply to all roadway-fronting facades or pedestrian areas such as parks and plazas.

Blank walls are not permitted on roadway-facing facades or pedestrian areas such as parks and plaza; fenestration or building wall articulation such as recessed or a similar delineation in the facade wall plane is required at or before a point no more than 30% of the total lineal facade length in order to provide visual interest. Facades adjacent to existing or future residential development should follow these same requirements for roadway facades, even if they do not face a road. Examples of these locations are the west boundary of the undeveloped Civic Centre parcel (abuts existing townhomes) and the west boundary of the Crossroads shopping center site (abuts apartments).

Recommendation 4.24: All new buildings shall have a minimum of 60% transparency at the ground level, and 30% transparency on upper levels.

Recommendation 4.25: All glass shall be transparent and all windows must have lintels and ledges where they are consistent with the architectural style of the facade.

Lighting

In addition to roadway and sidewalk lighting required for safety and security, new construction and redevelopment should consider the use of architectural lighting to provide nighttime interest. Examples include: soffit down-lighting, architectural façade mounted luminaires or interior lighting of transparent openings. Landscape and up-lighting are examples of special effect lighting; all new lighting should comply with the Town's energy efficiency goals, as well as meet Dark Sky requirements set by International Dark Sky Association, in order to minimize light pollution.

Recommendation 4.26: All new construction should provide architectural lighting to create visual nighttime interest.

Recommendation 4.27: All architectural lighting shall comply with local energy efficiency and Dark Skies requirements.

Faux vs. Authentic

Authentic finishes are those which clearly express the material of which they are made. Stone, brick, masonry and even well-formed concrete are all 'honest' materials that convey a sense of quality and pedestrian detail that is desired in downtown Parker.

'Faux' or 'fake' materials are often not as durable as the materials they imitate, and can cheapen the look of otherwise well-designed buildings.



Authentic materials: sandstone and brick.



'Faux' materials: vinyl siding, synthetic stucco (damaged).

Table for Two

With over 300 sunny days a year, Colorado is a true year-round climate. When asked what they'd like to see more of on Mainstreet, Parkersites roundly agreed they'd like more outdoor dining opportunities.

How much space do we need to set up that table and umbrella?



This cafe in Washington DC fits one four-person configuration into a nine foot wide former parking space.



Etai's in Denver's Stapleton gets just one four-person and one two-person table in its twelve-foot patio.



The Tailgate in Parker gets three generously spaced tables in its 30-foot patio.

Sidewalk and Streetscape

Within the public realm, most often defined as the area from the back of the roadway curb to the façade of adjacent buildings, the sidewalk is the most prominent element and its most basic requirement is to provide safe, continuous, unobstructed circulation for pedestrians. Sidewalk may be attached (adjacent to the curb) or detached (separated from the curb by a planted or unplanted 'amenity zone'), but in all cases should meet minimum standards for width and grade. Sidewalk should also include curb ramps to provide ADA-accessible transition from curb to street level at the end of each block; ramps should meet the most current version of ADA and Town standards. Preferred sidewalk configuration is described later in this chapter.

Signalized intersections and other high-use pedestrian areas will also include crosswalks and pedestrian crossing signals often accompanied by an audible indicator for vision-impaired users. Crosswalks in the Plan area should be highly improved and visible and may be applied to the roadway as paint or decals, or may be an inset materials such as colored and scored concrete. Plan area crosswalks should also include auto stop lines to cue drivers where to stop, providing more distance between pedestrians and

vehicles and increasing pedestrian comfortability. In order to maintain continuity with existing Old Town improvements, it is recommended that Mainstreet crosswalks are colored, square-scored concrete. Regardless of materials, all crosswalks must meet MUTCD, AASHTO and ADA standards for contrast.

The Town should consider Pedestrian crossing signals that automatically give a walk signal with each green cycle where high pedestrian activity occurs. User-activated pedestrian crossing signals, where a pedestrian pushes a button to call for the 'walk' signal, is appropriate in areas with less pedestrian activity. Overhead signs, flashing beacons, in-street yield and stop signs should also be utilized for safer pedestrian crossings in the plan area.

'Streetscape' are those other elements within the public realm that contribute to an attractive and welcoming pedestrian environment. Elements such as street trees, planters, benches, lighting, public art and other furnishings promote the human-scaled environment and 'hometown' feel cited as a top priority by the Town's residents. Streetscape can also serve as a critical 'common thread' tying together areas of dissimilar land use or architecture. Since Parker's West End is anticipated to have a lengthy period of land use transition, this latter function is particularly important to create cohesion within the area and between the West End and Old Town.



General Standards

Sidewalk Width

When setting a minimum sidewalk width, it is important to consider functional width versus physical width; since pedestrians will naturally shy away from walking directly adjacent to the curb, a five-foot detached sidewalk is functionally wider than a five-foot attached sidewalk. Shy distance also has an inverse relationship with speed of the adjacent roadway, with pedestrian comfort reducing (and shy distance increasing) as speed increases. Functional width can also be reduced by other factors such as rain and snow when splashback or snow storage reduces the area which pedestrians can traverse. Where planters, shrubs or trees are proposed, spacing and location should respond to on street parking intervals so as to not impede door openings or sidewalk access. All planters should be set back 12 inches from the back of curb, and should not impede sidewalk clear zone.

All of these factors mean that sidewalk width must be carefully considered in relationship to roadway classification and speed, as well as pedestrian volume. Recommended minimum widths and cross-sections differ for Old Town and West End, and are discussed later in this chapter.

Attached vs. Detached Sidewalk

Attached and detached sidewalks provide very different aesthetics; attached sidewalk is most commonly used in denser, downtown environments and conveys a more 'urban' feel while detached sidewalks are more common in lower-intensity, more residential areas. The Town's vision for a walkable downtown area recommends that Mainstreet through the Plan area be differentiated from the rest of the Town by using a more urban, attached section. Mainstreet sidewalks will also be wider than those found on intersecting and side streets, in order to accommodate the anticipated higher volumes of pedestrians as well as sidewalk activities and furnishings, such as advertising, outdoor merchandising, planters and window boxes. Due to the different contexts of Old Town and the West End, the width of Mainstreet sidewalks will vary; specific recommendations on sidewalk configuration are described later in this chapter.

Recommendation 4.28: Differentiate Mainstreet and other downtown streets through the use of wider, attached sidewalks.



Planters should be set back a minimum of 12 inches from back of curb, so as not to obstruct vehicular door swing or sidewalk access. This image also shows how sandwich signs can be placed to not obstruct pedestrian flow.

Crosswalks, Curb Ramps and ADA Accessibility

All sidewalks should meet ADA requirements for longitudinal and cross-slope, while also providing adequate drainage in order to avoid ponding and icing. Although different styles of curb ramp may be used along the Mainstreet corridor, for consistency and ease-of-use for the vision impaired every effort should be made not to change or alternate styles on a random basis. For example, the scale of Old Town intersections may be best suited to wraparound ramps, while the larger radii in the West End may be better served by directional ramps; it is best, however, not to alternate styles in a haphazard manner but to change style in a logical sequence.

While continuity and consistency is a high priority, it is acceptable for certain selected intersections to have a higher level of street design and finish in order to provide urban design emphasis as well as mitigate safety hazards for all users. For example, crosswalk materials could be upgraded from decal or paint to colored concrete or pavers. The Mainstreet/Pikes Peak Drive intersection, where urban design cues should encourage pedestrians to 'turn the corner' and also explore Pikes Peak Drive, is an example of the appropriate application of higher levels of finish. As a downtown and a PACE Center gateway, the Mainstreet/PACE Center Drive intersection would be another appropriate location for upgraded intersection finish. Appropriate West End locations for enhanced intersections will depend heavily upon future development, although the 20 Mile Road intersection will act as a downtown gateway and is a good candidate.

Sidewalks should maintain a clear walk zone and avoid any permanent or temporary signage or furnishings that could impede pedestrian flow. Sandwich boards are an example of such an obstruction and are of particular concern in narrow sections of sidewalk where pedestrians-fully abled or with mobility devices-would have difficulty maneuvering around the obstruction.

An area that is particularly lacking in adequate pedestrian facilities is the Pine Drive (Mainstreet) intersection, and improvements to this intersection should be made in conjunction with development occurring on the property to the east of the intersection (Pine Curve property). Lack of a northern crosswalk and excessive crossing distance are the primary concerns. To address the first issue, significant intersection design elements should be utilized and a highly visible crosswalk should be installed from the northwest corner of the intersection to the facing curb to offer as much comfort and protection to pedestrians as possible. In tandem with the new crosswalk, the design should include pedestrian refuges for those who cannot make the crossing in a single signal cycle. Pedestrian refuges should be protected with bollard or other appropriate device compliant with AASHTO standards.

A similar pedestrian refuge is also desirable at the south east-west crosswalk. The Town should evaluate whether southbound volume on Pine Drive warrants the existing right travel lane terminating at Stonehenge Way. If this lane can be removed, the roadway cross-section could be re-allocated and restriped to allow construction of a center pedestrian refuge. Although this lane was only recently added, the Town should consider the pros/cons and necessary actions for rebalancing downtown streets to be truly multi-modal. A reduced curb radius on both the northwest and southwest corners of the intersection would also reduce pedestrian crossing distance, and would also allow the installation of directional curb ramps to clarify movement for vision-impaired pedestrians.

Recommendation 4.29: Evaluate and inventory all ramps within the plan area. Upgrade missing and sub-standard/inconsistent ramps to meet Town of Parker and ADA standards, with an emphasis on consistency in the plan area.

Recommendation 4.30: In tandem with redevelopment of the adjacent property, redesign the eastern intersection of Pine Drive and Mainstreet to be more pedestrian friendly including important design elements such as pedestrian refuges, reduced curve radii, directional ramps, crosswalks on all four sides of the intersection, stop lines and a higher level of finish and design for crosswalks.

Recommendation 4.31: Create a consistent higher level of finish and design for all crosswalks at Mainstreet intersections

Recommendation 4.32: Work with CDOT to make improvements to the intersection and crosswalk at Parker Road and Mainstreet to improve pedestrian safety, comfort and convenience.

Recommendation 4.33: Evaluate and make upgrades to all crosswalks within the Plan area.



Upgraded crosswalks like this unit paver one can make an urban design statement and call attention to important intersections.

Curb Cuts

Particularly in a downtown environment that does not yet offer centralized, shared or public parking, virtually all buildings require vehicular access of some type, whether for service and delivery or for employee or patron parking. Vehicular traffic crossing the pedestrian zone always carries with it the potential for conflict between pedestrians and vehicles and detracts from the pedestrian-oriented environment; vehicular access across the sidewalk also increases the potential for vehicular congestion due to motorists waiting to turn into the driveway. New development should locate all access points off secondary streets or alleys and the Town should actively work with existing development to evaluate the feasibility of closing existing Mainstreet curb cuts. The Town should also consider a professional parking analysis at the same time to assist in determining the feasibility and timing of closing certain existing curb cuts.

Recommendation 4.34: *Design new development to provide access exclusively off secondary streets or alleys, with no new curb cuts on Mainstreet.*

Recommendation 4.35: *Work with existing development to evaluate the feasibility of closing existing Mainstreet curb cuts.*



Lighting

Street lighting, typically installed at 20 to 30 feet above the roadway, provides adequate illumination for safety and security for both pedestrians and vehicles. Pedestrian lighting, typically installed at 12 to 16 feet, adds interest and acts in tandem with other street furnishing to create a sense of place and identity. The Plan area utilizes both street and pedestrian lighting, but with a certain degree of irregularity in both spacing and actual fixtures.

In order to create the cohesive streetscape that will tie together Old Town and the West End, a consistent application is needed. A single style of streetlight and a single style of pedestrian light, already installed as part of the 2008 Mainstreet Streetscape improvements, should be used along the entire length of the Mainstreet corridor. Pedestrian lights along Mainstreet should include appropriate hardware for banners for seasonal and event display.

Street lights should be used at intersections only, except in unusual situations or extra-long blocks where pedestrian lights do not provide adequate coverage between intersections. As a vital placemaking element, however, pedestrian lights should not be eliminated in favor of street lights. Pedestrian lights should be spaced between street lights, at approximately 90 feet on center.



The Town's signature 5-globe lights are a crucial part of Parker identity. Building on the already existing locations, 5-globe lights should be limited to the medians and corners of intersecting arterial roadways where they will act as 'gateway' features to the downtown, and to Mainstreet medians and intersections, where they should be installed on all four corners (preferably on pedestals as at Pikes Peak Drive) to create consistency and rhythm as one moves through the downtown area.

Recommendation 4.36: *Use the existing lighting fixtures identified and constructed as part of the 2008 Mainstreet Streetscape improvements as the only street/pedestrian light combination to be used throughout the entire Plan area.*

Recommendation 4.37: *Relocate existing Town-owned 5-globe lights to medians and Mainstreet intersections and re-pattern their locations within the Plan area.*



What is a 'Furnishing Family'?

Street furnishings such as benches, trash receptacles and even lighting are an important part of defining character, be it historic, classic or contemporary. They are also critical in creating a sense of cohesion, and will be an important tool to tie together the West End and Old Town.

At present, downtown Parker has a wide variety of different styles and colors of each of these types of furnishings. As opportunity arises, the Town should work toward replacing this collection of mismatched pieces with a single 'family' of furnishings that are chosen to coordinate with each other and to promote the desired downtown vibe.



Street Trees and Other Plantings

Due to their verticality and the beautiful visual impact of their canopy, street trees are the most visible element of the streetscape. Street trees are critical to the Town's overall 'green' infrastructure and to establishing rhythm and consistency along the corridor. Street trees also provide a sense of scale and enclosure as they arch over the street, and offer all-important shade as well as seasonal interest as they flower in the spring and change colors in the fall. Their canopies also reduce the amount and rate at which rainwater strikes the ground, thereby reducing erosion and stormwater management costs and trees help to clean the air.

Like lighting, Mainstreet does have a certain number of trees, however, their installation is irregular and does not supply the visual impact achieved by a consistent, regularly-spaced line of trees. The 30' on-center spacing established by the redesigned streetscape in front of the PACE Center should be replicated throughout Old Town and the West End. Specific treatment of the ground plane will vary between Old Town and the West End and is described in the following section. Where there is insufficient room to install trees adjacent to the curb or where such retrofit would cause an excessive amount of misalignment with adjacent sidewalk, trees can be installed behind the curb at the same 30-foot spacing.

Tree species should comply with existing regulations, and should be chosen with an eye to seasonal interest. In order to avoid disease and heartiness problems associated with an urban monoculture, several species should be employed along the length of Mainstreet.

Recommendation 4.38: Install a consistent line of street trees in the amenity zone that are spaced at 30 feet on center, along the entire length of Mainstreet. Where trees cannot be installed adjacent to the curb in the amenity zone, install at the same spacing at back of sidewalk.

Furnishings

Streetscape is the common thread that will tie both parts of the Plan Area together, internally and to each other. All furnishings should be of a consistent style and color and, like the architecture, avoid a design that is overly themed. It is not necessary that all furnishings come from a single manufacturer or a single manufacturer line, but a single example of each element should be used along the entire length of Mainstreet, i.e. all benches are the exact same bench in the exact same color.

As noted previously, safe and continuous pedestrian circulation is the primary goal of the public realm. The minimum pedestrian widths described in the Old Town and West End text should remain clear of all obstructions, both permanent and temporary. To this end, all streetscape furnishings, including but not limited to benches, trash and recycling receptacles, planters and lights should be located either behind the sidewalk or in the curbside amenity zone. Due to safety and user comfort concerns, benches should be located only behind the sidewalk and not in the amenity zone. Bike racks should be placed to avoid wheel overhang into the adjacent roadway or sidewalk.

Recommendation 4.39: Locate all furnishings in the amenity zone or behind the sidewalk so as to maintain pedestrian circulation.

Public Art

Public art in Parker is a distinguishing part of the community's history, culture and uniqueness. As such, the Town embraces the social, cultural and economic value that it brings as a part of placemaking in the downtown area. The Town has a robust arts and culture scene and downtown enhancements should seek to promote Parker's bid to become a state-certified creative district. In addition to supporting artists and creative professionals through land use policy, as described in the preceding chapter, the Town should also seek opportunities

to integrate more visible artwork into downtown character. Public art can be integrated into the amenity zone of the public right-of-way-where it can act as an additional element to tie together Old Town and the West End, on private parcels through public-private partnerships, and within public open spaces such as parks and plazas. Public art can be a permanent part of these locations, or can be installed on a special event or long-term temporary basis.

Recommendation 4.40: *Use public art as an additional common thread to tie together the public realm of the West End and Old Town.*

Recommendation 4.41: *Identify locations for permanent and temporary/rotating art installations.*

Utilities

Utilities are a necessary part of the public realm, and can include signal controllers, electrical power boxes and phone pedestals. In some cases, such as phone pedestals, these appurtenances can be located behind buildings or behind the sidewalk. In other cases, such as traffic signal boxes, distance and maintenance requirements dictate that these elements be in the public realm. Even if a utility can be moved, it can be cost prohibitive to do so relative to the aesthetic or functional gains of relocating it.

Therefore, the Town should work with the utility companies to allow painted murals on utility boxes as well as investigate the potential to relocate or eliminate utility appurtenances as opportunities arise, as with infrastructure or development projects. New utilities, to the greatest degree possible, should be located outside of the public realm. If a new utility is constrained to either the amenity zone or the sidewalk area, the utility box should be located in the amenity zone in a location that disrupts the regular spacing of trees and lights to the least degree possible. Utility alignment should be considered during the review of all projects to minimize the impact on the streetscape.

Recommendation 4.42: *Locate new utilities and relocate old utilities underground and outside the public realm, as opportunity arises.*

Recommendation 4.43: *Consider the effect of the location of underground utility lines and easements on the design of public and private spaces.*

Mainstreet Old Town

The recommended cross-section for Old Town follows the recently installed streetscape in front of the PACE Center. Combined sidewalk/amenity zone is 15 to 18 foot wide (min/max), with attached sidewalk and trees in grates. This standard is divided into a one-foot step out zone, a four-foot tree zone and a ten- to thirteen-foot circulation zone. Sidewalk should be colored concrete with



The Town has a variety of public art pieces installed throughout the downtown area, and wishes to install additional pieces.

darker bands matching tree spacing and with trees in 4'x6' grates at 30 feet on-center. On blocks with on-street parking, tree grates should be set back 12 inches from back of curb to allow for a 'step-out strip' for individuals alighting from vehicles. For visual consistency and alignment of trees, this strip should extend the entire length of the block even if parking is prohibited in certain segments. The step-out strip may terminate in regular scoring at curb bump-outs.

Some sections of Old Town already utilize colored concrete, but without banding; in order to increase consistency, sandblast or dye can be used to add banding to these sections.

In cases where an existing building is demolished or a vacant parcel is developed, new construction should be held to the new streetscape standard but with precedence given to maintaining a consistent street façade so that horizontal modulation between facades does not exceed five feet.

Recommendation 4.44: *Create a consistent streetscape throughout Old Town with a combined sidewalk/amenity zone of 15 – 18 feet. Sidewalk should be attached, with trees in grates at 30 feet on center. Colored concrete banding should follow tree spacing.*



Old Town streetscape standards should follow the dimensions and spacing set by PACE Center streetscape.

Specific Direction for special cases

- Sidewalk at the southwest corner of the western intersection of Pine Drive/Mainstreet should be widened and trees installed with new development. Back of sidewalk should align with sidewalk in front of the chapel and private stone house immediately west of the vacant parcel. In order to achieve this alignment, Victorian Drive needs to be extended south, with stone house potentially moved.

Recommendation 4.45: *Widen sidewalk and install trees at the southwest corner of the western intersection of Pine Drive and Mainstreet with development.*

- The Mainstreet Center (The Schoolhouse) provides opportunity to install regular street trees behind the sidewalk without disrupting alignment of sidewalk.

Recommendation 4.46: *Install trees at 30 feet on center behind the sidewalk along the entire frontage of the Mainstreet Center.*

- The sidewalk on the south side of O'Brien Park should be redesigned to extend some of the Old Town streetscape elements west to Parker Road. These elements include street trees spaced at 30' and colored concrete. The purpose of extending these urban design elements is to tie O'Brien Park to the Old Town experience and to draw those on Parker Road towards Old Town by providing visual cues.

Recommendation 4.47: *Redesign the sidewalk and streetscape along south edge of O'Brien Park to extend some of the Old Town streetscape elements west towards Parker Road.*

- Reconstruct the sidewalk in front of the Wells Fargo building along Mainstreet on the southeast corner of the Parker Road intersection to extend many of the Old Town streetscape elements and draw elements from the new design for the south side of O'Brien park, across Mainstreet. The redesign will not only establish the downtown streetscape thread, but will also allow better alignment for pedestrians across the adjacent strip mall access drive.

Recommendation 4.48: *Reconstruct the sidewalk on south side of Mainstreet from Parker Road east to the strip mall drive to extend the Old Town streetscape elements.*

West End

The West End contains a variety of landscape and public realm conditions that are likely to persist into the mid- and long-term future. Planted medians feature a variety of trees, shrubs, ornamental grasses and ground cover plantings. The assisted living facility is fronted by a detached sidewalk, an intermediate landscape zone with evenly spaced trees and perennials, and a grass strip against the curb. The apartments directly west of Dransfeldt road also use a similar Mainstreet treatment of detached sidewalk and planted amenity zone. These more residential-style treatments contrast with attached sidewalk and irregular tree plantings in sod that line the north side of Mainstreet.

The challenge of the West End is to create a streetscape that continues the aesthetic and rhythm established in Old Town, but in a modified style that can integrate and harmonize with the existing median and residential landscapes. Changing the amenity approach from tree grates to tree cut-outs allows for perennial and shrub plantings while still maintaining the urban, attached sidewalk. Sidewalk material and tree spacing are the same as Old Town in order to provide continuity along the entire length of Mainstreet.

Recommended cross-section for the West End is a 16-foot wide, colored concrete sidewalk with trees in planting cut-outs. The 16-foot width is conceptually divided into an 8-foot sidewalk, wider than side streets in order to accommodate higher levels of foot traffic, and a 8-foot amenity zone to promote optimal growing conditions for plant materials. Cut-outs should be eight feet wide and 12 feet long with trees planted at 30 feet on-center; cut-outs should include perennial and/or shrub plantings.

Although on-street parking is not permitted or anticipated in the future along this section of Mainstreet, BRT remains a possibility and could suggest that a step-out strip be included in the streetscape design. Unlike on-street parking, however, public transit stops at roughly the same location each time



Planting cut-outs are an alternative to tree grates, and allow the inclusion of shrubs and perennials in the streetscape. Cut-outs are proposed in the West End.

and the stop could be adjusted so that doors do not conflict with planting locations. In the case of BRT in particular, the inclusion of off-board ticketing facilities and premium stop amenities means that portions of streetscape identified as BRT stops are likely to be redesigned and reconstructed at the time of service initiation in order to meet specific transit needs. For this reason, a step-out strip is not necessary.

Recommendation 4.49: *Create a consistent streetscape throughout the West End with a 16-foot wide colored concrete sidewalk and trees in 8'x12' cut-outs at 30 feet on center.*

Intersecting Streets

The majority of secondary roads within the Plan area are constrained more by existing right-of-way than by actual built structures. This condition allows more flexibility in creating an optimized public realm, defined largely by the Town's ability or willingness to acquire right-of-way. In virtually all cases, adequate sidewalk and curb ramp facilities already exist and it is the introduction of elements such as street trees, detached sidewalk and bicycle lanes that poses difficulties with available public property. In some cases, the Town may need to prioritize between enhanced streetscape and bike facilities or acquire additional right-of-way in order to have both.

Roadway cross-sections are discussed more fully in Chapter 6: Circulation Recommendations, but the recommended cross-section for all local and collector streets within the plan area is a six-foot detached sidewalk with an eight-foot wide amenity zone. Although there are many examples of street trees being planted in areas as narrow as four feet,

these trees tend to be stunted and have a limited life span. Particularly in Colorado's arid climate, an eight-foot amenity zone promotes healthier trees with a higher chance of success. Likewise, while a five-foot sidewalk does provide the minimum 60-inch width necessary for two wheelchairs to pass (or one wheelchair to turn in a circle and reverse direction), a six foot sidewalk provides a more comfortable passage, accommodates slightly higher levels of foot traffic and better aligns with the Town's pedestrian focus. Sidewalk should be uncolored gray concrete in order to support Mainstreet differentiation through the use of colored concrete. The amenity zone may be sod or perennial/shrub plantings as desired by the adjacent landowner.

Recommendation 4.50: *Public streets other than Mainstreet should have a minimum 6' sidewalk and 8' amenity zone with trees planted at 30-feet on center in a sod or planted amenity zone.*

Catalytic Projects

Gateways

Vehicular Gateways

The majority of downtown patrons enter the West End or Old Town via Parker Road, Twenty Mile Road, Mainstreet or Pine Drive. Regardless of which way they arrive, all visitors should have a clear sense of arrival and an appreciation that they have entered a special, pedestrian-oriented downtown.

The greatest challenge to creating this sense of arrival, common at all three major entry points, is the volume of future traffic and scale of the roadway. Existing and projected future volumes indicate that dropping traffic lanes at these three locations would impact the system's ability to move automobile traffic. Since the number of lanes may not change at these entry points, it becomes the role of urban design to make the feel of the roadway change. The installation of a consistent line of street trees, complemented by and interspersed with pedestrian lights, is one means by which a more detailed street environment can change the feel of a street and reinforce a change in roadway character. These and other human-scaled changes are recommended in the preceding text for both Old Town and the West End.

Physical gateways or gateway elements is another tool for calling the visitor's attention to the fact that they have entered a special area. There are many ways to create gateways. In some cases, gateways may be a literal element spanning roadway or sidewalk as shown in Figure 22. This type of gateway may be an option over Mainstreet at Twenty Mile Road or at Pine Drive, but is not an option at Parker Road due to that facilities status as a state highway.

In other cases, 'gateway' may be created by a single iconic element such as a large-scale piece of artwork or by paired monuments. Still another type of gateway is a gateway series, a line of objects that reinforce a type of formal entry drive. These types of gateways would be appropriate for Parker Road and would be located on both sides of Parker Road both north and south of the Mainstreet intersection.

Important considerations in the design and placement of gateway elements, whether a literal gateway or an iconic/sequential gateway, are scale and location. Gateway elements must be scaled so that they will stand out and be noticed against the size and speed of the adjacent roadway. Similar goals apply to selecting where to locate the elements; right at the downtown 'entrance' (i.e. the intersection) may not be the best location due to the number of other elements demanding motorist attention such as signals, signage as well as the mental and visual focus required to navigate the intersection itself.

Roadway scale and volume of users suggest that Parker Road be considered the primary gateway, while Pine Drive and Twenty Mile Road be considered secondary gateways. All three locations may have a different style or themed gateways tied to adjacent context or may use different versions of a single design. The Master Plan recommends that the Town work with stakeholders and the local public art committee to identify preferred gateway designs. Gateway elements should be considered in tandem with wayfinding elements so that the two complement each other and work together, rather than compete, to create identity and clear circulation guidance for visitors.

Recommendation 4.51: Introduce art and other gateway elements at the Parker Road, Twenty Mile Road and Pine Drive entrances to the Plan Area.

Recommendation 4.52 Parker Road and Mainstreet should be the primary gateway and the preferred gateway form would be a series of gateway art and/or monument components.

Recommendation 4.53: The gateway monument at Pine Drive and Mainstreet should include an archway element over Mainstreet Located north of Town Hall.

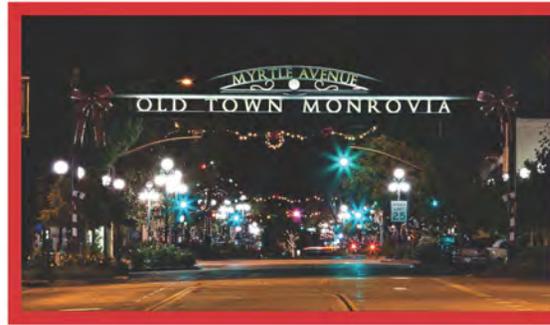
Recommendation 4.54: The gateway monument at Twenty Mile Road and Mainstreet should include art and monumentation that takes it cues from the Pine Drive and Parker Road gateway features.

Pedestrian and Bicycle Gateways

Vehicular gateway elements, combined with a clear shift to a more highly detailed, pedestrian-oriented streetscape, will provide sufficient pedestrian and bicycle cues for non-motorized arrivals at downtown's three main gateways.

For all other entries into the downtown area, wayfinding cues such as signage, pavement markings, bicycle racks or distinctive public art, should be provided where pedestrians and cyclists reach a 'decision point' to get to downtown. Turning off of Sulphur Gulch and onto a north-south sidewalk or bike path is an example of this type of location.

Arch



Very Strong Support @ Previous Public Outreach Engagements

Very Strong Support @ Previous Public Outreach Engagements

Monuments



Art

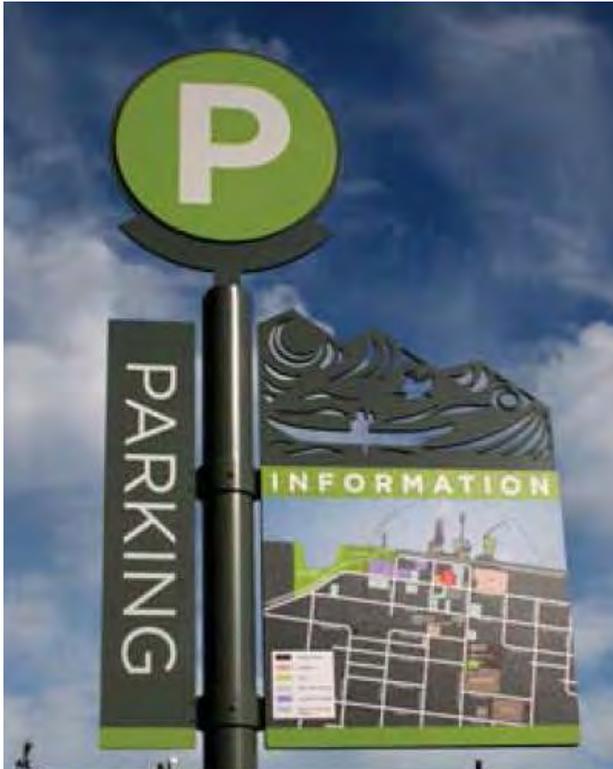


Strong Support @ Previous Public Outreach Engagements

Strong Support @ Previous Public Outreach Engagements

Figure 23 Stakeholder Input: Gateway Options

Stakeholders at a Wine Walk and Farmers' Market were asked their opinions on a variety of different types of gateways, as shown above. Arches across the roadway received the most positive responses, followed by large-scale artwork. Monuments were not favored.



Wayfinding

Signage and wayfinding work in tandem with gateways to attract and guide people to downtown, and help them navigate and locate destinations once they have arrived. Perhaps a less obvious but equally important function, a well-designed signage and wayfinding system can also manage traffic and help by directing visitors to less congested routes into downtown (i.e. not Mainstreet) and to their specific destination.

As of the writing of this document, the Town is developing a stand-alone signage and wayfinding plan, and for this reason the Master Plan does not provide specific direction or metrics for this family of elements. With this caveat, signage and wayfinding should be coordinated with other urban design elements of the Plan area, in order to provide a consistent and strong identity. It should be implemented in a consistent fashion, so that

each phase of installation is complete and useful in itself. For example, wayfinding systems will typically present information in a sequential and progressively more detailed fashion: regional signs will direct visitors to the downtown area; local signs will direct visitors to the West End, Old Town and public parking, and pedestrian-level signage will direct visitors to specific destinations such as the PACE Center. It is recommended that signs be phased by information level (for example all the regional signage) instead of by geography (such as all the signs on Parker Road).

Recommendation 4.55: *Coordinate wayfinding signage with other urban design elements in the Plan area.*

Recommendation 4.56: *Introduce wayfinding based on information level so that all signage is complete and useful at every phase.*



Like furnishings, signage and wayfinding also creates a 'family' of similar elements to guide people to and through downtown.

Figure 24 Signage & Wayfinding

Crossroads Shopping Center Redevelopment

The preceding Land Use chapter discussed the potential catalytic impact of redeveloping the Crossroads Shopping Center and adjacent area. The sketches on this page and the next page show two conceptual layouts for the area; these two layouts demonstrate a number of critical urban design principles and considerations that should guide planning and redevelopment of this important site.

Architecture

Common to both concepts is an office (or hotel) building with an integrated parking structure on the northwest corner of the Mainstreet/Parker Road intersection. Bringing this architecture up to road edge on both Parker and Mainstreet is of the utmost importance and should be a requirement for any redevelopment approvals. The drawings further illustrate a rounded façade, such as might be achieved with a glass curtain wall or other signature architectural feature on that corner. Vehicular drop-off is internal to the site.

Street Edge and Underpass Plaza

Other urban design recommendations shown in both concepts include lining Parker Road with vertical architecture and creating a conscious plaza environment at the west portal to the Kieffer's Crossing pedestrian and bicycle underpass. Placing buildings along Parker road creates visual interest, signals a change in development pattern and reinforces Downtown's primary Mainstreet Gateway. The plaza assists in non-vehicular wayfinding and enhances user experience of the underpass.

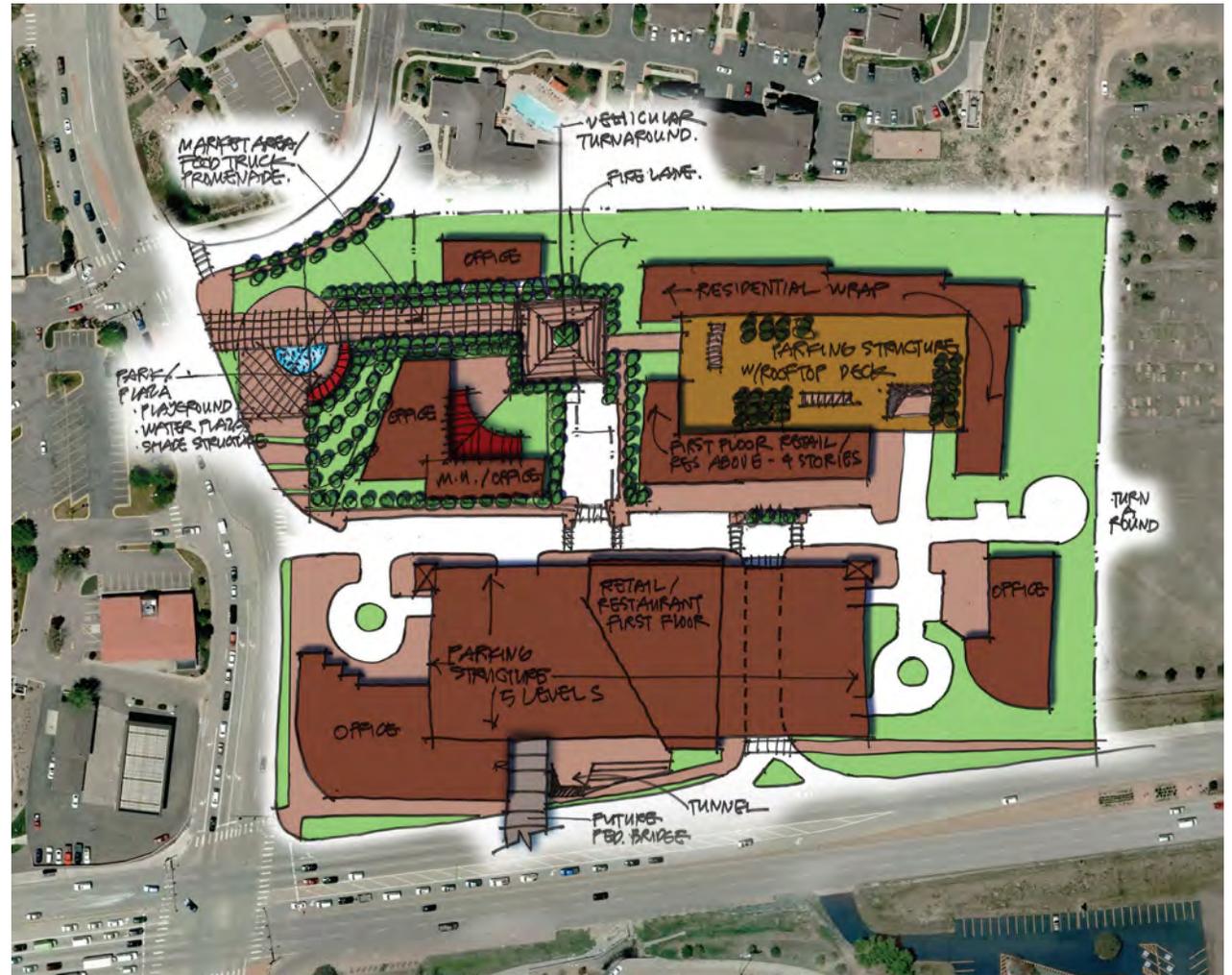


Figure 25 Concept A

Concept A: This application of design guidance for the Crossroads Shopping Center area places all parking in shared parking structures, in order to provide space for higher density development. Residential uses include condo- and apartment-type dwellings on the western edge of the site.

Circulation and Open Space

Both schemes bring new buildings adjacent to the park and introduce active first floor uses which in turn brings enclosure and scale to the park attracting more people, and expanding the potential for off season and indoor/outdoor use (depending upon tenanting of buildings). Concept A on the preceding page retains the current boundaries of the park and the Stage Run intersection. Concept B slightly reconfigures Stage Run to create a 'T' intersection with the development's western roadway.

Parking

Structured parking is integral to achieving the desired density of use in this redevelopment site which in turn will increase the daytime population to patronize downtown shops and restaurants. The concepts show varying degrees of intensity with Concept B illustrating a combination of structured and surface parking, while Concept A consolidates all parking in structures.

Recommendation 4.57: The following design principles shall provide direction for future Crossroads Shopping Center redevelopment:

- (1) provide signature architecture and massing at the Parker Road/Mainstreet intersection,
- (2) provide an architectural edge along the entire Parker Road edge of the property,
- (3) use building placement and design to integrate existing parks into the development and provide park programming and activation,
- (4) provide a significant plaza/gateway treatment at the west portal of Kieffer's Crossing.

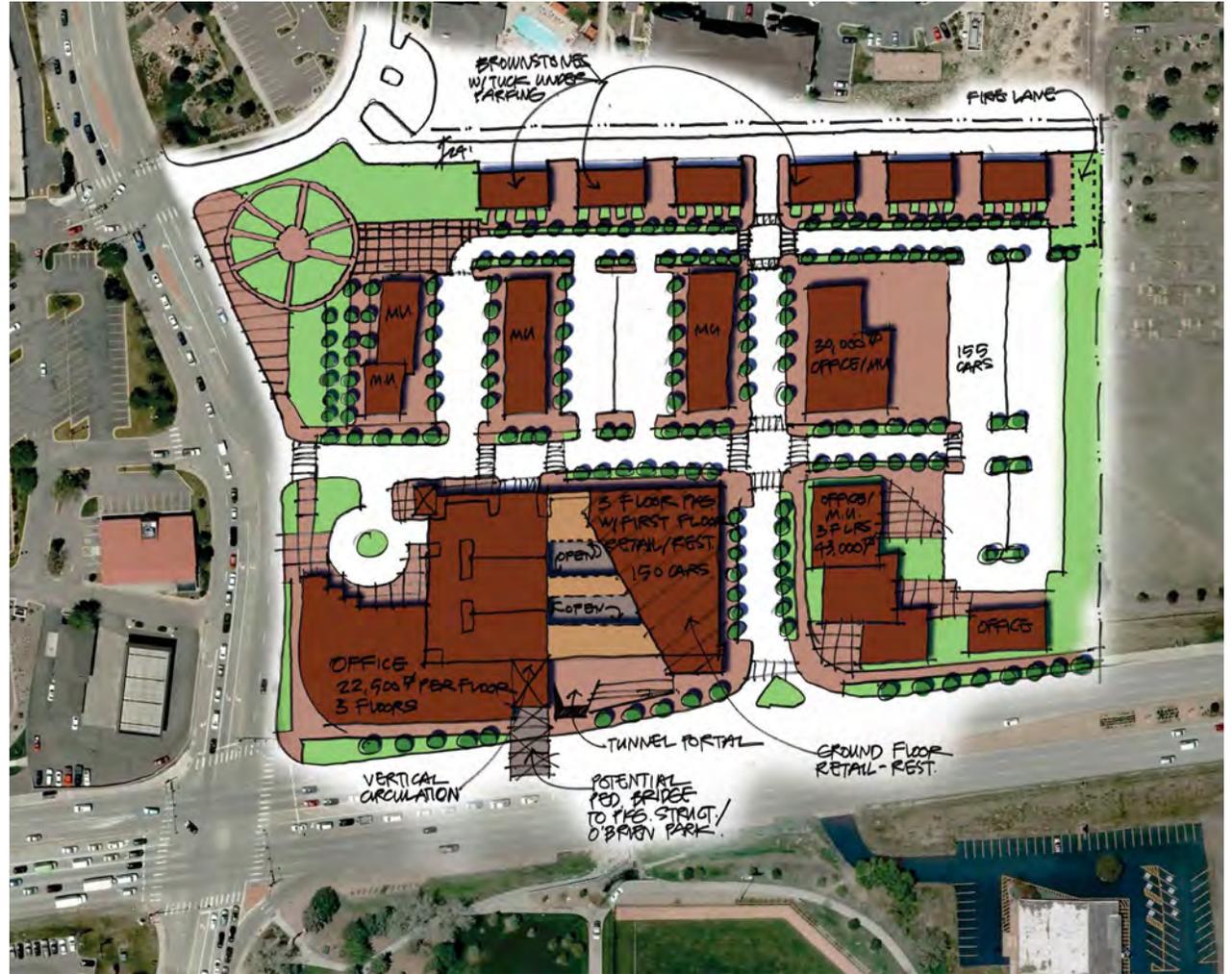


Figure 26 Concept B

Concept B: An alternate application of design guidance for Crossroads Shopping Center uses a mixture of surface and structured parking, with townhomes providing transition of density at the site's western edge.

Special Projects

Stage Run Plaza and Development

The Stage Run Plaza is anticipated to be constructed as part of private development but with allowance or easement for public use. The sketch on the right shows conceptual configuration of this development area; this drawing is intended only to illustrate several important urban design principles associated with this area rather than to suggest preliminary plaza/park design.

Central Axis

One of the plaza's primary goals is to create visual and physical, non-vehicular connection from Mainstreet to one of the West End's most significant destinations, the existing movie theater. The northern terminus of this connection is the theater itself while the southern connection should align with the (proposed) mid-block crossing of Mainstreet. The mid-block crossing is intended to align with internal access points to the new apartment complex on the south side of Mainstreet.

Permanent structures are prohibited in the existing utility easement running diagonally right-to-left from Mainstreet to Stage Run. For this reason, the new public space will use streetscape along the entrance road to relate and tie into Mainstreet. This connection may take the form of a widened sidewalk, similar paving or similar planting design.

Street Trees and Other Plantings

Due to their verticality and the beautiful visual impact of their canopy, street trees are the most visible element of the streetscape. Street trees are critical to the Town's overall 'green' infrastructure and to establishing rhythm and consistency along the corridor. Street trees also provide a sense of scale and enclosure as they arch over the street, and offer all-important shade as well as seasonal interest as they flower in the spring and change colors in the fall. Their canopies also reduce the amount and rate at which rainwater strikes the



Figure 27 Central Public Plaza

A central public plaza provides visual and physical connection between Mainstreet and the interior of the Stage Run development. The plaza can also be used as a special event venue.

ground, thereby reducing erosion and stormwater management costs and trees help to clean the air.

Architecture

To activate the plaza and create a contained space with a sense of enclosure and scale, the plaza should be lined with buildings with active ground-floor uses. Surface or structured parking as a plaza edge is strongly discouraged. Plaza facades should be treated as primary building faces, with the same high quality level of finish required for Mainstreet-abutting structures.

Underground utilities severely limit potential development on the parcel at the southern end. Visually engaging uses that are compatible with the easement restrictions, such as an open-air gazebo or simple shade structure, are appropriate in this context and can activate the site.

Urban Design Elements

The plaza should be small enough to feel like a contained, destination space but large enough to feel like more than a simple circulation corridor. It should be designed with discrete areas; the sketch illustrates an open flexible plaza area closest to the theater, and east-west circulation spine, and a second gardenesque area with a bosquet and a water feature. Personality of each of these areas could differ, but the key concept is to create a variety of spaces that can be used in many ways to activate the plaza as a whole. Water features are always popular, and help create motion, interest and activity when few people are in the plaza.

Recommendation 4.58: *The following design principles shall provide direction for future Stage Run plaza design:*

(1) align the plaza to provide visual and physical connection between Mainstreet and the existing movie theater site,

(2) provide visually interesting uses compatible with the underground utility easement,

(3) provide 'rooms' or areas within the larger plaza with a particular eye to event programming,

(4) provide scale, activity and enclosure through an architectural edge on the east and west sides of the plaza, rather than surface parking.

Pilgrims Place

Streets are the most ubiquitous element of the public space system. As such, across the nation, communities are considering ways in which roadways and alleys can do more than just convey vehicles, but also contribute to and enhance the public realm, even act as flexible multi-purpose spaces that better serves all travel modes.

Current Use and Location

A low-volume roadway parallel to and just 200 feet south of Mainstreet, Pilgrims Place is a 400' dead end public road that functions more as alley access to adjacent public and private parking than it does as actual vehicular connection between points. Pilgrims Place is also adjacent to a small unused private grass area, and if a user is in the know, can act as vehicular or foot connection to the strip mall to the west and to the restaurant's side patios at Mainstreet and Victorian Drive.

Improvements to Pilgrim's Place should include safer, ADA-compliant pedestrian access, a more attractive foot connection to Mainstreet, clarified pedestrian and vehicular connections to the strip mall and activation of the existing pocket park. Improvements to Pilgrims Place should also result in a flexible space that allows for both use as a transportation corridor and, when appropriate, closure for an events space.

Parking and Access Reconfiguration

The sketches on the next page illustrate two approaches to potential urban design and circulation enhancements for Pilgrims Place and environs. Option 1 requires the redesign of access and layout of the private parking immediately north of the roadway and may require acquisition of private land for plaza expansion to the west and south. Option 2 expands the Pilgrim Place right-of-way to the south in order to provide pedestrian enhancements without requiring parking redesign (although such consolidation would also work with this concept).

In Option 1, the unused drive-through structure on the adjacent building (a former bank) is removed and primary lot access is moved to Pikes Peak Drive; this change allows a more efficient one-way drive aisle oriented in an east-west direction. Introducing this new access point and consolidate the existing, multiple access points on Pilgrims Place into a single driveway aligned with the St. Mathew's church parking provides clarified circulation patterns and provides a safer pedestrian environment. A pedestrian walkway directly behind the adjacent building provides access to Pikes Peak and to Mainstreet via the restaurant's pedestrian alley.

Two bays of angled parking, one single-loaded and one doubled-loaded, offer a total of approximately 65 parking spaces. While lack of complete striping makes it difficult to determine current capacity, it is estimated that reconfiguration offers a similar number of spaces as that found under existing conditions. Parking may remain private and could follow a variety of management models. One option would be to retain private ownership but lease the lot to the Town for public parking. Alternately, the lot could be designated generally for the tenants and customers of the adjacent building only or even signed with a particular number of spaces allotted to each tenant.

Option 2 focuses on improvements that can be made with less impact to existing parking and utilities. Pilgrims Place expands to the south and a sidewalk is added to the roadway's southern edge. Depending upon the amount of property acquisition, sidewalks on both edges of the roadway would be possible, but if width is sufficient for only one side the southern side avoids conflict with existing utility poles and minimizes interface between pedestrians and cars from the parking lots on the roadways northern edge.

Urban Design Elements

In both options, Pilgrims Place itself would be treated as a fully shared space intended to accommodate pedestrians, cyclists and vehicles. A single, enhanced paving material would be applied along the entire length of the roadway and would extend to a new plaza in the adjacent private grassy area and the pedestrian passage leading to Mainstreet between the restaurants. This treatment creates an organized, well defined travel configuration and multi-purpose space.

An additional potential enhancement, removable bollards could be placed at the east and west ends of the alley, making it possible to close the street for use as a stand-alone public space or extended, special-event spillover space from O'Brien Park.

The redesign also defines a formalized sidewalk connection between Pilgrims Place and the strip mall to the west. This small improvement provides a huge public benefit by defining a specific circulation path for pedestrians replacing the current option of wandering through overly-wide parking behind the mall building, and provides a strong wayfinding cue that there is another destination to the west of the grass area. Existing overhead utilities should be located underground to minimize the visual clutter and remove the design constraints of the poles.

Recommendation 4.59: Redesign Pilgrims Place to provide improved pedestrian access to Mainstreet and the strip mall area to the west.

Recommendation 4.60: Redesign Pilgrims Place as a shared, multi-modal 'flexible' space that can be closed as a special event space.

Two different concepts illustrate how Pilgrims Place could serve as a multi-modal shared space. The option 1 shows reconfiguration of adjacent private parking and a new stage/performance area within the private park, while option 2 would still require additional public right-of-way for improvements but does not require realignment of private parking.



Figure 28 Pilgrims Place Multi-Modal Concept Option 1



Figure 29 Pilgrims Place Multi-Modal Concept Option 2

Townhome Integration

Three buildings of townhomes line the north side of Mainstreet across from the PACE Center; with setbacks varying between 30 and 40 feet and varying amounts of grade change from street to building, these homes provide a significant break in the urban street edge envisioned for Mainstreet. Although redevelopment remains a future possibility, the Town's desire for near-term investment in this part of downtown call for an interim solution that respect the privacy of the townhome residents but also promote a more urban edge to these properties.

The images at right illustrate the continuation of urbanizing this area with a widened sidewalk, retaining wall and street trees. Key to this concept is the retaining wall which serves a dual purpose in addressing the grade difference between the street and building while creating a stylized, urban edge to the street. The wall and its materials should be designed to support the vocabulary used in the streetscape rather than the more residential, suburban vocabulary of the townhomes. The wall could also be topped with fencing as an additional design element and potential art opportunity. This concept would require the acquisition of additional right-of-way to bring the sidewalk, including street trees, to the width described in this urban design section.

Recommendation 4.61: *Widen the sidewalk and add street trees and street amenities to create an urban public realm in front of the Town and Country townhomes adjacent to Mainstreet.*



Figure 30 Streetscape and Fencing

Enhanced streetscape and a fence-topped retaining wall provide a more urban edge to this section of Mainstreet.

PACE Center Development

The Town has established a number of principles for future development on this site.

First among the principles is visual permeability from Mainstreet to the PACE Center itself. New development must carefully balance the need for a consistent street wall with an appropriate plaza or other 'break' providing visual access to the PACE Center. This break should be publicly accessible and its potential for public programming evaluated. The site and public space should incorporate public art, and also provide physical pedestrian access through the site, connecting Mainstreet, new development, and the PACE Center.

New development should also follow the goals of active ground floor uses outlined for all of Mainstreet. Structures should have primary, active pedestrian entrances and storefronts on both north and south sides, maximizing pedestrian access and synergy between potential future retail and restaurant uses and the PACE Center. Architectural style should complement but not compete with the PACE Center; development may utilize similar form, massing or materials but should not try to exactly duplicate PACE architecture.

The sketch at right illustrates one way in which site elements may be arranged, according to these guiding site principles.

Recommendation 4.62: *The following design principles should provide direction for future PACE Lot 2 redevelopment:*

- (1) maximize pedestrian access from Mainstreet to the PACE Center and to new development,*
- (2) ensure active, ground-floor uses abutting Mainstreet,*

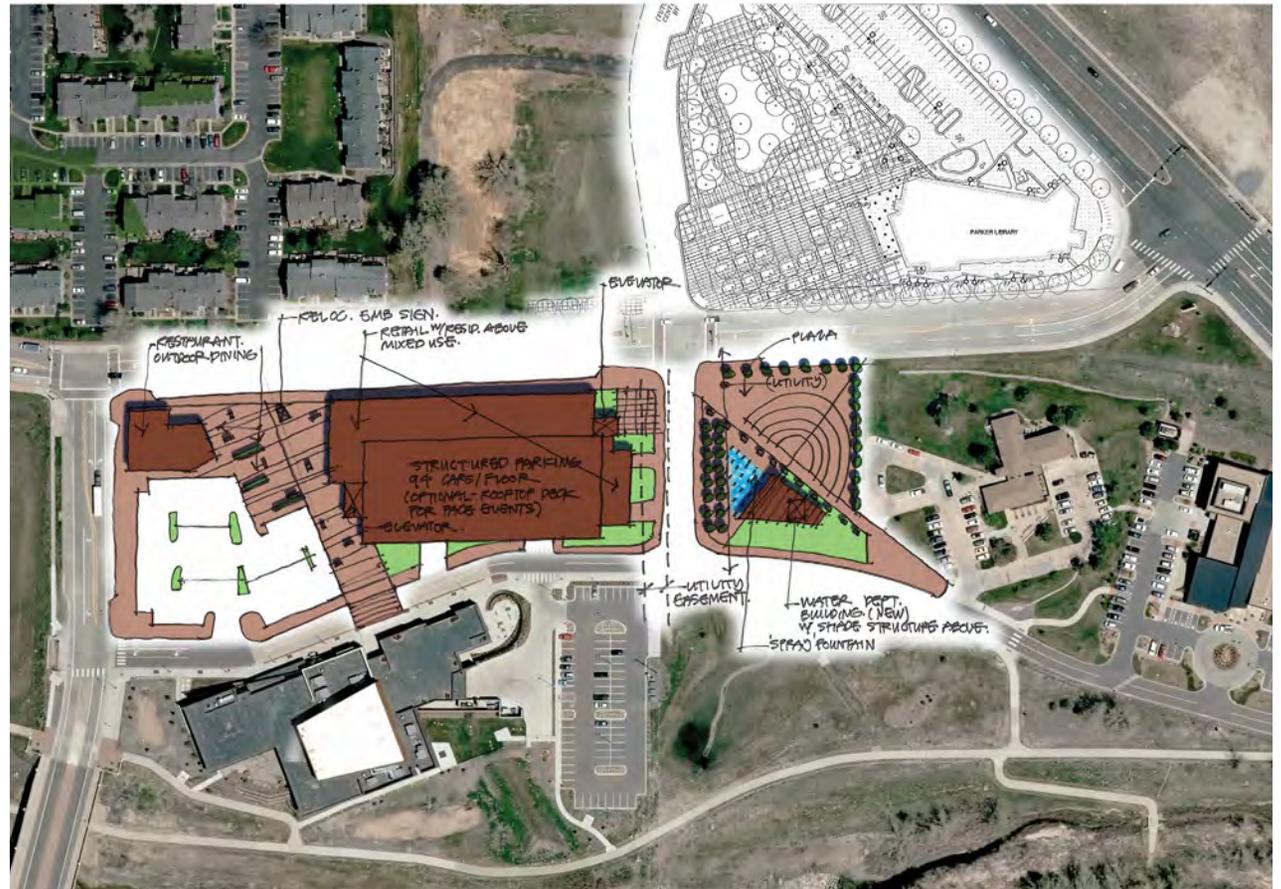


Figure 31 Parcel 2 Integration with Xeric Garden

This conceptual sketch illustrates how PACE Lot 2 could integrate with the site of the existing xeric garden.

- (3) incorporate public space and public art into the development,*
- (4) provide primary facades on all four sides of new development and*
- (5) utilize an architectural style that complements but does not duplicate or compete with the PACE Center.*

Existing Xeric Demonstration Garden

The southeast corner of PACE Center Drive and Mainstreet, currently occupied by a xeric demonstration garden is a challenging site to redevelop. The site's proximity to the new Parker library, the PACE Center and future redevelopment on the northeast and northwest corners provide a high level of visibility and potential foot, bicycle and vehicular traffic. The continued use of the small water department building and the need for vehicular access to the enclosure, severely limit the size, configuration and access to a potential structure.

The sketches at right illustrate two conceptual approaches to activate this corner. Critical to both concepts is the introduction of some sort of use giving people a reason to be in or traverse through the area. The top sketch retains the site as public open space, but creates a more highly designed and activated area with an amphitheater, a spray fountain and a water

or art wall. As it exists today, the site is too isolated to attract clientele, but could attract patrons with suitable, similar uses anchoring this end of Mainstreet.

The bottom sketch offers a simpler, less programmed plaza to be activated by outdoor dining from an adjacent restaurant and a small parking garage for additional downtown patron parking. Like the previous concept, this option would be dependent upon the creation of critical mass on the intersection's other three corners.

Recommendation 4.63: Explore options to activate the southeast corner of PACE Center Drive and Mainstreet, giving priority to active uses that complement PACE and library activities.

Two concepts show ways to activate the site of the existing xeric garden. The top drawing shows an independent plaza space, while the bottom drawing envisions an indoor-outdoor integration of plaza and building, potentially a restaurant or other active use.



Figure 32 Xeric Garden Activation



Figure 33 Xeric Garden Activation

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Putting it Together

Working toward Our Goals

The chart below shows a complete list of the land use recommendations contained in this chapter, and shows how each recommendations ties with the goals established for the project.

PA: Plan Area WE: West End OT: Old Town

Plan Order	Plan Area	RECOMMENDATION	Plan Area Goals										
			P-1	P-2	P-3	P-4	P-5	P-6	P-7	P-8	P-9	P-10	P-11
			Attract Desirable Uses	Solicit Public Support	Foster partnerships.	Build on Existing Assets	Create cohesive + distinctive character	Year-Round, 24/7 Activity	Create gateways	Emphasize quality design.	Promote multi-modality	Enhance pedestrian safety	Enhance ped/bike connexns
Design Districts													
4.1	PA	Redraw Greater Downtown Zone Districts to more closely align with and support the vision of the Mainstreet Master Plan.		X		X	X						
4.2	PA	Amend the General Land Use Plan contained in Parker 2035: Changes and Choices Master Plan to extend the Downtown Core character area west to Twenty Mile Road.		X		X	X						
Height, Massing and Transition													
4.3	OT	Required height for all new Mainstreet-facing buildings in the West End should be 2 floors minimum and 4 floors maximum. Parcels on the northwest and southwest corners of Parker Road should be a minimum of 3 stories and a maximum of 5 stories.				X	X						
4.4	OT	Required height for all new development that does not abut Mainstreet should be a minimum of 2 stories and a maximum of 5 stories.					X		X				
4.5	WE	Front setback for all new Mainstreet-facing buildings in the West End is a minimum of 12 and maximum of 20 feet from back of sidewalk.					X						X
4.6	WE	Side setbacks for all new Mainstreet-facing buildings in the West end is a minimum of zero and a maximum of 10 feet. When the side setback area is to be used for outdoor dining, up to a 24-foot setback may be considered, depending upon adjacent use.					X			X			
4.7	WE	All new buildings in the West End shall have primary, active pedestrian entrances facing Mainstreet.					X			X			X
4.8	WE	Ground floor elevation of all new buildings in the West End shall have universal access.					X			X			X
4.9	WE	Drive-thrus, gas stations and automobile repair shops of any configuration should not be permitted on Mainstreet-abutting buildings in the West End.					X			X		X	X
4.10	PA	Buildings on the northwest, southwest and southeast corners of Parker Road should be a minimum of 3 stories and a maximum of 5 stories with integrated signature architectural elements.					X		X	X			
4.11	PA	Minimize setbacks from both roadways on the Parker Road/Mainstreet corners.					X		X	X			X

Table 2 Urban Form Recommendations

West End Goals							Old Town Goals							
W-1	W-2	W-3	W-4	W-5	W-6	W-7	O-1	O-2	O-3	O-4	O-5	O-6	O-7	O-8
Establish West End character	Foster partnership	Create Pedestrian Amenities	Provide new open space	Preserve BRT potential	Improve land use mix	Promote active ground floor uses	Improve land use mix	Promote active ground floor uses	Preserve historic character	Foster partnership	Promote multi-modality and events	Build on existing assets	Provide parallel E-W routes	Manage parking
X									X			X		
X														
									X			X		
X														
X														
X														
X														

Table 2 Urban Form Recommendations

Plan Order	Plan Area	RECOMMENDATION	Plan Area Goals										
			P-1	P-2	P-3	P-4	P-5	P-6	P-7	P-8	P-9	P-10	P-11
			Attract Desirable Uses	Solicit Public Support	Foster partnerships.	Build on Existing Assets	Create cohesive + distinctive character	Year-Round, 24/7 Activity	Create gateways	Emphasize quality design.	Promote multi-modality	Enhance pedestrian safety	Enhance ped/bike connexns
4.12	PA	Mainstreet-abutting properties between Parker Road and the east leg of Victorian Drive should be a minimum of two and a maximum of four stories for the parcel on the southeast corner of Mainstreet and Parker Road should be minimum of three and a maximum of five stories.					X		X				X
4.13	OT	On the south side of Mainstreet, including the PACE Center and the municipal campus, require two to three stories with up to 20% of the Mainstreet façade in front of the PACE Center being allowed to consist of small one-story elements to emphasize height variation.					X						
4.14	OT	Allow a maximum of one story on the eastern portion of the Plan area immediately adjacent to the Parker Vista neighborhood.											
4.15	PA	For development not abutting Mainstreet, buildings should be a minimum of two and a maximum of five stories.						X					
4.16	OT	All new buildings in Old Town shall adhere to a zero lot line on their Mainstreet frontage. Outdoor dining or merchant zones may be setback a maximum of ten feet with appropriate 42" tall fencing for dining areas. Doors should be recessed.					X			X			X
4.17	OT	All new buildings in Old Town shall have primary, active pedestrian entrances facing Mainstreet.					X			X			X
4.18	OT	Ground floor elevation of all new buildings in the Old Town shall have universal access.					X			X			X
4.19	OT	Drive-thrus, gas station and automobile repair of any configuration are not permitted on Mainstreet-abutting buildings in Old Town.					X			X		X	X
4.20	OT	Gradually transition new Old Town development using height and massing to existing, adjacent single-family residential development.											
Architectural Character													
4.21	PA	New development should emphasize architectural authenticity in place of adhering to a prescribed style.					X			X			

Table 2 Urban Form Recommendations

West End Goals							Old Town Goals							
W-1	W-2	W-3	W-4	W-5	W-6	W-7	O-1	O-2	O-3	O-4	O-5	O-6	O-7	O-8
Establish West End character	Foster partnership	Create Pedestrian Amenities	Provide new open space	Preserve BRT potential	Improve land use mix	Promote active ground floor uses	Improve land use mix	Promote active ground floor uses	Preserve historic character	Foster partnership	Promote multi-modality and events	Build on existing assets	Provide parallel E-W routes	Manage parking
									X			X		
									X					
								X	X			X		
								X	X			X		
								X	X			X		
									X			X		
X												X		

Table 2 Urban Form Recommendations

Plan Order	Plan Area	RECOMMENDATION	Plan Area Goals										
			P-1	P-2	P-3	P-4	P-5	P-6	P-7	P-8	P-9	P-10	P-11
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4.22	PA	New development should not mix architectural character themes within a single building.								X			
4.23	PA	Use high quality materials and prohibit faux veneer finishes.					X			X			
4.24	PA	All new buildings shall have a minimum of 60% transparency at the ground level, and 30% transparency on upper levels.					X			X			
4.25	PA	All glass shall be transparent and all windows must have lintels and ledges where they are appropriate and consistent with the architectural style of the façade.					X			X			
4.26	PA	All new construction should provide architectural lighting to create visual nighttime interest.					X			X			
4.27	PA	All architectural lighting shall comply with local energy efficiency and Dark Skies requirements.								X			
Sidewalk and Streetscape													
4.28	PA	Differentiate Mainstreet and other downtown streets through the use of wider, attached sidewalks.					X			X			
4.29	PA	Evaluate and inventory all ramps within the plan area. Upgrade missing and sub-standard/inconsistent ramps to meet Town of Parker and ADA standards, with an emphasis on consistency in the plan area.									X	X	
4.30	OT	In tandem with redevelopment of the adjacent property, redesign the eastern intersection of Pine Drive and Mainstreet to be more pedestrian friendly including important design elements such as pedestrian refuges, reduced curve radii, directional ramps, crosswalks on all four sides of the intersection, stop lines and a higher level of finish and design for crosswalks.									X	X	
4.31	PA	Create a consistent higher level of finish and design for all crosswalks at Mainstreet intersections.					X			X	X	X	X
4.32	PA	Work with CDOT to make improvements to the intersection and crosswalk at Parker Road and Mainstreet to improve pedestrian safety, comfort and convenience.					X			X	X	X	X

Table 2 Urban Form Recommendations

West End Goals							Old Town Goals							
W-1	W-2	W-3	W-4	W-5	W-6	W-7	O-1	O-2	O-3	O-4	O-5	O-6	O-7	O-8
Establish West End character	Foster partnership	Create Pedestrian Amenities	Provide new open space	Preserve BRT potential	Improve land use mix	Promote active ground floor uses	Improve land use mix	Promote active ground floor uses	Preserve historic character	Foster partnership	Promote multi-modality and events	Build on existing assets	Provide parallel E-W routes	Manage parking
X									X			X		
X									X			X		
X									X			X		
X									X			X		
X									X			X		
		X									X	X		
		X									X	X		

Table 2 Urban Form Recommendations

Plan Order	Plan Area	RECOMMENDATION	Plan Area Goals										
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			Attract Desirable Uses	Solicit Public Support	Foster partnerships.	Build on Existing Assets	Create cohesive + distinctive character	Year-Round, 24/7 Activity	Create gateways	Emphasize quality design.	Promote multi-modality	Enhance pedestrian safety	Enhance ped/bike connexns
4.33	PA	Evaluate and make upgrades to all crosswalks within the Plan area.				X				X	X	X	X
4.34	PA	Design new development to provide access exclusively off secondary streets or alleys, with no new curb cuts on Mainstreet.				X					X	X	
4.35	PA	Work with existing development to evaluate the feasibility of closing existing Mainstreet curb cuts.				X					X	X	
4.36	PA	Use the existing lighting fixtures identified and constructed as part of the 2008 Mainstreet Streetscape improvements as the only street/pedestrian light combination to be used throughout the entire Plan area.				X	X						
4.37	PA	Relocate existing Town owned 5-globe lights to medians and Mainstreet intersections and re-pattern their locations within the plan area.					X						
4.38	PA	Install a consistent line of street trees in the amenity zone that are spaced at 30 feet on center, along the entire length of Mainstreet. Where trees cannot be installed adjacent to the curb in the amenity zone, install at the same spacing at back of sidewalk.					X			X			
4.39	PA	Locate all furnishings in the amenity zone or behind the sidewalk so as to maintain pedestrian circulation.					X				X	X	
4.40	PA	Use public art as an additional common thread to tie together the public realm of the West End and Old Town.			X		X		X	X			
4.41	PA	Identify locations for permanent and temporary/rotating art installations.			X		X						
4.42	PA	Locate new utilities and relocate old utilities underground and outside the public realm, as opportunity arises.					X				X	X	
4.43	PA	Consider the effect of the location of underground utility lines and easements on the design of public and private spaces.					X			X			
4.44	OT	Create a consistent streetscape throughout Old Town with a combined sidewalk/amenity zone of 15 – 18 feet. Sidewalk should be attached, with trees in grates at 30 feet on center. Colored concrete banding should follow tree spacing.					X				X	X	

Table 2 Urban Form Recommendations

West End Goals							Old Town Goals							
W-1	W-2	W-3	W-4	W-5	W-6	W-7	O-1	O-2	O-3	O-4	O-5	O-6	O-7	O-8
Establish West End character	Foster partnership	Create Pedestrian Amenities	Provide new open space	Preserve BRT potential	Improve land use mix	Promote active ground floor uses	Improve land use mix	Promote active ground floor uses	Preserve historic character	Foster partnership	Promote multi-modality and events	Build on existing assets	Provide parallel E-W routes	Manage parking
		X									X	X		
X														
									X					
		X										X		
		X										X		
X	X	X								X		X		
X		X												
									X			X		

Table 2 Urban Form Recommendations

Plan Order	Plan Area	RECOMMENDATION	Plan Area Goals										
			P-1	P-2	P-3	P-4	P-5	P-6	P-7	P-8	P-9	P-10	P-11
			Attract Desirable Uses	Solicit Public Support	Foster partnerships.	Build on Existing Assets	Create cohesive + distinctive character	Year-Round, 24/7 Activity	Create gateways	Emphasize quality design.	Promote multi-modality	Enhance pedestrian safety	Enhance ped/bike connexns
4.45	OT	Widen sidewalk and install trees at the southwest corner of the western intersection of Pine Drive and Mainstreet with development.					X				X	X	
4.46	OT	Install trees at 30 feet on center behind the sidewalk along the entire frontage of the Mainstreet Center.					X						
4.47	OT	Redesign the sidewalk and streetscape along south edge of O'Brien Park to extend some of the Old Town streetscape elements west towards Parker Road.									X	X	X
4.48	OT	Reconstruct the sidewalk on south side of Mainstreet from Parker Road east to the strip mall drive to extend the Old Town streetscape elements.					X				X	X	X
4.49	WE	Create a consistent streetscape throughout the West End with a 16-foot wide colored concrete sidewalk and trees in 8'x12' cut-outs at 30 feet on center.					X			X		X	X
4.50	PA	Public streets other than Mainstreet should have a minimum 6' sidewalk and 8' amenity zone with trees planted at 30-feet on center in a sod or planted amenity zone.					X			X		X	X
Catalytic and Special Projects													
4.51	PA	Introduce art and other gateway elements at the Parker Road, Twenty Mile Road and Pine Drive entrances to the Plan Area.					X		X				
4.52	PA	Parker Road and Mainstreet should be the primary gateway and the preferred gateway form would be a series of gateway art and/or monument components.					X		X				
4.53	OT	The gateway monument at Pine Drive and Mainstreet should include an archway element over Mainstreet Located north of Town Hall.				X	X		X				
4.54	WE	The gateway monument at Twenty Mile Road and Mainstreet should include art and monumentation that takes it cues from the Pine Drive and Parker Road gateway features.				X	X		X				
4.55	PA	Coordinate wayfinding signage with other urban design elements in the Plan area.					X				X		X

Table 2 Urban Form Recommendations

West End Goals							Old Town Goals							
W-1	W-2	W-3	W-4	W-5	W-6	W-7	O-1	O-2	O-3	O-4	O-5	O-6	O-7	O-8
Establish West End character	Foster partnership	Create Pedestrian Amenities	Provide new open space	Preserve BRT potential	Improve land use mix	Promote active ground floor uses	Improve land use mix	Promote active ground floor uses	Preserve historic character	Foster partnership	Promote multi-modality and events	Build on existing assets	Provide parallel E-W routes	Manage parking
									X			X		
									X			X		
												X		
X		X												
									X			X		
X														
X														
									X			X		
X														

Table 2 Urban Form Recommendations

Plan Order	Plan Area	RECOMMENDATION	Plan Area Goals										
			P-1	P-2	P-3	P-4	P-5	P-6	P-7	P-8	P-9	P-10	P-11
			Attract Desirable Uses	Solicit Public Support	Foster partnerships.	Build on Existing Assets	Create cohesive + distinctive character	Year-Round, 24/7 Activity	Create gateways	Emphasize quality design.	Promote multi-modality	Enhance pedestrian safety	Enhance ped/bike connexns
4.56	PA	Introduce wayfinding based on information level so that all signage is complete and useful at every phase.									X	X	X
4.57	WE	The following design principles shall provide direction for future Crossroads shopping center redevelopment: (1) provide signature architecture and massing at the Parker Road/Mainstreet intersection, (2) provide an architectural edge along the entire Parker Road edge of the property, (3) use building placement and design to integrate existing parks into the development and provide park programming and activation, (4) provide a significant plaza/gateway treatment at the west portal of Kieffer's Crossing.								X			
4.58	WE	The following design principles shall provide direction for future Stage Run plaza design: (1) align the plaza to provide visual and physical connection between Mainstreet and the existing movie theater site, (2) provide visually interesting uses compatible with the underground utility easement, (3) provide 'rooms' or areas within the larger plaza with a particular eye to event programming, (4) provide scale, activity and enclosure through an architectural edge on the east and west sides of the plaza, rather than surface parking.								X			
4.59	OT	Redesign Pilgrims Place to provide improved pedestrian access to Mainstreet and the strip mall area to the west.									X	X	X
4.6	OT	Redesign Pilgrims Place as a shared, multi-modal 'flexible' space that can be closed as a special event space.						X					
4.61	OT	Widen the sidewalk and add street trees and street amenities to create an urban public realm in front of the townhomes adjacent to Mainstreet.					X			X			
4.62	OT	The following design principles should provide direction for future PACE Lot 2 redevelopment: (1) maximize pedestrian access from Mainstreet to the PACE Center and to new development, (2) ensure active, ground-floor uses abutting Mainstreet, (3) incorporate public space and public art into the development, (4) provide primary facades on all four sides of new development and (5) utilize an architectural style that complements but does not duplicate or compete with the PACE Center.	X	X	X	X	X			X			
4.63	OT	Explore options to activate the southeast corner of PACE Center Drive and Mainstreet, giving priority to active uses that complement PACE and library activities.	X			X	X	X		X			

Table 2 Urban Form Recommendations

West End Goals							Old Town Goals							
W-1	W-2	W-3	W-4	W-5	W-6	W-7	O-1	O-2	O-3	O-4	O-5	O-6	O-7	O-8
Establish West End character	Foster partnership	Create Pedestrian Amenities	Provide new open space	Preserve BRT potential	Improve land use mix	Promote active ground floor uses	Improve land use mix	Promote active ground floor uses	Preserve historic character	Foster partnership	Promote multi-modality and events	Build on existing assets	Provide parallel E-W routes	Manage parking
X		X	X		X	X								
X		X	X		X	X								
												X		X
											X	X		
							X	X		X		X		
												X		

Table 2 Urban Form Recommendations

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Circulation Recommendations

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Circulation Overview

Parker's transportation vision, put forward in the *Town's Parker 2035 Master Plan* and articulated in the 2014 *Town of Parker Transportation Master Plan* (TMP), is to 'develop and sustain a safe, convenient, and efficient transportation system incorporating various modes of travel including automobiles, public transportation, bicycle and pedestrians.' This vision aligns perfectly with the national Complete Streets movement, and recognizes that transportation choice is good for the individual, good for business, and good for the community. The circulation recommendations included in this Mainstreet Master Plan examine how the large-scale goals of the TMP should be carried out within the downtown context, and to promote and enhance the specific character of the West End and Old Town. This chapter is divided into five sections addressing vehicular, bicycle and pedestrian circulation, as well as public transit and parking.



Vehicular Circulation

Mainstreet serves as the central east-west spine for not only the plan area but also for the Town and Douglas County as a whole. The lack of alternate east-west routes and the need for a more fine-grained grid was identified in the Town's 2003 *Greater Downtown District Circulation Network Visionary Plan*, and this need for diffused traffic flow and connectivity continues today. The high volume of through- and local traffic contributes to high speeds on Mainstreet's wider, 4-lane section west of Parker Road, and increasing congestion on Mainstreet's narrower two-lane section east of Parker Road. A prime goal of future circulation efforts must be to identify transportation system additions that can enhance the potential for alternate routing that does not funnel such high numbers of cars through Mainstreet. This section describes potential options to achieve this goal.

***Recommendation 5.1:** Pursue circulation measures that create and promote alternate routes to Mainstreet and a finer grain, gridded roadway network.*

New Roadways + Roadway Re-alignments

Pikes Peak Avenue Extensions

In order to align with the concept of a more gridded roadway system providing alternate routes to Mainstreet, as outlined in the Town's Greater Downtown District Circulation Network Visionary Plan 2003, Pikes Peak Avenue should be extended on both sides of Parker Road. On the east side, the roadway will connect with and create a full intersection at the existing 'T' of Pikes Peak Drive and Pikes Peak Avenue, south of St. Matthew's Church. On the west side of Parker Road, the extension will formalize the existing drive aisle separating the two big box parking lots and connect with a north-south extension of Briargate Lane. Both of these roadway extensions would be completed in coordination with private property owners when they redevelop their sites.

Although the two extensions will align across Parker Road, current CDOT standards require access to the east side to be right-in/right-out only and access to the west side to be a three quarters movement (right-in/right-out and left-in with no left-out). A full movement, signalized intersection is not anticipated at this time, but should be a long term consideration to help relieve east/west congestion at the Parker Road and Mainstreet intersection.

***Recommendation 5.2:** Extend Pikes Peak Avenue from Pikes Peak Drive to Parker Road.*

***Recommendation 5.3:** Introduce new east-west segment of Pikes Peak Avenue between Parker Road and the north-south extension of Briargate Lane.*

Briargate Lane Extension

To provide improved access and increased redevelopment flexibility into the existing self-storage parcel east of the assisted living facility, the 4-way intersection of Stage Run and Mainstreet should be replaced by a 4-way intersection with Briargate Lane. This reconfiguration also ties directly to the east-west extension of Pikes Peak Avenue proposed in the preceding section and supports the creation of a gridded system with alternates to Mainstreet.

In this reconfiguration, Briargate Lane would extend southward from its existing terminus at Stage Run, through the bank parcel and between self-storage and big box. Stage Run would terminate at a 'T' with the extended Briargate and a new Mainstreet/Briargate signal would replace the Mainstreet/Stage Run signal. This new roadway would be developed in coordination with the the private property owners when as they redevelop their sites.

Recommendation 5.4: In coordination with redevelopment, create a four-way, full-movement signalized intersection at Mainstreet/Briargate Lane, in tandem with the closure of the adjacent Stage Run intersection.

Victorian Drive Extension

This plan recommends that the eastern leg of Victorian Drive be extended south to Pikes Peak Avenue. This extension offers benefit to both the Mainstreet Center and future development of the currently vacant parcel between the Mainstreet Center and Pine Drive. An extended Victorian Drive will improve access to Mainstreet Center's rear parking, improve general circulation in this area and will allow the Center to convert the existing driveway into an active and pedestrian-oriented outdoor space, such as a plaza or art exhibition area. The roadway extension will also provide an additional point of access into the vacant parcel, particularly useful if future development introduces a parking structure. The Victorian/Mainstreet intersection should be considered for signalization to manage anticipated increase in traffic movements.

Recommendation 5.5: In coordination with development and redevelopment, extend the east leg of Victorian Drive to Pikes Peak Avenue.

Pikes Peak Avenue Extension

At the east end of the study area, Pikes Peak Avenue should be extended east to Stonehenge Way. With a full-movement intersection at Stonehenge and Mainstreet (Pine Drive), this extension will provide an alternate route to Town Hall and the PACE Center; the current Pikes Peak Avenue connection to Mainstreet allows only right-in/right-out access. This extension will require reconfiguration of the existing one-way, southbound entry into Stonehenge, as well as reevaluation of the safest alignment for the Sulphur Gulch Trail Crossing.

Recommendation 5.6: Extend Pikes Peak Avenue to Stonehenge Way.

New Signals

This plan proposes new traffic signals at four locations, and the removal of a signal at one location. All new signals must meet MUTCD standards. The first location for a new signal is the new Briargate/Mainstreet intersection described above; this change assumes the removal of the existing signal at Stage Run and Mainstreet.

The second new signal is Longs Way and Parker Road. The Town has contacted CDOT regarding the potential for this signal but no definitive decision has been made at the time of this Plan. The introduction of this signal would provide critical congestion mitigation to Mainstreet by providing:

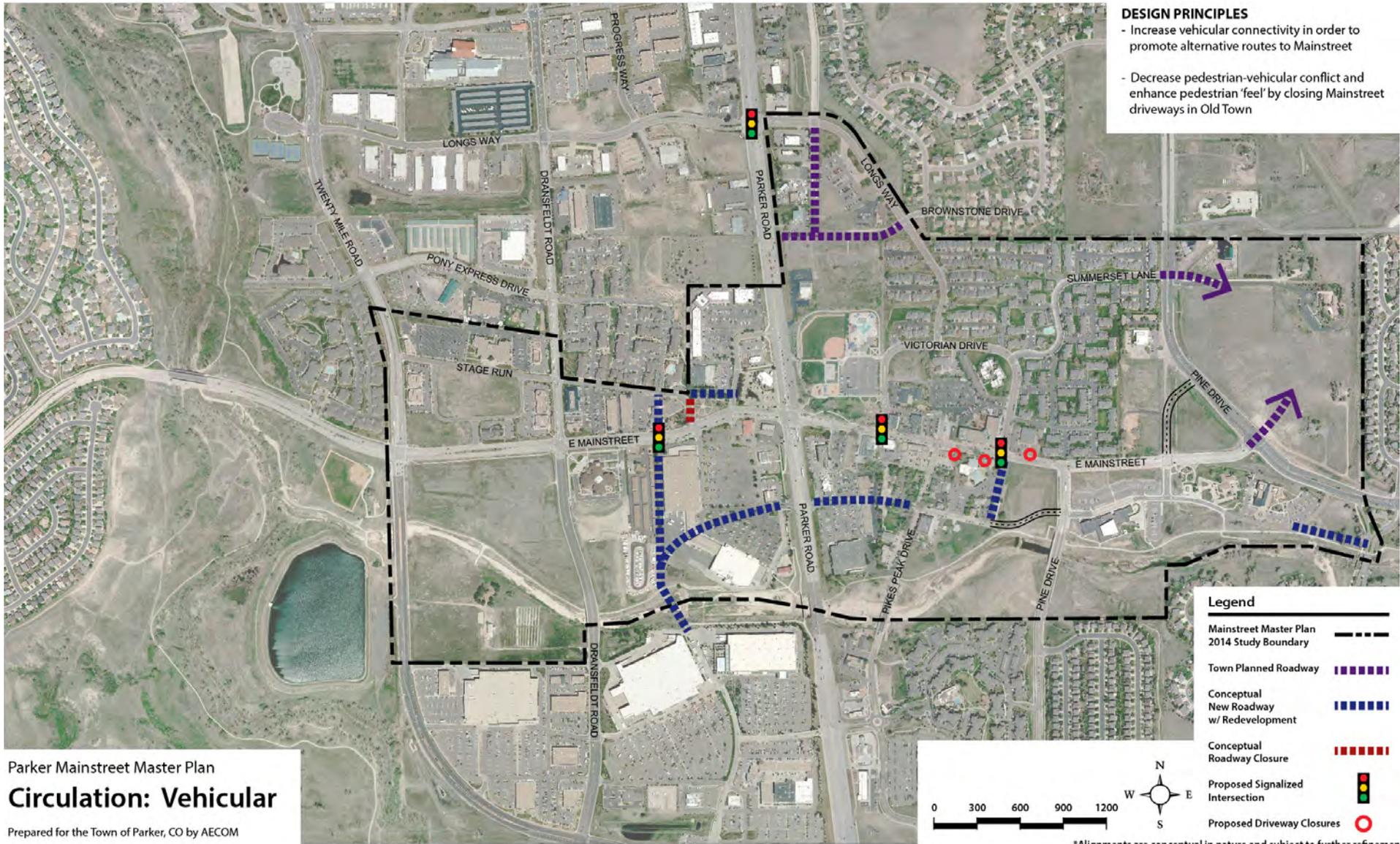
- An efficient alternate route to access the RTD Park-n-Ride, for both buses and private vehicles;
- A more intuitive and alternate east west route into Old Town from visitors arriving from southbound Parker Road; and
- Would also allow the introduction of a primary bicycle route paralleling Mainstreet on the north.

Finally, new signals should be considered at both ends of Victorian Drive. These signals will help to better manage pedestrian movements, particularly as the eastern leg is converted from a three-way to four-way intersection.

Recommendation 5.7: Signalize the Longs Way and Parker Road intersection.

Recommendation 5.8: Signalize the intersections of Mainstreet and Victorian Drive.

Recommendation 5.9: In coordination with redevelopment, remove the existing signal at Mainstreet and Stage Run in tandem with the installation of the proposed Briargate Lane signal.



Parker Mainstreet Master Plan
Circulation: Vehicular

Prepared for the Town of Parker, CO by AECOM

Figure 34 Circulation: Vehicular

The introduction of new roadways and roadway extensions focus on promoting parallel route options to the Mainstreet corridor.

Promoting Alternate Routes

As one of only a few east-west through-routes within the greater Parker area, Mainstreet carries a large amount of traffic not destined for West End or Old Town shops and restaurants.

Lincoln Avenue, 1.25 miles to the north, and the Twenty Mile Road/Hilltop Road combination, approximately 0.6 miles to the south, are the closest alternate routes to Mainstreet. Although neither of these routes provide direct east-west connectivity to the concentration of homes between Hilltop and Canterbury Parkway in the same way that Mainstreet does, increasing levels of congestion demand make it more and more important that motorists seek alternate routes. As congestion reaches a certain level, the most direct route, i.e. Mainstreet, may in fact not be the fastest route. The Town should work with other local agencies, including the Chamber of Commerce, to promote the use of alternate routes to Mainstreet for everyday travel as well as during special events. Promotional materials for downtown businesses and events should also provide directions that utilize routes other than Mainstreet to the greatest degree possible.

Alternate routing should also be included in the Town's current signage and wayfinding effort, so that directional signage to some destinations may direct motorists to these alternate routes in place of Mainstreet.

Recommendation 5.10: *Use marketing and promotional materials for downtown campaigns and events to promote alternate, non-Mainstreet routes for arriving downtown.*

Recommendation 5.11: *Use regional and local directional signage to promote alternate routes to Mainstreet, particularly for motorists driving to non-Mainstreet destinations.*

Additional Vehicular Concepts

Old Town Couplet: One-Way Conversion

A one-way couplet for Mainstreet and Victorian Drive, in which Mainstreet would carry eastbound vehicles and Victorian Drive Westbound, has been proposed as a potential solution to Old Town's existing vehicular congestion. This proposal focuses primarily on vehicular mobility, but must also consider impacts to pedestrians, cyclists and local business, as discussed below.

Although much theory and conflicting research has been produced regarding vehicular efficiency and pedestrian safety of one-way versus two-way roadways, the current prevailing national trend is converting one-way streets to (or back to) two-way operation. At mid-century, when many two-way streets were converted to one-way operation or new infrastructure was designed as one-way pairs, the primary concern of towns and cities was vehicular efficiency. One-way streets allowed higher travel speeds and increased roadway capacity.

Recent decades, however, have seen a more holistic evaluation of the pros and cons of one-way streets. While increased vehicular speed and capacity may be appealing and even appropriate in some situations, there are other contexts –more specifically pedestrian-focused retail downtowns like Parker–where this efficiency is not desirable. First and foremost, the reduction of the visual 'friction' produced by opposing traffic promotes both less attentive and higher-speed travel, a combination

generally acknowledged as a potential danger to pedestrian and bicycle safety. Though the Town would not increase the speed limit on Mainstreet, higher speeds could be an unintended consequence of a one-way couplet. Less measurable but also important is pedestrian experience; generally associated with highly urbanized downtown areas, one-way streets can create a very different feel than the 'hometown' or 'main street' that is so often cited by residents as the very heart of Parker character. Certainly, one-way streets communicate a very definite hierarchy, one in which vehicular convenience is paramount above all other modes, including pedestrians and bicycles.

Higher speeds and single directional travel also reduce business visibility and access, a critical concern for small-scale storefronts like those found in Parker. Ease of orientation and intuitive movement are also compromised, another consideration for a town that focuses equally on local residents and outside, regional visitors.

Instead of a character-changing conversion to one-way streets, this plan recommends that the Town actively promote alternative and parallel routes for motorists whose end destination is not Mainstreet. Twenty Mile Road/Hilltop Road are an alternative for some origins and destinations; the recommended signalization of Parker Road and Longs Way would provide additional flexibility.

Recommendation 5.12: *Maintain existing two-way function of Mainstreet.*

Recommendation 5.13: *Promote Twenty Mile Road and Longs Way as alternative, parallel routes to Mainstreet.*



Bicycle Circulation

Public outreach indicated a strong interest in biking to and from destinations in the downtown area; the biggest barrier, however, was cited as a lack of appropriate bicycle facilities (dedicated bike lanes, pavement markings, bike racks, signage etc.). Specifically, the majority of respondents indicated that they were not comfortable biking in regular vehicular traffic, and that they would prefer dedicated bicycle facilities to increase their level of comfort.

In a well-known paper published in 2005, titled 'Four Types of Cyclists', City of Portland Bicycle Coordinator Roger Geller identifies four broad categories of people who bike for transportation. These categories relate to level of comfort riding in mixed multi-modal traffic, and are correlated with types of facilities most favored by each group.

The four types of cyclists are:

- Strong and Fearless : <1% of the population; biking is part of personal identity, undeterred by any roadway condition
- Enthused and Confident: 7% of the population; ride regularly, often bicycle commuters, appreciate but don't require dedicated bicycle facilities
- Interested but Concerned: 60% of the population; like bicycling, but are afraid to ride
- No Way No How: 33% of the population; not interested in biking at all and for a variety of reasons (topography, inability, lack of interest)

Most communities target their bicycle facilities at the third group, and indeed this is where the largest gains can be had, the most people attracted to step out of their vehicles and trying cycling for short around Town trips. This is also the group that is most likely to be swayed by the introduction of dedicated facilities and amenities that make them feel safer. Parker is no different in this respect, and should continue to concentrate on introducing bicycle facilities that are family-friendly - especially in the Mainstreet corridor.

Aside from introducing comfortable, dedicated facilities, the Town should also focus on providing a safe place to park the bike when not in use, and on programs that increase the communities awareness of and comfort with bike facilities.

Pilot Projects

Pilot projects offer an excellent way to clarify demand and function for both type and location of facilities. Pilot projects do entail more than simply throwing down some paint or temporary barriers to delineate a bike lane; they also require sufficient time for true evaluation, comprehensive evaluation metrics and a formal outreach strategy.

A generally accepted minimum test period is one year. Projects in other cities have noted that it takes at least a month for users of all modes to get used to the changes, and the ability to test the design in all four seasons. This extended test period also allows time to really get the word out about the project - the critical third piece of the pilot - and to test it during special events as well as everyday conditions.

It is absolutely critical that facilities be evaluated on a broad set of metrics that includes not only transportation data but also user perceptions and economic impacts to adjacent businesses. Some metrics that may be used (not a comprehensive list) include:

- **Multi-modal volumes** (by mode, peak and off-peak)
- **Multi-modal operations, safety and conflict**
- **Multi-modal compliance with traffic regulations and signs**
- **Cyclist demographics** (age, gender, group size, vulnerable groups)
- **Economic impacts on adjacent businesses**
- **Stakeholder perceptions of facilities** (all modes)
- **Parking implications**

***Recommendation 5.14:** Use pilot projects to introduce new bicycle facilities in a measurable time frame and to fine-tune and test facilities.*

***Recommendation 5.15:** Leave pilot projects in place for a minimum of one year; support pilots with appropriate outreach and well-thought out evaluation metrics.*

Bicycle Routes and Facilities

This plan promotes the use of alternate, parallel routes to Mainstreet for both vehicles and bicycles. To the south, the Sulphur Gulch multi-use trail offers excellent connectivity in this regard, and should be better connected to Mainstreet via north-south bicycle facilities on Dransfeldt Road, Pikes Peak Drive and PACE Center Drive. Pine Drive south of Mainstreet already provides this connection via a striped, on-street bike lane.

Recommendation 5.16: Provide ‘spur ‘ north-south connections for bicycles between Sulphur Gulch Trail and Mainstreet on the following roadways: Dransfeldt Road, Pikes Peak Drive, PACE Center Drive.

Sulphur Gulch and Longs Way/Plaza Drive

To the north, there are two options for a parallel bike connection: Longs Way is recommended, but requires the installation of the Longs Way/Parker Road signal recommended in an earlier section of this chapter. A viable second choice, already with a signal at Parker Road, is Plaza Drive.

Although approximately a half-mile from Mainstreet, Longs Way traveling west offers excellent potential to increase non-motorized connection into the residential neighborhoods immediately west of Cherry Creek. An existing maintenance/access easement, owned by Parker Water and Sanitation, between the Clarke Farms homes flanking the open space provides opportunity to create this connection and aligns perfectly with the Railbender skate park and tennis courts trail system and across to Longs Way. To pursue this new connection, the Town should work

with appropriate authorities to ensure protection of any potentially vulnerable wildlife, such as Prebles Jumping Mouse, which may be present. Continuing east and across Parker Road, Longs Way feeds directly into Victorian Drive and right into the heart of Old Town as described in the following sections.

Recommendation 5.17: Construct an east-west trail connection over Cherry Creek between Clarkeville Way and Twenty Mile Road.

Recommendation 5.18: When the Parker Road/Longs Way signal is installed, delineate an east-west bike lane along Longs Way connecting the Cherry Creek Trail to Old Town with buffered bike lanes from Twenty Mile Road to Brownstone Drive and bike lanes from Brownstone Drive to Victorian Drive.

Longs Way: Twenty Mile to Dransfeldt

Long’s Way exhibits two different cross-sections between Twenty Mile and Parker Roads. The eastern portion of this segment, from Twenty Mile to Dransfeldt, features a 44-foot curb-to-curb cross-section with on-street parking on both sides of the roadway and one travel lane in each direction. The low-impact approach to bicycle facilities on this roadway would eliminate parking on both sides and use this recovered space to provide a generous 7-foot bike lane with a 3-foot buffer. The buffer could be simple striping, bollards or a narrow raised median; each of these approaches have different maintenance implications, particularly in terms of snow clearing. Curbs would not move, and all improvements would still fit within the 44-foot cross-section. An enhanced version of this approach would use the same curb-to-curb cross-section, but would install pedestrian lights, street trees and a widened sidewalk behind the curb, in order to provide a finer-grain, more pedestrian-scaled street. These additional improvements could also be expected to provide some level of traffic calming by changing the character of the street.

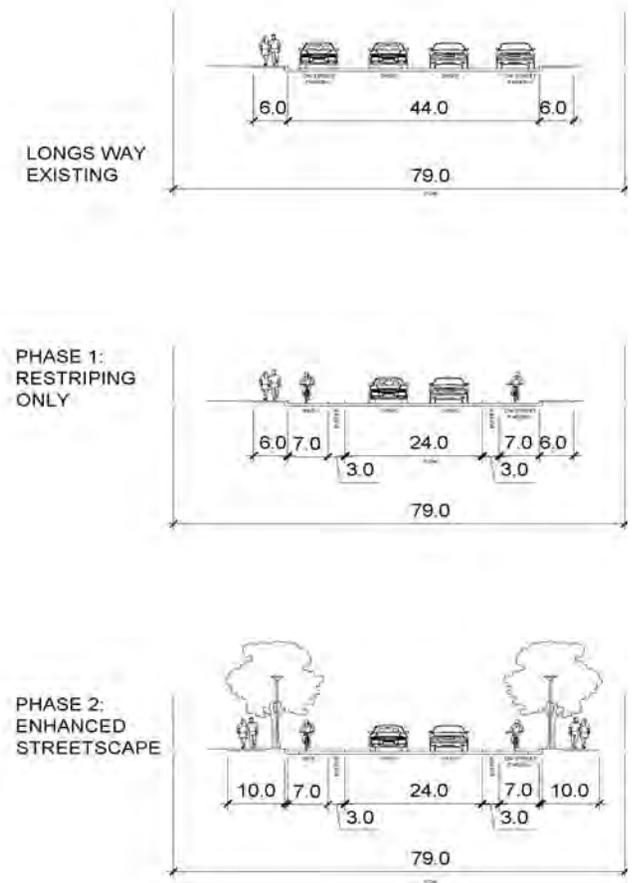
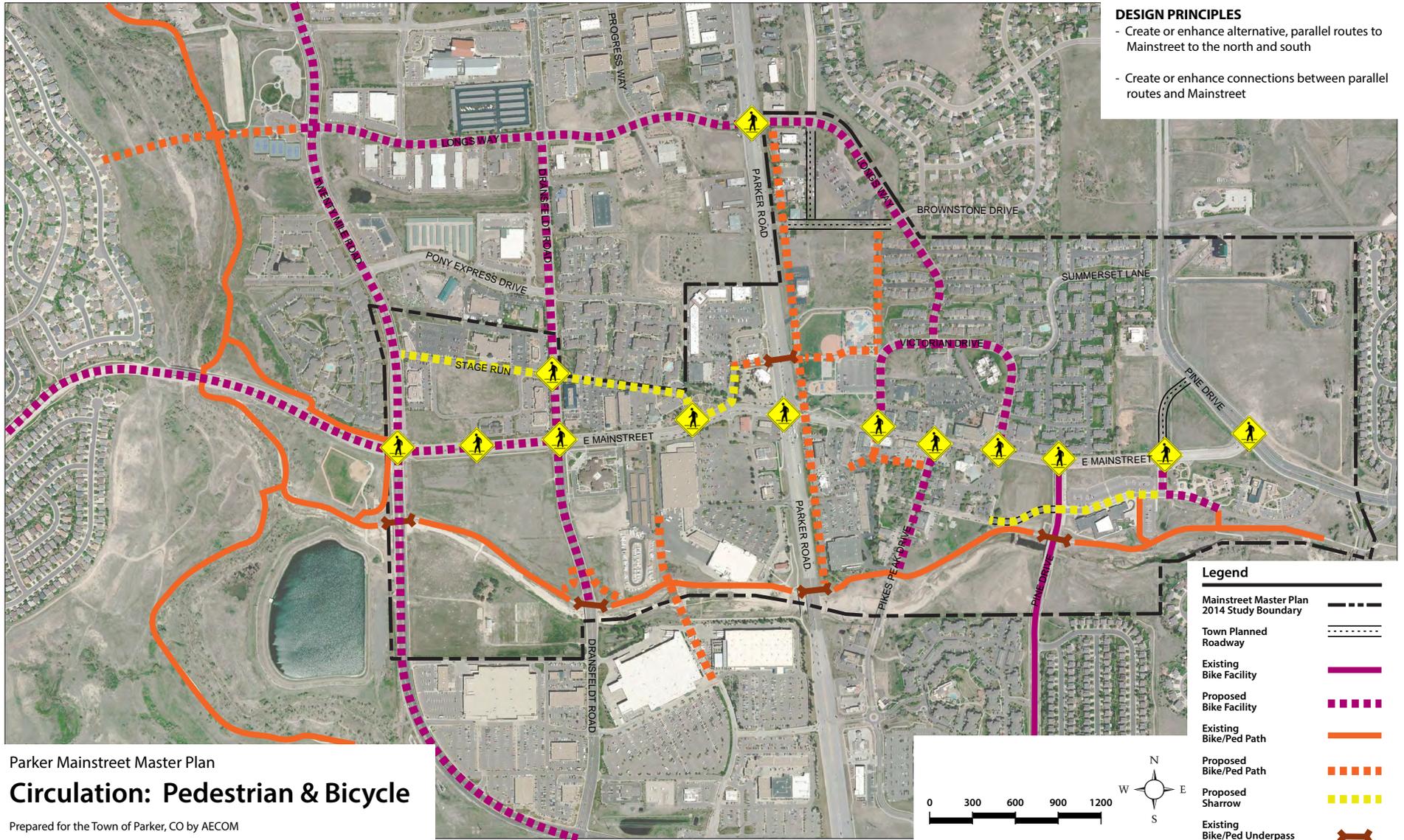


Figure 35 Cross Sections: Longs Way 20 Mile to Dransfeldt



- DESIGN PRINCIPLES**
- Create or enhance alternative, parallel routes to Mainstreet to the north and south
 - Create or enhance connections between parallel routes and Mainstreet

Parker Mainstreet Master Plan
Circulation: Pedestrian & Bicycle

Prepared for the Town of Parker, CO by AECOM

Figure 36 Circulation: Pedestrian & Bicycle
An extensive system of new bicycle and pedestrian facilities aims to promote multi-modal choice.

Longs Way: Dransfeldt to Parker Road

Between Dransfeldt and Parker Roads, Long's Way currently features two travel lanes in each direction separated by a 14-foot wide raised median. No on-street parking is provided, and sidewalks are attached. Bicycle facilities could be introduced by removing a vehicular lane in each direction and restriping this space for the 7-foot bike lane and 3-foot buffer proposed on the eastern segment of Long's Way. The center median is retained to provide lane continuity and to maintain curb location, thereby keeping implementation costs low. Similarly, enhanced pedestrian facilities could be an optional 'Phase 2' enhancement.

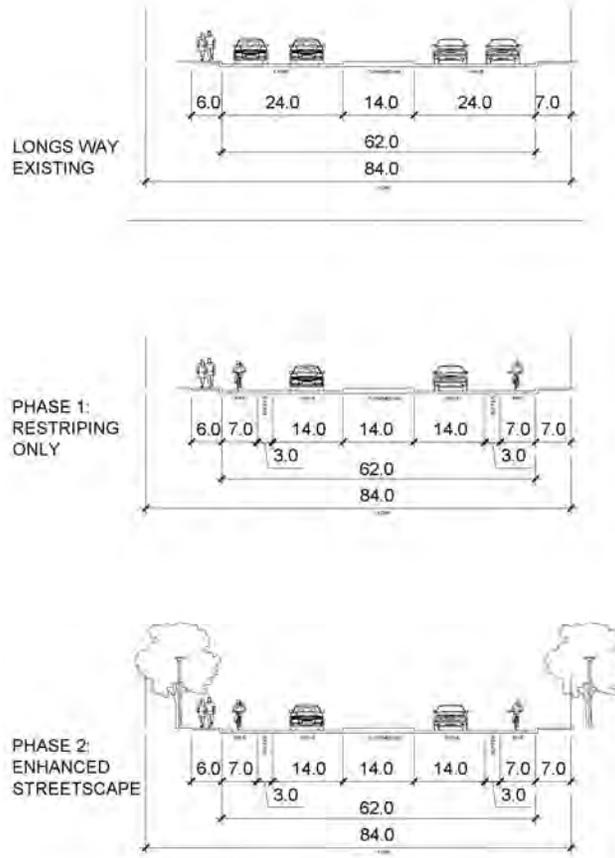


Figure 37 Cross Sections: Longs Way Dransfeldt to Parker Road

Longs Way: Parker Road to Senior Center

On the east side of Parker Road, Longs Way has two different sections; these sections are based on variations in the public right-of-way. Between Parker Road and the Parker Senior Center, Longs Way occupies a 48-foot curb-to-curb section; pavement division is unmarked but generally functions as one travel lane in each direction and on-street parking on both sides of the street. The roadway is not centered within the right of way but instead abuts the east side of the public corridor.

Striped bike lanes can be introduced by better defining the travel lane width and location. The new cross section would maintain curb-to-curb width and have one travel lane in each direction, a 6-foot striped bike lane on each side protected by a 3-foot buffer, and on-street parking on the west side only. Although parking could be on either side, the west side is recommended to provide easier access to the Parker Senior Center. As on the other side of Parker Road, streetscape enhancements behind the curb could be considered as a value-added, second phase.

Longs Way: Parker Senior Center to Victorian

From the Senior Center south to Victorian Drive, Longs way occupies a 53-foot right-of-way with a 34-foot curb-to-curb section. The sidewalk is notably narrow on the west side and overly wide on the east side, at five and 14 feet respectively. New bike lanes maintain the 6-foot width introduced in the abutting northern segment of Longs Way, but drop the buffer in order to fit into the more constrained right-of-way. No parking is provided. Enhanced streetscape can be considered, with the west side requiring property acquisition or easement from the private property owners.

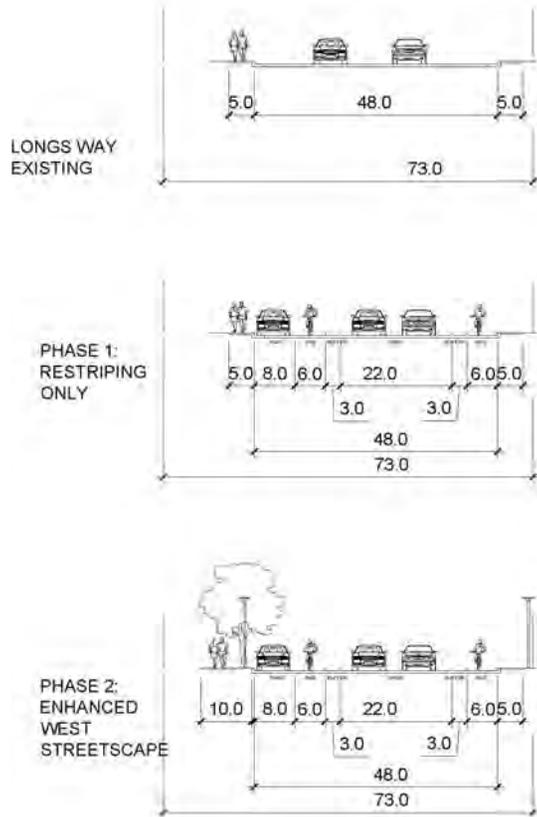


Figure 38 Cross Sections: Longs Way Parker to Senior Center

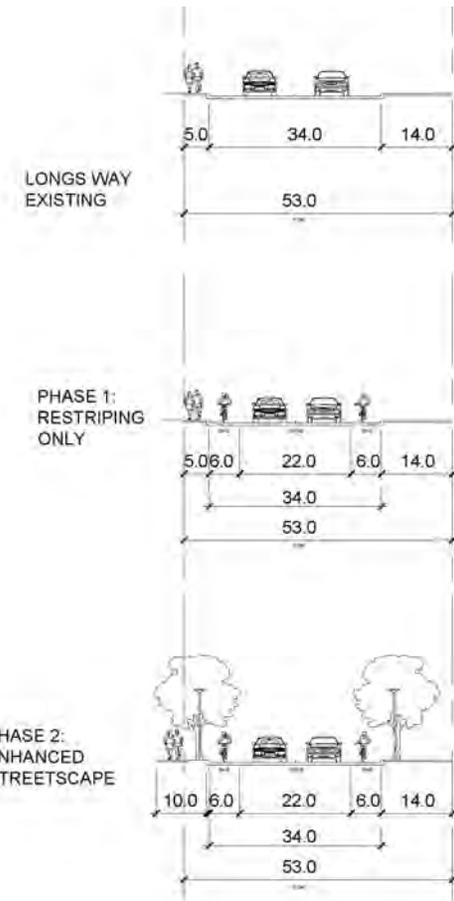


Figure 39 Cross Sections: Longs Way Senior Center to Victoria

Victorian Drive

Victorian Drive features a varied right-of-way that fluctuates between 55 and 65 feet in width. Due to this variation, the roadway will need to be fully redesigned to determine the configuration of facilities at any specific point. This Master Plan provides three cross sections at three different widths. Victorian Drive's proximity to Mainstreet suggests a priority for enhanced pedestrian facilities, and widened sidewalk and streetscape are illustrated in each of the three sections.

The narrowest section includes a single travel lane in each direction and 6-foot striped bike lanes. No parking is provided. This section requires 34 feet curb-to-curb, and an overall right-of-way (including enhanced pedestrian zone) of 59 feet.

The medium-width section uses the laneage and dimensions established in the narrow section, and adds on-street parking on one side of the roadway. This section requires 42 feet curb to curb, and an overall right-of-way (including enhanced pedestrian zone) of 67 feet.

The wide or 'ideal' section expands the medium section to include on-street parking on both sides. This section requires 50 feet curb to curb, and an overall right-of-way (including enhanced pedestrian zone) of 75 feet.

It is important to note that the introduction of a bike lane will also require reconfiguration of the diagonal parking directly east of Parker Station on the west side of the roadway. Motorist's movement required to enter and exit diagonal parking, whether head-in or rear-in, are potentially dangerous to cyclists and incompatible with a bicycle facility.

Recommendation 5.19: *Introduce striped on-street bike lanes along the entire length of Victorian Drive.*

Recommendation 5.20: *Convert existing angle parking on the east side of Parker Station to parallel parking in order to allow for recommended bike lanes.*

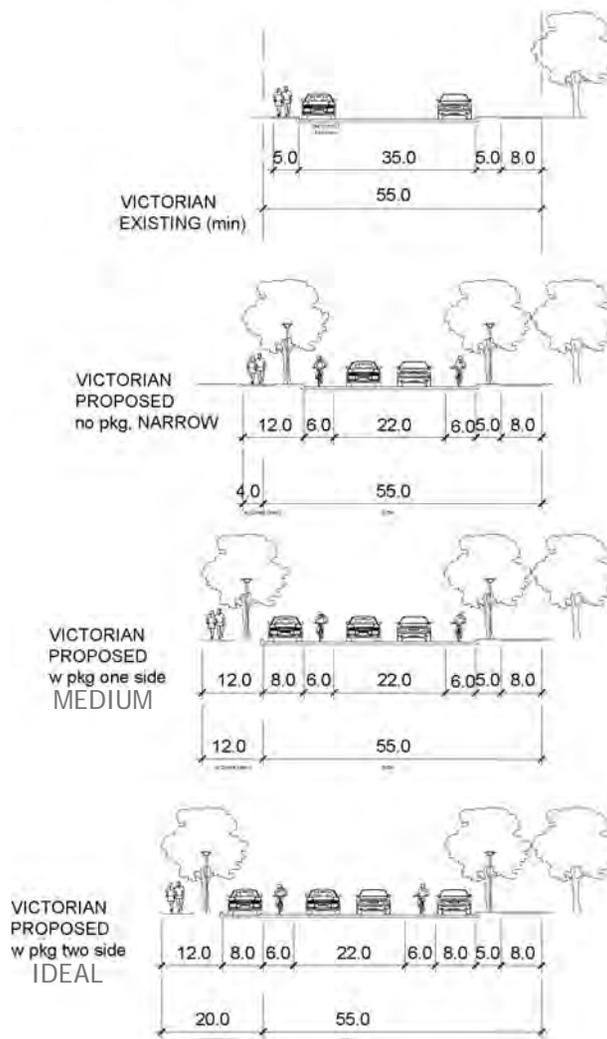


Figure 40 Cross Sections: Victorian Drive

PACE Center Drive

PACE Center Drive will serve as an important connection between Sulphur Gulch Trail and the new Parker library, as well as the PACE Center itself. With the library as a primary destination on this segment, demographics can be expected to lean even more towards children and families; for this reason, an off-street, multi-use trail is appropriate on the east side of PACE Center Drive and will provide a higher degree of comfort for users.

The existing roadway features three travel lanes (one extra-wide southbound lane and two average-width northbound lanes) in a 39-foot curb-to-curb section. Sidewalk on both sides is a generous nine feet. It should be noted that parcels on both sides are Town-owned, and that underground utilities on the western side prohibit street tree plantings. Low-impact introduction of bicycle facilities maintain existing curb-to-curb cross-section. Again noting the strong destination draw of the new library, the eastern sidewalk should be reconfigured to introduce an amenity zone and 12-foot shared use path that connects to the existing spur from the Sulphur Gulch Trail. The western sidewalk may be enhanced with planters and pedestrian lighting. Both concepts are illustrated on the following page.

Recommendation 5.21: *Introduce an off-road, shared-use path on PACE Center Drive.*

Pine Drive - South of Mainstreet

Pine Drive south of Mainstreet already provides an on-street bike lane connecting Sulphur Gulch, the southern neighborhood and Mainstreet. No additional improvements are proposed.

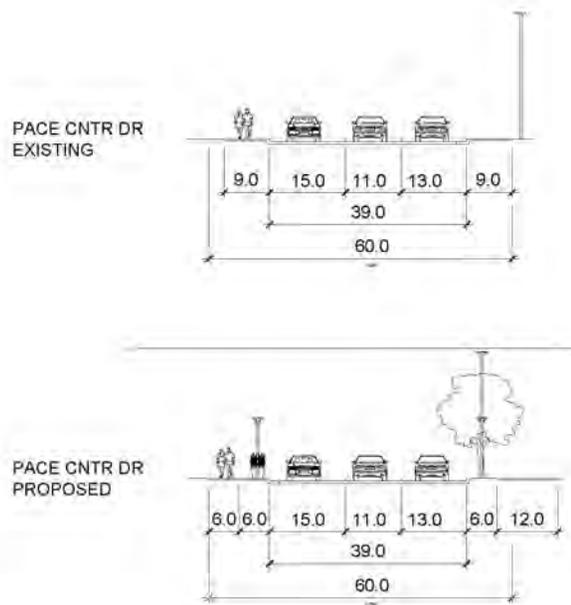


Figure 41 Cross Sections: PACE Center Drive

Dransfeldt Road Sulphur Gulch to Mainstreet

Between Mainstreet and the Sulphur Gulch Trail, Dransfeldt features 65 feet of pavement in an 80-foot right-of-way. In order to align facilities on both sides of Mainstreet, this southern segment of Dransfeldt should also utilize an off-road, shared use path on the west side. Existing west-side sidewalk would be removed and reconfigured as a 12-foot shared-use path with a planted amenity zone against the curb. Like the northern segment, this configuration provides a more-comfortable facility for unsure cyclists (in light of the speed and volume of the adjacent roadway), maintains curb location and results in lower implementation costs.

Recommendation 5.22: *Introduce an off-road, shared use path on the west side of Dransfeldt Road between Mainstreet and the Sulphur Gulch Trail.*

Mainstreet to Long's Way

Dransfeldt will act as an important connector to the West End, from the new Long's Way bike lane and from Sulphur Gulch. North of Mainstreet, Dransfeldt has 50 feet of pavement within an 80-foot right-of-way. The roadway is offset and squeezed against the east side of the corridor, resulting in a narrow five-foot attached sidewalk on the east side. Due to the volume and speed of traffic on Dransfeldt, an off-street facility is likely to be more comfortable for the intended cycling demographic; this approach has the additional advantage of maintaining existing curb locations and thus resulting in significantly lower installation costs. Existing west-side sidewalk would be removed and reconfigured as a 12-foot shared-use path with a planted amenity zone against the curb.

Recommendation 5.23: *Introduce an off-road, shared use path on the west side of Dransfeldt Road between Mainstreet and Long's Way.*

DRANSFELDT: 80' ROW S of Mainstreet

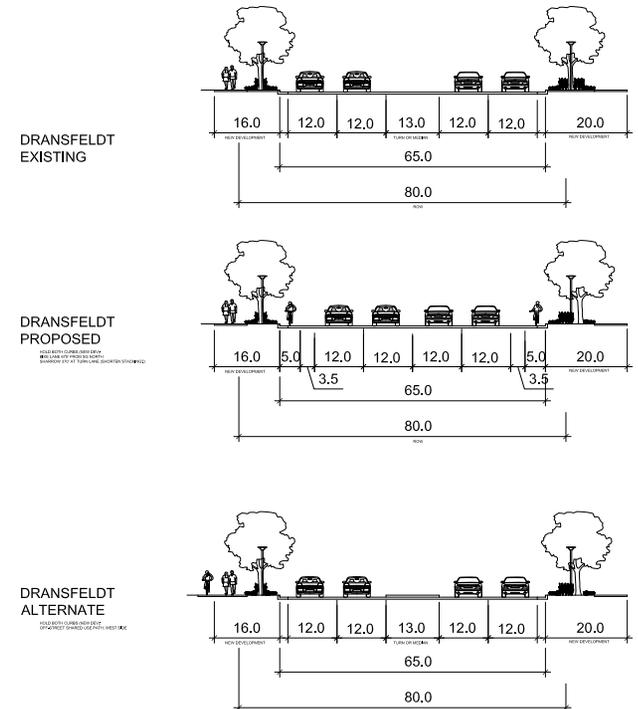


Figure 42 Cross Sections: Dransfeldt

Stage Run

Stage Run is a private street but offers the opportunity for additional connectivity with the Kieffer's Crossing underpass. With a single lane in each direction, no on-street parking and a correspondingly narrow right-of-way, Stage Run cannot accommodate a dedicated bicycle facility of any kind. Generally used only on low-volume, low-speed local roadways, shared-use arrows ('sharrows') may be appropriate on this street. Their function is primarily wayfinding for cyclists and alerting motorists to the presence of cyclists. Sharrows are decals applied to the roadway surface, showing a stylized cyclist topped by two chevrons in the direction of travel, and may be complemented by sign-based bicycle signage.

Recommendation 5.24: Consider introducing sharrows on Stage Run (a private street).



Mainstreet Twenty Mile to Dransfeldt

Lone Tree has proposed a multi use trail on the south side and a cycle track on the north side of Ridgeway Parkway (which changes name to Mainstreet at the eastern edge of Lone Tree) between Interstate 25 the City of Lone Tree Boundary. Douglas County has constructed a multi use trail on the south side of Mainstreet. Continuing this 12' wide multi use trail on Mainstreet from Chambers to Dransfeldt Road allows cyclists additional access to West End destinations, particularly the new apartment complex situated between Twenty Mile and Dransfeldt, the existing Twenty Mile shopping area to the south and to the ramp access to Sulphur Gulch Trail.

Recommendation 5.25: Continue the future Ridgeway/Mainstreet multi-use trail on the south side of Mainstreet from Twenty Mile Road to Dransfeldt Road.

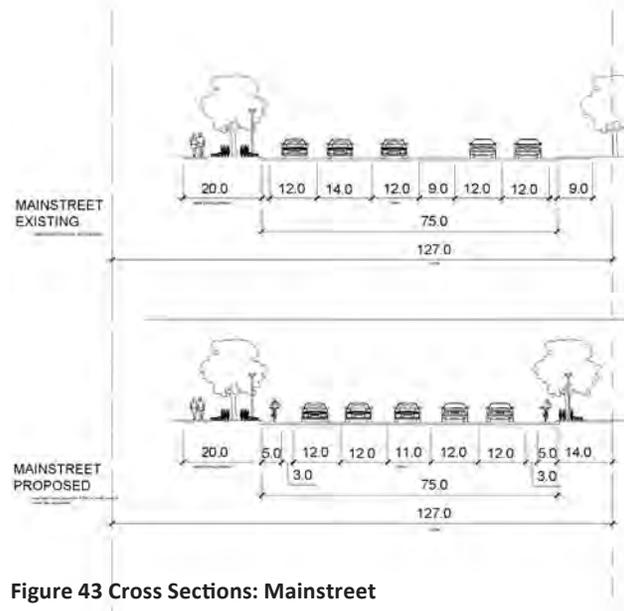


Figure 43 Cross Sections: Mainstreet



Pikes Peak Drive

Pikes Peak Drive occupies an extremely narrow 50-foot right of way with a 40-foot curb-to-curb section. The roadway's narrow right-of-way leaves only five feet on either side for a narrow attached sidewalk, inappropriate to a pedestrian-oriented downtown shopping and dining zone. In order to fulfill the town's vision of a vibrant, multi-modal downtown district, this street more than any other has a need not just for bicycle facilities but also for an expanded pedestrian zone, neither of which are achievable individually, much less together, in the existing right-of-way. These roadway enhancements are of particular priority due to the heavily used Sulphur Gulch Trail access onto Pikes Peak Drive leading into the downtown area.

The proposed drawing illustrates an ideal cross section that balances pedestrian comfort and bicycle comfort with the need for on-street parking to support adjacent small businesses. The new cross-section requires a minimum nine foot property acquisition, assumed to be on the west side of the roadway in order to preserve the enhanced streetscape already present in front of Victorian Peaks on the east side. The new cross section removes parking on one side

and widens the pavement by two feet in order to accommodate two six-foot bike lanes. Minimum bike lane width adjacent to the curb is five feet, in order to allow shy distance from the longitudinal seam of the gutter pan, but six feet is proposed in order to acknowledge the narrowed 11-foot vehicular lanes. Likewise, four feet is considered minimum width for a bike lane between traffic and vehicular parking but is illustrated here as five feet due to narrow travel lane and anticipated high volume of parking turnover. In addition, the Town may wish to consider locating the east bike lane between the parking lane and the curb, a configuration gaining popularity in some cities but typically accompanied by an additional buffer area between the parked car and the bicycle lane, in order to allow for door swing.

The proposed bike facility on Pikes Peak Drive should be completed in phases due to the impact on adjoining properties, parking and trees created by the preferred section. Phase 1 should include sharrows and a widened sidewalk.

Phase 2 will be a long term outcome that requires significant consideration of the impacts on parking, street character and adjoining properties.

Recommendation 5.26: *Introduce improved wayfinding signage, on-street sharrows and widened sidewalks on Pikes Peak Drive.*

Recommendation 5.27: *Study and consider a long term outcome that requires acquiring additional right-of-way, removing on-street parking on one side and introduce on-street bike lanes with improved sidewalks on Pikes Peak Drive.*

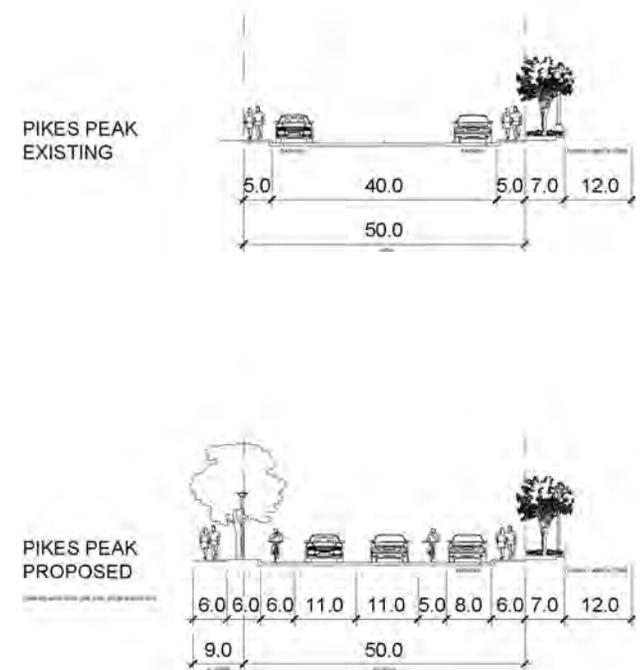


Figure 44 Cross Sections: Pikes Peak Drive

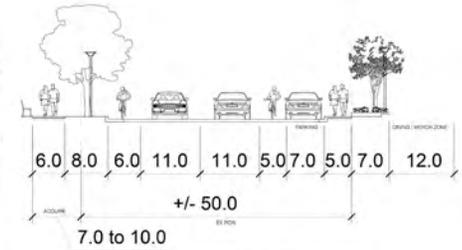


IMPACT SUMMARY
 -BACK OF SIDEWALK EXTENDS 7' TO 10' INTO PRIVATE PROPERTY ON WEST SIDE OF PIKE'S PEAK DRIVE
 -BACK OF SIDEWALK MAINTAINS EXISTING BOUNDARY ALONG PIKE'S PEAK CT
 -PROPERTY FENCE MUST BE REMOVED/MOVED ON WEST SIDE OF PIKE'S PEAK DR. AT PARCEL 1
 -TWO PARKING SPACES IN THE CHAMBER OF COMMERCE PARKING LOT MUST BE REMOVED
 -BIKE LANES ADDED TO PIKE'S PEAK DR
 -13 MATURE TREES MUST BE REMOVED ON WEST SIDE OF PIKE'S PEAK DR
 -STREET PARKING REMOVED ON WEST SIDE OF PIKE'S PEAK DR AND NORTH SIDE OF PIKE'S PEAK COURT (REDUCTION OF +/- 65 SPOTS)
 -PIKE'S PEAK COURT SAME AS OPTION A
 -BENCHES ADDED

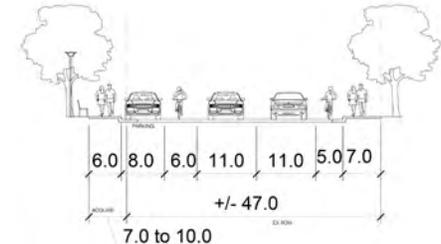
- KEYED NOTES PIKE'S PEAK DR, CT IMPROVEMENT CONCEPT**
1. BACK OF SIDEWALK EXTENDS 7' TO 10' TO WEST (TYP)
 2. 5' BIKE LANE ON EAST SIDE*
 3. 6' BIKE LANE ON WEST SIDE*
 4. 7' STREET PARKING ON EAST SIDE, SOUTH OF PIKE'S PEAK CT*
 5. 5' SIDEWALK ON EAST SIDE, SOUTH OF PIKE'S PEAK CT
 6. 6' SIDEWALK AND 8' TREE LAWN ON WEST SIDE
 7. 7' SIDEWALK ON EAST SIDE, NORTH OF PIKE'S PEAK CT
 8. 6' SIDEWALK
 9. 6' TREE LAWN
 10. STREET PARKING ON SOUTH SIDE
 11. 11' DRIVE AISLE (TYP)
 12. FLOWLINE (TYP)
 13. EXISTING TREE REMOVAL (TYP)
 14. TRANSITION TO EXISTING SIDEWALK
 15. STORMWATER INLET
 16. BICYCLE SHARROWS (TYP)
 17. CURB AND GUTTER
 18. (NOT USED)
 19. PROPOSED STREET TREE (TYP) @ +/-30' O.C.**
 20. PROPOSED PEDESTRIAN LIGHT (TYP)
 21. PROPOSED BENCH (TYP)
 22. (NOT USED)
 23. RIGHT-OF-WAY (TYP)
 24. BUILDING FOOTPRINT (TYP)
 25. REMOVE FENCE
 26. BIKE RACKS
 27. STOP SIGN

*DOES NOT MEET PARKER MINIMUM STANDARDS.
 **ADJUSTED FOR STREET LENGTH

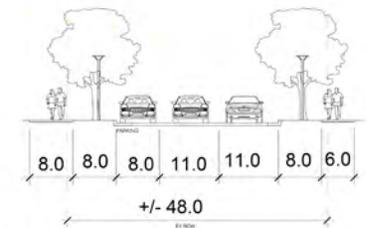
PLEASE NOTE: DIMENSIONS IN THIS DRAWING ARE BASED ON GIS DATA AND AERIAL PHOTOGRAPHY. ACTUAL DIMENSIONS MUST BE VERIFIED.



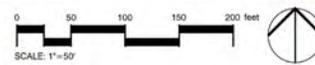
1 PIKE'S PEAK DRIVE SECTION SOUTH
 SCALE: N.T.S.



2 PIKE'S PEAK DRIVE SECTION NORTH
 SCALE: N.T.S.



3 PIKE'S PEAK AVENUE SECTION
 SCALE: N.T.S.



EXPANDED R.O.W. OPTION

Figure 45 Conceptual Streetscape Design Pike's Peak

This conceptual streetscape design shows the 'ideal' Pike's Peak section described on the previous page applied to existing conditions.

Bicycle Parking

Formal bicycle parking provides benefit to the downtown as a whole, not just the cyclist, by preventing damage to street furnishings and trees from bikes locked to them, maintaining a tidy streetscape appearance, and preventing haphazardly parked bikes from falling over and blocking sidewalks or businesses. It also sends a clear message to cyclists that they are a welcome and respected part of the downtown circulation system.

Type

The first consideration of bicycle parking is type. For the short-term parking (typically two hours or less) that can be expected in downtown Parker, bike racks are an appropriate choice; covered parking such as bike lockers or indoor bike rooms may be appropriate at the Parker Park-n-Ride or apartment complexes but are not necessary within the public right-of-way.

Racks should be selected to meet the following criteria:

- The rack should support the bike in at least two places to prevent it from falling over and allow the rider to use both hands to lock the frame or unload cargo.
- The rack should be configured so that a standard u-lock can be used to lock the frame and at least one wheel to the rack. This requirement must also be achievable for bicycles without a top tube, such as ladies' step-through bicycles.
- The rack should be securely and permanently anchored to the ground.
- The rack should resist cutting, bending or other vandalism.

Recommended bike rack styles for downtown Parker are the 'inverted U' and artistic bike racks.

Recommendation 5.28: Designate 'inverted U' racks as the standard for downtown bicycle parking and encourage/incorporate artistic bike racks.

Location

Racks can be placed individually within the streetscape right-of-way, or can be grouped into a bike corral. Bike corrals are an excellent way to provide a large amount of bike parking at popular bike destinations, and can be located within or outside of the right-of-way. Many cities have used on street parking spaces to create bike parking; a single vehicular parking space can park 20 bikes. These types of projects typically mark the corral with signage and physical barriers that protect bikes and prohibit 'poaching' by cars.

Recommended, potential locations for a bike corral in Old Town include the southwest and northeast corners of Mainstreet and Pikes Peak Drive and the northwest corner of the eastern intersection of Mainstreet and Victorian Drive (east of Parker Station). The bike corral on the northeast corner of Mainstreet and Pikes Peak Drive would be contemplated within the grey concrete area between diagonal parking and the asphalt/grassy area. The bike corral on the southwest corner of corner of Mainstreet and Pikes Peak Drive would be contemplated on the east side of the Hess Exchange Building where the visitors center is. In order to accommodate a bike corral at the northwest corner of Mainstreet and Victorian Drive, parking in this last location must be converted to parallel parking to allow the introduction of a bike lane; a bike corral directly adjacent to the bike lane creates an excellent synergy to ride to downtown, park the bike, and walk.

Whether located singly or grouped and in the right-of-way or on adjacent property, racks should be located to meet the following criteria:

- Rack locations should be convenient to adjacent destinations, typically no more than 50 feet from the door or riders may choose to lock their bikes to street furniture or trees.

Bicycle parking options include the 'inverted u' style rack (top), post and ring racks (middle). Racks may be used to convert on-street parking spaces to dedicated bike corrals (bottom).





image: autostraddle.com



image: examiner.com



image: nj.com

Community bicycle events like Denver’s Tour de Fat (above) and Moonlight Ride (middle) promote cycling, with or without costumes. Bicycle safety classes (bottom) promote good bicycling habits.

- Rack locations should be located in high-traffic areas with passive surveillance, preferably visible from inside adjacent buildings.
- When locating bike racks, the Town should also consider access to the racks during events and impacts on tent locations during common events such as the Farmer’s Market and Wine Walk.

When located in the amenity zone, racks should be:

- Oriented so that the bicycle does not project beyond the curb and into the roadway or into the pedestrian clear zone of the adjacent sidewalk.
- Installed parallel to the roadway and a minimum of 24 inches from back of curb so that riders are able to lock their bikes without danger of being struck by vehicles. Racks arranged in a line should be a minimum of 48 inches from each other or from adjacent obstruction such as trees or lights at their closest points; 72 inches is preferred to allow for easier circulation between racks. If parallel orientation is not possible and racks must be oriented perpendicular to the roadway, the nearest point of the rack should be a minimum of 24 inches from the back of curb, 36 inches preferred. Racks should be 36” from each other or other vertical obstructions such as trees or lights, 48 inches preferred for easier circulation between racks.
- A minimum of 36 inches clear from any adjacent, vertical structure (such as a wall).

Recommendation 5.29: Locate bike parking in high-visibility areas within 50 feet of destinations.

Recommendation 5.30: Install bicycle racks with sufficient clear distances from roadway, other racks, and adjacent vertical elements.

Recommendation 5.31: Consider installation of a bike corral within the core area of Old Town.

Bicycle Promotion

New bicycle facilities are only half the equation for getting more people on bikes; the other half is getting the word out! Guided tours of the new system and group rides such as Denver’s ‘Moonlight Classic’ or Pueblo’s ‘Tour de Taco’ rolling food tours build excitement and get people out on the bike network. Even ‘bicycling 101’ classes –though perhaps not directly related to new facilities but instead aimed at increasing citizen’s comfort riding bicycles—have been used with great success in other communities to promote cycling for both transportation and recreation.

It is also important to promote new facilities in order to open a dialogue about how the facilities are working. Bicycle networks tend to be dynamic systems that are rolled out in phases, with new pieces added as demand increases and existing pieces adjusted in response to user input. Advertising and encouraging the use of new facilities will ensure more comprehensive pilot projects, as described in the preceding section of this document.

Recommendation 5.32: Organize group rides and events to advertise and promote the use of new bicycle facilities.



Pedestrian Circulation

Downtown Parker already enjoys a pedestrian-focused environment within part of the Old Town area; the long-term vision of this plan is to extend that pedestrian character throughout the Old Town area and to the West End, thereby linking the two areas together. Many of the enhancements described in Chapter 5: Urban Form Recommendations will promote this expanded pedestrian character; this section describes additional, infrastructure-based improvements that will support these goals.

Pedestrian Enhancements Mid-Block Crossing

Current development of the new 306-unit apartment complex south of Mainstreet in the West End, coupled with the future plaza and commercial development on the north side, introduces a stronger pedestrian demand between both sides of Mainstreet between Twenty Mile Road and Dransfeldt Road.

The new apartment complex between Twenty Mile Road and Dransfeldt Avenue features a primary, mid-block entry point directly across from the existing, unnamed roadway accessing the movie theater complex on the north side of Mainstreet. In order to promote pedestrian safety, a pedestrian-activated mid-block crossing is highly recommended at this location. Depending upon observed demand once the apartments are occupied, the crossing may be introduced in the short-term or may be linked to development of the still-vacant parcels within the Stage Run area.

A HAWK (High-Intensity Activated crossWalk), also referred to as a Pedestrian Hybrid Beacon (PHB), should be installed at this proposed mid-block crossing in compliance with the guidelines contained in the Manual on Uniform Traffic Control Devices (MUTCD). The existing, approximately 20-foot wide planted median should be modified to provide ADA-compliant ramps on both sides and a level refuge area in the center. The pedestrian crossing should be aligned with the west side of the access to align with the proposed future pedestrian plaza.

Recommendation 5.33: *Introduce a mid-block pedestrian crossing on Mainstreet between Twenty Mile and Dransfeldt Roads in coordination with development of the park/plaza to the north.*

Pedestrian-Priority Intersections

Intersections with high volumes of pedestrians - all intersections in the Plan area - should also consider further enhancements to pedestrian safety and comfort. Conditions vary by intersection, but most intersections marked on the accompanying map, as pedestrian priority intersections already have adequate curb ramps and crosswalks; the Town should enhance existing crosswalks with higher contrast color or customized artistic designs aimed at improving visibility of the intersections. Where on-street parking is present (or introduced in the future), parking should terminate sufficiently far in advance of the intersection to allow for a 'bump-out' (aka, curb extension) to reduce crossing distance and increase pedestrian visibility. Other features that should be considered are raised or tabled intersections and embedded crosswalk lighting.

Pedestrian Enhancement to Parker Road/Mainstreet Intersection

At-grade pedestrian improvements to the Parker Road/Mainstreet Intersection are desirable, but complicated by Parker Road's status as a state highway subject to CDOT standards and policies. The usual toolkit of options to decrease pedestrian crossing distance and increase pedestrian comfort and safety are: curb bump-outs, raised crosswalks, median refuge, adjusted signal timing, and closure of free right turns (aka 'pork chops'). The Town should continue to work with CDOT to evaluate options to improve the pedestrian safety, comfort and convenience at this important gateway intersection.

Recommendation 5.34: *Introduce additional crosswalk enhancement to increase visibility, safety, comfort and convenience at priority pedestrian intersections.*

Recommendation 5.35: *Work with CDOT to develop pedestrian enhancements to improve the safety, comfort and aesthetics of the Mainstreet and Parker Road intersection.*



image: landscapeperformance.org



image: landscapeperformance.org

Cherry Creek's Fillmore Plaza before (top) and after (bottom) renovations. Tenancy and street activity has increased appreciably since the plaza became a multi-use roadway.

Sulphur Gulch Trail

The Sulphur Gulch Trail is a significant east west trail corridor that functions as a parallel route to Mainstreet for pedestrians and bicyclists. The Trail also crosses under Parker Road ¼ of a mile south of the intersection of Mainstreet and Parker Road providing a safe and comfortable crossing of Mainstreet. How this trail connects to the pedestrian circulation system in Mainstreet corridor is important. There is no pedestrian connection from the Mainstreet and Parker Road intersection on the east side of Parker Road and a non-descript sidewalk connection on the west side of Parker Road. Sidewalk connections and wayfinding signage should be improved to offer more options to pedestrians desiring to cross Parker Road.

Kieffer's Crossing Re-alignment and Enhancement

Less than a tenth of a mile north of Mainstreet, Kieffer's Crossing offers a safer, low-stress way for pedestrians and cyclists to cross Parker Road. The culvert underpass, however, is underutilized. The first issue is wayfinding; although small directional signs to the underpass do exist at Crossroads shopping center and O'Brien Park, few people know of its existence. Similarly, the nondescript character and narrow width of the sidewalk accessing the underpass provide no supporting wayfinding cues.

The second issue pertains to the alignment and context of the facility itself; aligned diagonally from southwest to northeast, a person entering either side of the tunnel cannot see through to the other end. This acute angle and lack of full visibility seriously compromises both the actual and perceived safety of the tunnel, potentially allowing concealment in the tunnel and discouraging its use. Banked topography and retaining walls at the eastern, O'Brien Park entry further this sense of hidden-ness. The tunnel should provide a full line of sight from one end to the other, and should have open, easily-viewed entrances on both sides. The east side should be aligned with

primary circulation paths in O'Brien Park; it may be necessary to relocate the park's major east-west pathway between the baseball field and basketball court in order to achieve this relationship.

Recommendation 5.36: Re-align Kieffer's Crossing to allow an unobstructed line of sight from one end to the other, reconfigure entries to integrate into adjacent context and provide more prominent wayfinding signage.

Additional Pedestrian Concepts Evaluated

Mainstreet Pedestrian Mall

Members of the public suggested the concept of Mainstreet becoming a pedestrian-only mall closed to traffic either all of the time or for programmed events. While the idea of a strolling pedestrian-only zone is appealing, past experience across the country has shown little success; market analysis shows that the visibility of vehicular traffic and the convenience of on-street parking are vital to the success of pedestrian-focused downtowns.

The Pearl Street Mall in Boulder, likely the inspiration for this idea, is in fact one of the few successful pedestrian malls in the country. Fillmore Plaza in Denver's Cherry Creek North district may be a more apt example; this one-block pedestrian-only plaza struggled with low occupancy and a 'dead' feel during non-event times, until the City undertook a complete

overhaul in 2011 which redesigned and reopened the space to vehicular traffic. The area is now thriving, and can still be closed for occasional special events.

As discussed in the preceding part of this chapter, Mainstreet is one of only a few corridors with east-west connectivity in Town. Permanent closures would have far-ranging impacts on the entire transportation network. Temporary closures need to continue to be well noticed and well thought out to mitigate automobile traffic impacts, not to mention economic impacts.

Pedestrian Bridge Considerations

Parker Road is a challenging divide between Old Town and the West End, and stakeholders and Town staff alike have wondered if a pedestrian bridge might be a solution to consider. Such overpasses can provide valuable connections across difficult natural and human-built features; they can also become very expensive and underutilized eyesores. The critical factors determining the success or failure of a pedestrian overpass relate to the type of obstruction, the available circulation alternatives and the strength of the destinations and origin on each side of the bridge.

Bridges are most frequently considered across features that are difficult, daunting or dangerous to cross on foot or on a bicycle. Interstate highways and railroads are often fenced to prohibit such crossing, so that a pedestrian bridge is the only option. Six-lane Parker Road (eight lanes with turn lanes) meets all three of these criteria, but it is, however, technically and legally possible to cross it on foot even though only the heartiest of able-bodied adults without children or strollers might feel comfortable doing so. It is this possibility that gets weighed against the inconvenience (time and effort) of going out-of-direction to access a bridge, take the stairs or elevator up to the overpass level,

descend the other side, and again walk or ride back to the original path leading to one's destination.

Vertical circulation is an important issue, both in terms of user convenience and visual impact; all pedestrian bridges must be ADA-compliant by including either a ramp or an elevator to access the upper level. An elevator requires very specific operations and maintenance commitments by either the Town or other private entity. Assuming a typical clearance, structure depths, and grades, a ramp would be approximately 380' long: longer than the current distance between the Parker Rd/Mainstreet intersection and the existing Kieffer's Crossing underpass (although the ramp could be designed to include a switchback or switchback to shorten the overall length.)

As land use conditions exist now, there is very little pull for pedestrians to cross Parker Road. Even with successful redevelopment in the West End, making an out-of-direction movement to access a bridge will be a tough call against the possibility of simply crossing Parker Road at grade.

There are, however, conditions in which a pedestrian bridge might be a good investment. The first of these conditions would be the development of a public parking structure on the site of the existing Parker Crossroads shopping center. In this case, a pedestrian bridge would serve as a visible connection between the west-side parking and east-side attractions, and would act as a marker for the public parking reservoir and clearly announce that it can connect users to attractions on both sides of Parker Road. In a similar scenario, the development of medium- to high-density residential on this site would provide a concentrated group of users desirous of accessing O'Brien Park, events held in the park and Old Town itself.



Pedestrian bridges can be fairly utilitarian (top) or act as iconic gateway features (bottom).

Public Transit and Shuttles

Existing Bus Service and Park-n-Ride

As noted in Chapter 2: Existing Conditions, public buses run on Dransfeldt Road, Parker Road and Mainstreet from the West End and as far east as Victorian Drive. The RTD Park-n-Ride is located east of Parker Road on Long's Way approximately a half mile from Mainstreet. The location of the park-n-ride does not promote synergy between commuting and downtown businesses; a location closer to downtown could encourage patrons to grab a pre-work coffee or an after-work drink, or pick up convenience retail items before heading home. This plan recommends that the Town work with RTD to explore the potential for a shared park-n-ride facility, preferably structured, that is closer to town and could double as public parking for the downtown area. This change would also free the existing park-n-ride lot for redevelopment, potentially in tandem with or as a further extension of the planned O'Brien Park expansion. A relocated park-n-ride should be located, however, so that RTD buses can access the facility without using Mainstreet.

***Recommendation 5.37:** Explore the potential for a shared-use Park-n-Ride/public parking structure more convenient to Mainstreet.*

Future BRT

In 2005 the Town conducted a study which concluded that bus rapid transit (BRT) was the preferred transit type to connect downtown Parker with the southeast light rail corridor. BRT is an enhanced transit service that typically includes a dedicated transit lane, enhanced patron shelters and off-vehicle ticketing. The proposed BRT lanes would begin in the Plan area's West End and continue west to the Ridgeway Parkway Park-n-Ride at the new end of line for the southeast light rail corridor. The current Mainstreet right of way between Dransfeldt Road and Twenty Mile Road is wide enough to include the required components of BRT which are: two travel lanes to reserve for bus service and an amenity zone and sidewalk wide enough to include BRT-specific elements such as shelters and ticket vending machines. The right of way east of Dransfeldt Road is not wide enough to accommodate BRT.

***Recommendation 5.38:** Maintain enough right of way to allow for two lanes and wide sidewalks along the potential BRT route in order to allow for construction of the future facility.*



image: blog.archpaper.com
Bus Rapid Transit (BRT) like this system in Cleveland requires dedicated travel lane and off-vehicle ticketing. Bus stops typically include enhanced shelters and patron amenities.

Downtown Circulator

A Downtown Circulator may provide a tool to address two distinctly different access issues: event parking, and daily ‘hops’ along the plan area. In the case of events and festivals, the Town may wish to offer a shuttle service in addition to or instead of the valet parking discussed in the next ‘Parking’ section. Used this way, the utility and feasibility of this type of shuttle service would require that remote event parking be concentrated in a single large area, or that temporary event parking be located in sufficient proximity and with reasonably direct (i.e., not circuitous) access such that multiple locations could be serviced efficiently with a single shuttle route. The actual route of the shuttle should be considered and designed in tandem with a larger event parking management strategy.

An ‘everyday’ shuttle is a decidedly long-term proposition for the Town, and will depend upon a much higher density of attractions and daily users than currently exists. Once sufficient density of uses and users has been achieved, the most likely circulator route is limited to Mainstreet itself, traveling from Twenty Mile Road to North Pine Drive in a ‘barbell’

configuration with a loop at each end. Shown on Figure 46 Circulator Route (Non-Event), the western loop provides easy access to amenities in the West End, while the eastern loop provides an efficient turnaround with access to both the PACE Center and the new library. The low density and circuitous roadway system of residential areas beyond this core area suggest that a longer circulator route providing service into the neighborhoods would be extremely inefficient in terms of ridership and timing.

Recommendation 5.39: Investigate potential special-event shuttle route in tandem with a temporary parking management plan.

Recommendation 5.40: Consider introduction of a Downtown Shuttle once sufficient use and user density has been reached.



image: philadelphiaspeaks.com

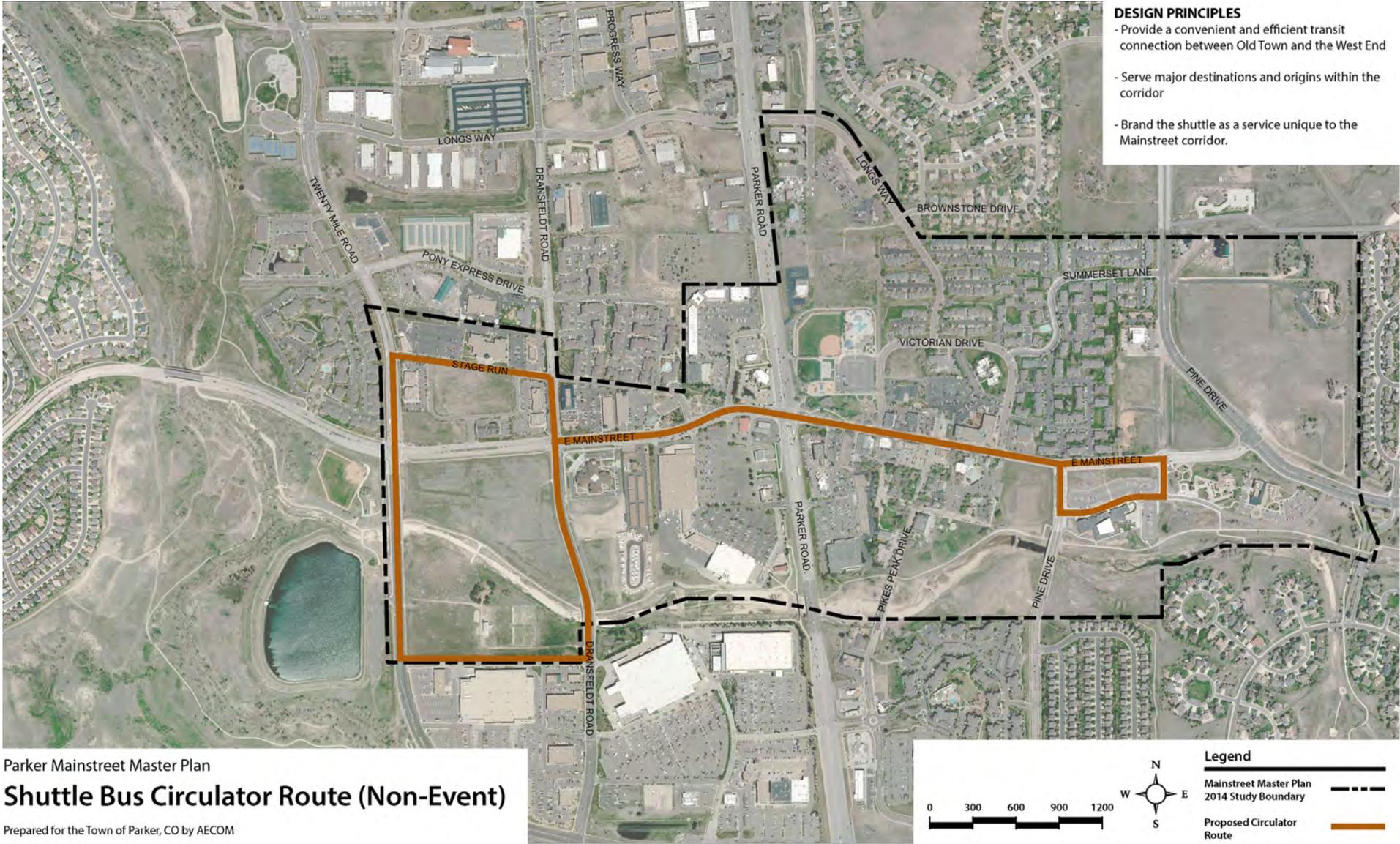


image: police.southtexascollege.edu



image: visitmcperson.com

Circulator shuttle can range in size from full-size buses, to small vans or even historic rubber-tired trolleys. Shuttles often feature special graphic branding.



- DESIGN PRINCIPLES**
- Provide a convenient and efficient transit connection between Old Town and the West End
 - Serve major destinations and origins within the corridor
 - Brand the shuttle as a service unique to the Mainstreet corridor.

Parker Mainstreet Master Plan
Shuttle Bus Circulator Route (Non-Event)

Prepared for the Town of Parker, CO by AECOM

Figure 46 Shuttle Bus Circulator Route (Non-Event)

A downtown shuttle transporting visitors from one end of downtown to the other may be a possibility as the study area densifies.

Parking

As noted in the Existing Conditions chapter of this report, the downtown area has approximately 264 on-street and 780 off-street, public parking places. All public parking is free and has no time limits, a condition leading to lack of turn-over and frustrated 'circling' for parking. There are also a number of significantly-sized private lots, most notably the 265-space pay-for-parking lot behind Parker Station, that are often used by the public.

Current policy in the Old Town area does not have any parking minimums or maximums for new development; this approach offers the advantage of allowing the market to dictate an appropriate level of parking rather than selecting an arbitrary number, but can lead to more surface parking or increased demand for already-limited on-street parking.

Parker's largest parking challenge is during its many special events, particularly Parker Days which in 2014 drew 145,000 attendees over the three-day festival. Although the Town cannot possibly park the extreme demand of an event of this magnitude, it does need a strategy to deal with daily and weekend demand, as well as smaller special events.

In order to fully understand and respond to any parking challenges in the Old Town area, the Town will need to commission a formal parking study that analyzes all aspects of parking in downtown and provides recommendations. This parking study will provide further direction regarding the recommendations in this chapter.

Short-Term Parking Management

The ultimate goal for downtown parking is two-fold: 1) to access parking reservoirs-both public and private-from roadways other than Mainstreet and 2) to park once, then walk from destination to destination. Public parking should be consolidated, in order to reduce congestion resulting from 'hunting for parking', and located at the perimeter but within convenient walking distance to downtown destinations, to promote a walkable pedestrian core.

Consolidation and Signage

The current high demand for existing public parking suggests that removal of any on- or off-street public parking is not possible unless that parking can be replaced in another location and within reasonable walking distance to Mainstreet. While parking access points may be reconfigured in the short-term, large scale changes to parking location and size will need to be deferred until such time that the Town is ready to build or partner to build a parking structure.

Until parking can be consolidated in a meaningful way, dynamic parking signage can be a useful tool in reducing the congestion from hunting for parking. Signage would serve double duty to call attention to parking location, and would display real-time information on availability.

***Recommendation 5.41:** Maintain existing public parking locations and quantities until parking structure is built.*

Time Limits and Paid Parking

A management strategy is needed to increase the turnover of the 288 on-street parking spaces within the Old Town portion of Mainstreet. Stakeholder input indicated strong support for time limits, but distinct opposition to paid parking. Both approaches require similar levels of enforcement to be effective, although paid parking obviously provides a funding stream to offset some of these labor costs. This plan recommends that the Town institute time limits for on-street parking; a full parking study will be needed to determine appropriate limits and potential catalysts for when, if ever, to transition to paid parking.

Shared Parking

Another parking management strategy often employed in downtown areas is shared parking. In this approach, land uses with non-overlapping hours of use pursue formal agreements to utilize a shared pool of parking. Examples of this type of shared use might be an office (typically daytime, weekday use) and a movie theater (predominantly evening and weekend use). Other good candidates would be transit facility park-n-ride (heavy weekday commuter use) and churches or temples (typically very well-defined evening or weekend use).

The RTD Park-n-Ride is downtown Parker's best opportunity for formal shared parking agreements, as discussed in the 'Event Parking' section of this chapter. Beyond this location, downtown Parker does not currently have obvious synergies of this type; both the PACE Center and the library have extended programming with regular parking demand, while big box, retail and restaurants have similar all-day and weekend demand. Shared parking is an important tool to keep in mind relative to long term redevelopment within the downtown area, particularly as the Town tries to increase daytime users such as office or education.

Recommendation 5.42: Continue to evaluate shared parking opportunities as new development is proposed.

Parking Access and Other Curb Cuts Mainstreet Center

This chapter recommends that the east leg of Victorian Drive be extended through to Pikes Peak Avenue, allowing straightened and more intuitive entry into the Mainstreet Center. This recommendation also provides opportunity for redesign of the existing access drive as an attractive, pedestrian-oriented landscape or entry feature for the adjacent Mainstreet Center and Chapel.

Recommendation 5.43: Close the Mainstreet access drive to Mainstreet Center parking in coordination with the extension of the east leg of Victorian Drive south of Mainstreet.

Other Curb Cuts

In order to reduce pedestrian-vehicular conflict and create a more pedestrian-focused downtown, this Plan also recommends the closure of the Mainstreet curb cut at 18901 East Mainstreet, a Parker Authority for Reinvestment (PAR)-owned parcel on the northeast corner of Victorian Drive and Mainstreet with redevelopment and the curb cut immediately east of the existing auto repair building (immediately west of the Mainstreet Center). On the auto repair parcel, the parcel's large rear lot and Pikes Peak access suggest that this closure would not significantly impact on-site circulation, but if the current owner is not amenable to this change, the closure may be deferred until the parcel redevelops but should be a condition of site approval at that time.

Recommendation 5.44: Close the Mainstreet access drive to the parcel on the northeast corner of Mainstreet and Victorian Way in coordination with redevelopment.

Recommendation 5.45: Close the Mainstreet access drive to the parcel on the southeast corner of Mainstreet and Pikes Peak Drive with redevelopment.

In contrast, the Town may wish to consider adding a curb cut on the north side of the 18900 block of Mainstreet in the West End. This block has two alley-served buildings currently occupied by a breakfast and Mexican restaurants. The north-south alley between the two buildings turns east and runs between Mainstreet and the Mexican restaurant's outdoor dining patio. Introduction of an alley curb cut would allow the enlargement of this existing patio to form a better, more pedestrian-focused street edge on Mainstreet. The curb cut should only be introduced, however, in tandem with the extension of active pedestrian uses to Mainstreet; otherwise, the increased friction of the curb cut is not counterbalanced by equivalent gains in the pedestrian realm.

Recommendation 5.46: Introduce an alley curb cut in the West End in tandem with expansion of pedestrian uses to the Mainstreet right-of-way edge.

Event Parking

In order to address the extensive and sometimes overwhelming parking demands associated with special events, the Town should formulate a strategic special event parking plan that identifies parking locations, priority (order in which they are parked), and type (open or valet), as well as other pertinent details such as cost.

One potential component of the strategic parking plan is use of public Town-owned vacant parcels for temporary or temporary overflow parking for events. These sites include the parcel immediately west of the new Parker library site, the Pine Curve site and the site south of Sulphur Gulch and east of Pine Drive. These uses should in no way preclude the development of these parcels, and careful consideration should be given to potentially hidden costs such as insurance or liability. Similarly, the Town may wish to investigate policies regarding use of vacant private properties for the same purpose.

Looking at additional close-in parking opportunities, the Town should also investigate a formalized joint-use agreement with RTD for weekend use of the existing Park-n-Ride lot on Longs Way. Such an agreement may be necessary to provide signage to the lot, and may make it possible to provide additional overflow parking for events in the grass area southwest of the paved lot. A formal agreement may also require monetary or maintenance considerations, or liability concerns, which the Town may or may not wish to undertake.

Another measure which may ease parking pressures during smaller events would be the use of valet parking to remote lots. Exact management of valet parking—who funds it, what existing parking lots or vacant parcels could be used, lease agreements for those lots and parcels if not publicly owned—would need to be explored in more detail. As of the writing of this document, the Chamber of Commerce has initiated a pilot program for Friday and Saturday nights; this program will provide valuable information on utilization, costs and benefits of such a service. Even if the program proves to be untenable at this time, valuable information on minimum thresholds or operating models may be able to provide an informative survey of when the service may be useful in the future.

Recommendation 5.47: Prepare a strategic special event parking plan that includes consideration of the following:

- Temporary event parking on the Town-owned parcels.
- Valet parking locations and management for events
- Shuttle bus service from outlying lots

Recommendation 5.48: Investigate a formal joint-use agreement for weekend use of the RTD Park-n-Ride lot.



Parker hosts a large number of yearly events.

Long-Term Parking Management

The preceding text discussed parking management strategies such as paid parking, dynamic parking availability signage, shared parking and consolidation of surface lots. All of these strategies should be initiated in the short-term but continually evaluated and adjusted in the long-term as well.

New Development

Parker has no parking requirements for new development in the Old Town area. This policy can be an advantage in allowing developers to save money constructing spaces, but can also backfire and result in increased surface parking, an issue this Master Plan is attempting to reduce.

In order to support this plan's goals of higher density, the Town should consider a parking maximum or 'cap' to begin to chip away at the large amount of surface area within the Plan Area. Alternately, the Town may allow an unrestricted amount of parking, but require that space beyond a defined level must be structured rather than surface parked. A special study of current trends would be necessary to determine appropriate caps by use.

Recommendation 5.49: Evaluate a parking cap or structured parking requirement for new development.

Structured Parking

The Town has previously discussed a public parking structure, and is very interested in building one when demand and economics are right. Potential garage locations and size should be determined through a formal parking and traffic study when the Town is ready to move forward with planning, design, financing and construction. This type of study will comprehensively evaluate the myriad factors that will ultimately determine garage and roadway performance, including garage size, signal timing current and land use context.

In advance of a formal parking study, the Town should keep track of land availability, sales and development plans and how changes in these areas could open potential for or preclude future public or public/private parking structures. In order to preserve existing downtown character, promote a pedestrian-focused environment, and to minimize impacts on already-congested Mainstreet, a future parking structure should be sited with the following three criteria in mind:

- access from a roadway other than Mainstreet,
- location on the periphery of the plan area,
- potential to catalyze development in its immediate area.

The first two criteria focus on mitigating vehicular congestion within the core Mainstreet area and on creating a pedestrian-focused environment. Less cars on Mainstreet creates a more pedestrian-friendly environment, while getting people out of their cars at the 'edges' and asking them to walk into the Old Town and West End cores not only increases business exposure but also enhances the potential for window shopping and discovery of new shops or attractions with which the visitor was not familiar. The third criterion, potential to catalyze development, looks to capitalize on the Town's investment in infrastructure, or to create opportunity for public-private partnership and shared parking.

The accompanying map shows locations which may conceptually be able to accommodate a standard parking structure footprint. Not all of these locations meet all three of the criteria above, and the Plan does not recommend any of these site over another.

Recommendation 5.50: Commission a full parking study to analyze current parking utilization, appropriate short term management strategies and long term parking solutions. This parking study should include, but not be limited to, consideration of the following:

- Introduce time limits on street parking, with appropriate enforcement.
- Installation of real-time dynamic parking signage showing parking availability.
- Valet parking
- Parking wayfinding signage
- Evaluation of a parking cap or structured parking requirement for new development
- Evaluation of a parking district or fee based system for new development
- Evaluation of the size, location, configuration and timing of a future parking structure

Recommendation 5.51: Track land use, property sales and development proposals which may present opportunity for or preclude a future parking structure.

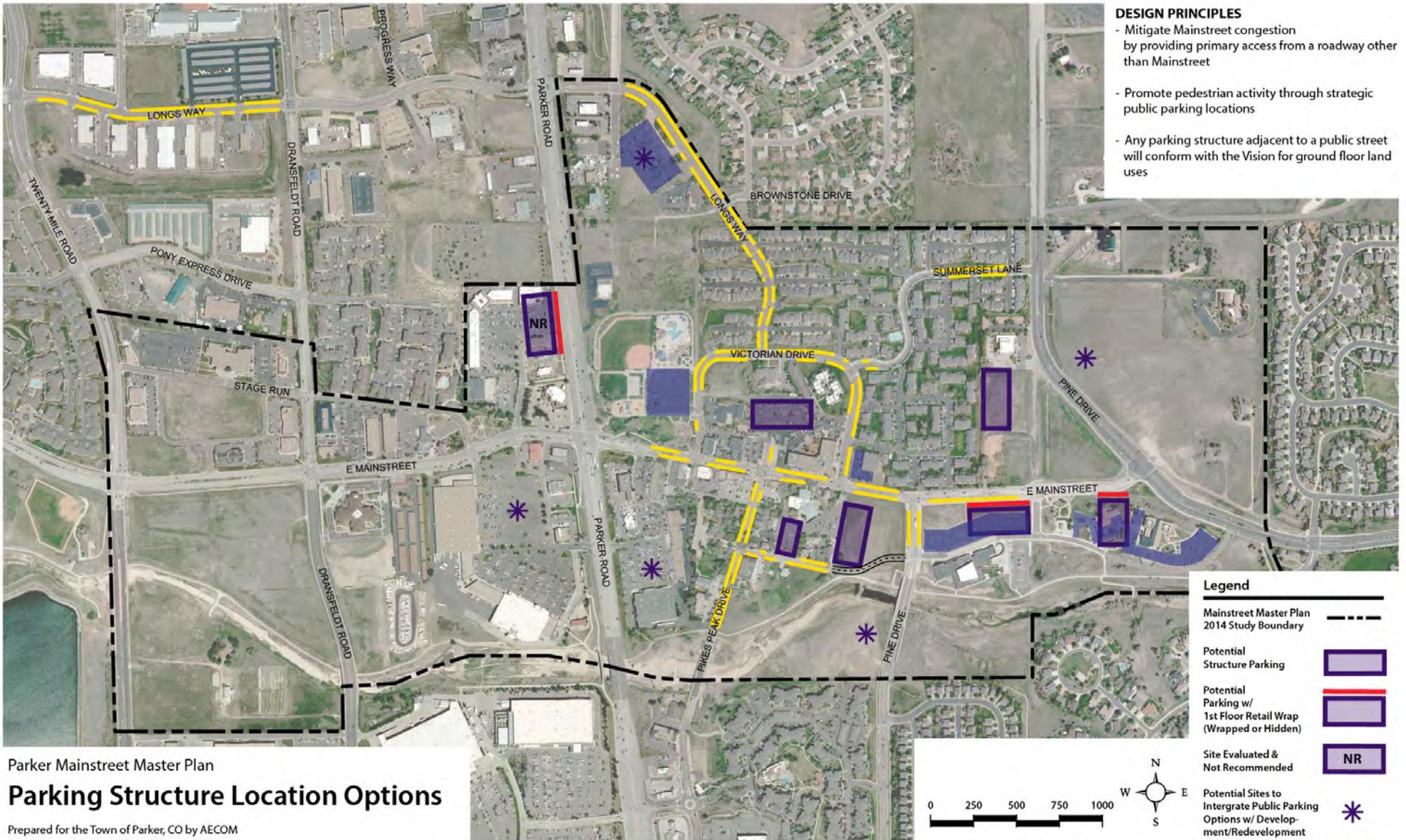


Figure 47 Parking Structure Location Options

The location of a future parking structure should be carefully evaluated to ensure that it does not worsen already congested Mainstreet traffic; a location that does not provide entrance or exit on Mainstreet is recommended.

Putting it Together

Working toward Our Goals

The chart below shows a complete list of the land use recommendations contained in this chapter, and shows how each recommendations ties with the goals established for the project.

PA: Plan Area WE: West End OT: Old Town

			Plan Area Goals										
Plan Order	Plan Area	RECOMMENDATION	P-1	P-2	P-3	P-4	P-5	P-6	P-7	P-8	P-9	P-10	P-11
			Attract Desirable Uses	Solicit Public Support	Foster partnerships.	Build on Existing Assets	Create cohesive + distinctive character	Year-Round, 24/7 Activity	Create gateways	Emphasize quality design.	Promote multi-modality	Enhance pedestrian safety	Enhance ped/bike connexns
Circulation Recommendations													
Motor Vehicles													
5.1	PA	Pursue circulation measures that create and promote alternate routes to Mainstreet and a finer grain, gridded roadway network.									x		
5.2	OT	Extend Pikes Peak Avenue from Pikes Peak Drive to Parker Road.									x	x	
5.3	WE	Introduce new east-west segment of Pikes Peak Avenue between Parker Road and the north-south extension of Briargate Lane.										x	
5.4	WE	In coordination with development/redevelopment create a four-way, full movement signalized intersection at Mainstreet/Briargate Lane, in tandem with the closure of the adjacent Stage Run intersection.	x										
5.5	OT	In coordination with development and redevelopment, extend the east leg of Victorian Drive to Pikes Peak Avenue.				x							
5.6	OT	Extend Pikes Peak Avenue to Stonehenge Way.										x	
5.7	PA	Signalize the Longs Way and Parker Road intersection.											x
5.8	OT	Signalize the intersections of Mainstreet and Victorian Drive.										x	x
5.9	WE	In coordination with redevelopment, remove the existing signal at Mainstreet and Stage Run signal in tandem with the installation of the proposed Briargate Lane signal.	x										
5.10	PA	Use marketing and promotional materials for downtown campaigns and event to promote alternate, non-Mainstreet routes for arriving downtown.											
5.11	PA	Use regional and local directional signage to promote alternate routes to Mainstreet, particularly for motorists driving to non Mainstreet destinations.											

Table 3 Circulation Recommendations

West End Goals							Old Town Goals							
W-1	W-2	W-3	W-4	W-5	W-6	W-7	O-1	O-2	O-3	O-4	O-5	O-6	O-7	O-8
Establish West End character	Foster partnership	Create Pedestrian Amenities	Provide new open space	Preserve BRT potential	Improve land use mix	Promote active ground floor uses	Improve land use mix	Promote active ground floor uses	Preserve historic character	Foster partnership	Promote multi-modality and events	Build on existing assets	Provide parallel E-W routes	Manage parking
											x		x	
											x		x	
					x									
												x		
													x	
													x	

Table 3 Circulation Recommendations

Plan Order	Plan Area	RECOMMENDATION	Plan Area Goals										
			P-1	P-2	P-3	P-4	P-5	P-6	P-7	P-8	P-9	P-10	P-11
			Attract Desirable Uses	Solicit Public Support	Foster partnerships.	Build on Existing Assets	Create cohesive + distinctive character	Year-Round, 24/7 Activity	Create gateways	Emphasize quality design.	Promote multi-modality	Enhance pedestrian safety	Enhance ped/bike connexns
5.12	OT	Maintain existing two-way function of Mainstreet.										x	x
5.13	PA	Promote Twenty Mile Road and Longs Way as alternative, parallel routes to Mainstreet.				x							x
Bicycles													
5.14	PA	Use pilot projects to introduce new bicycle facilities in a measurable time frame and to fine-tune and test facilities.									x		x
5.15	PA	Leave pilot projects in place for a minimum of one year; support pilots with appropriate outreach and well-thought out evaluation metrics									x		x
5.16	PA	Provide 'spur ' north-south connections for bicycles between Sulphur Gulch Trail and Mainstreet on the following roadways: Dransfeldt Road, Pikes Peak Drive, PACE Center Drive.				x					x		x
5.17	WE	Construct an east-west trail connection over Cherry Creek between Clarkeville Way and Twenty Mile Road.									x		x
5.18	PA	When the Parker Road/Longs Way signal is installed, delineate an east-west bike lane along Longs Way connecting the Cherry Creek Trail to Old Town with buffered bike lanes from Twenty Mile Road to Brownstone Drive and bike lanes from Brownstone Drive to Victorian Drive.									x		x
5.19	OT	Introduce striped on-street bike lanes along the entire length of Victorian Drive.									x		x
5.20	OT	Convert existing angle parking on the east side of Parker Station to parallel parking in order to allow for recommended bike lanes.									x		x
5.21	OT	Introduce an off-road, shared-use path on PACE Center Drive.									x		x

Table 3 Circulation Recommendations

West End Goals							Old Town Goals							
W-1	W-2	W-3	W-4	W-5	W-6	W-7	O-1	O-2	O-3	O-4	O-5	O-6	O-7	O-8
Establish West End character	Foster partnership	Create Pedestrian Amenities	Provide new open space	Preserve BRT potential	Improve land use mix	Promote active ground floor uses	Improve land use mix	Promote active ground floor uses	Preserve historic character	Foster partnership	Promote multi-modality and events	Build on existing assets	Provide parallel E-W routes	Manage parking
									x					
													x	
											x			
											x			
											x	x		
											x			
											x			
											x			
											x			

Table 3 Circulation Recommendations

Plan Order	Plan Area	RECOMMENDATION	Plan Area Goals										
			P-1	P-2	P-3	P-4	P-5	P-6	P-7	P-8	P-9	P-10	P-11
			Attract Desirable Uses	Solicit Public Support	Foster partnerships.	Build on Existing Assets	Create cohesive + distinctive character	Year-Round, 24/7 Activity	Create gateways	Emphasize quality design.	Promote multi-modality	Enhance pedestrian safety	Enhance ped/bike connexns
5.22	WE	Introduce an off-road, shared use path on the west side of Dransfeldt Road between Mainstreet and the Sulphur Gulch Trail.									x		x
5.23	WE	Introduce an off-road, shared use path on the west side of Dransfeldt Road between Mainstreet and Longs Way.									x		x
5.24	WE	Consider introducing sharrows on Stage Run (a private street).									x		x
5.25	WE	Continue the future Ridgeway/Mainstreet multi-use trail on the south side of Mainstreet from Twenty Mile Road to Dransfeldt Road.									x		x
5.26	OT	Introduce improved wayfinding signage, on-street sharrows and widened sidewalks on Pikes Peak Drive.									x		x
5.27	OT	Study and consider a long term outcome that requires acquiring additional right-of-way, removing on-street parking on one side and introduce on-street bike lanes with improved sidewalks on Pikes Peak Drive.									x		x
5.28	PA	Designate 'inverted U' racks as the standard for downtown bicycle parking and encourage/incorporate artistic bike racks									x		x
5.29	PA	Locate bike parking in highvisibility areas within 50 feet of destinations.									x		x
5.30	PA	Install bicycle racks with sufficient clear distances from roadway, other racks, and adjacent vertical elements.									x		x
5.31	OT	Consider installation of a bike corral within the core area of Old Town.									x		x
5.32	PA	Organize group rides and events to advertise and promote the use of new bicycle facilities.									x		x

Table 3 Circulation Recommendations

West End Goals							Old Town Goals							
W-1	W-2	W-3	W-4	W-5	W-6	W-7	O-1	O-2	O-3	O-4	O-5	O-6	O-7	O-8
Establish West End character	Foster partnership	Create Pedestrian Amenities	Provide new open space	Preserve BRT potential	Improve land use mix	Promote active ground floor uses	Improve land use mix	Promote active ground floor uses	Preserve historic character	Foster partnership	Promote multi-modality and events	Build on existing assets	Provide parallel E-W routes	Manage parking
		x												
		x												
		x												
											x			
											x			
											x			
											x			
											x			
											x			
											x			

Table 3 Circulation Recommendations

Plan Order	Plan Area	RECOMMENDATION	Plan Area Goals											
			P-1	P-2	P-3	P-4	P-5	P-6	P-7	P-8	P-9	P-10	P-11	
			Attract Desirable Uses	Solicit Public Support	Foster partnerships.	Build on Existing Assets	Create cohesive + distinctive character	Year-Round, 24/7 Activity	Create gateways	Emphasize quality design.	Promote multi-modality	Enhance pedestrian safety	Enhance ped/bike connexns	
Pedestrians														
5.33	WE	Introduce a mid-block pedestrian crossing on Mainstreet between Twenty Mile and Dransfeldt Roads in coordination with development of the park/plaza to the north.										x	x	x
5.34	PA	Introduce additional crosswalk enhancement to increase visibility, safety, comfort and convenience at priority pedestrian intersections.										x	x	x
5.35	PA	Work with CDOT to develop pedestrian enhancements to improve the safety, comfort and aesthetics of the Mainstreet and Parker Road intersection										x	x	x
5.36	PA	Re-align Kieffer's Crossing to allow an unobstructed line of sight from one end to the other, reconfigure entries to integrate into adjacent context and provide more prominent wayfinding signage.										x	x	x
Public Transit and Shuttles														
5.37	WE	Maintain enough right of way to allow for two lanes and wide sidewalks along the potential BRT route in order to allow for construction of the future facility.										x		
5.38	PA	Investigate potential special-event shuttle route in tandem with a temporary parking management plan.										x		
5.39	PA	Consider introduction of a Downtown Shuttle once sufficient use and user density has been reached.										x		
Parking														
5.40	OT	Maintain existing public parking locations and quantities until parking structure is built.												
5.41	PA	Continue to evaluate shared parking opportunities as new development is proposed.		x										
5.42	OT	Close the Mainstreet access drive to Mainstreet Center parking in coordination with the extension of the east leg of Victorian Drive south of Mainstreet.											x	

Table 3 Circulation Recommendations

West End Goals							Old Town Goals							
W-1	W-2	W-3	W-4	W-5	W-6	W-7	O-1	O-2	O-3	O-4	O-5	O-6	O-7	O-8
Establish West End character	Foster partnership	Create Pedestrian Amenities	Provide new open space	Preserve BRT potential	Improve land use mix	Promote active ground floor uses	Improve land use mix	Promote active ground floor uses	Preserve historic character	Foster partnership	Promote multi-modality and events	Build on existing assets	Provide parallel E-W routes	Manage parking
		x									x			
		x									x			
		x									x			
		x									x	x		
				x										
														x
														x
														x

Table 3 Circulation Recommendations

Plan Order	Plan Area	RECOMMENDATION	Plan Area Goals										
			P-1	P-2	P-3	P-4	P-5	P-6	P-7	P-8	P-9	P-10	P-11
			Attract Desirable Uses	Solicit Public Support	Foster partnerships.	Build on Existing Assets	Create cohesive + distinctive character	Year-Round, 24/7 Activity	Create gateways	Emphasize quality design.	Promote multi-modality	Enhance pedestrian safety	Enhance ped/bike connexns
5.43	OT	Close the Mainstreet access drive to the parcel on the northeast corner of Mainstreet and Victorian Way in coordination with redevelopment.										x	
5.44	OT	Close the Mainstreet access drive to the parcel on the southeast corner of Mainstreet and Pikes Peak Drive.										x	
5.45	WE	Introduce an alley curb cut in the West End in tandem with expansion of pedestrian uses to the Mainstreet right-of-way edge.											
5.46	PA	Prepare a strategic special event parking plan that includes consideration of the following: <ul style="list-style-type: none"> • Temporary event parking on the Town-owned parcels. • Valet parking locations and management for events • Shuttle bus service from outlying lots 											
5.47	OT	Investigate a formal jointuse agreement for weekend use of the RTD Park-nRide lot.											
5.48	PA	Evaluate a parking cap or structured parking requirement for new development.											
5.49	PA	Commission a full parking study to analyze current parking utilization, appropriate short term management strategies and long term parking solutions. This parking study should include, but not be limited to, consideration of the following: <ul style="list-style-type: none"> • Introduce time limits on street parking, with appropriate enforcement. • Installation of real-time dynamic parking signage showing parking availability. • Valet parking • Parking wayfinding signage • Evaluation of a parking cap or structured parking requirement for new development • Evaluation of a parking district or fee based system for new development • Evaluation of the size, location, configuration and timing of a future parking structure 											
5.50	PA	Track land use, property sales and development proposals which may present opportunity for or preclude a future parking structure.											

Table 3 Circulation Recommendations

West End Goals							Old Town Goals							
W-1	W-2	W-3	W-4	W-5	W-6	W-7	O-1	O-2	O-3	O-4	O-5	O-6	O-7	O-8
Establish West End character	Foster partnership	Create Pedestrian Amenities	Provide new open space	Preserve BRT potential	Improve land use mix	Promote active ground floor uses	Improve land use mix	Promote active ground floor uses	Preserve historic character	Foster partnership	Promote multi-modality and events	Build on existing assets	Provide parallel E-W routes	Manage parking
														x
														x
		x												x
														x
														x
														x
														x

Table 3 Circulation Recommendations

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6 Implementation

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Phasing and Prioritization

The recommendations outlined in the Mainstreet Master Plan will be carried out over the next 20 plus years by a variety of public and private entities and initiatives. Some recommendations are clear and straight-forward, and can be implemented in a fairly short amount of time under the purview of a single department or entity. Other recommendations will require cooperation and collaboration between a larger number of parties, as well as an extended period of time, to implement. As the saying goes, however, the journey of a thousand miles begins with a single step, and the recommendations contained in this document are not intended to be implemented all at once.

The accompanying matrix divides recommendations into five phases:

- 'groundwork'
- Phase 1
- Phase 2
- Phase 3
- 'AA' or 'as appropriate'

Within this broad categorization, the Town will need to establish further refined priorities. This sorting should weigh each action's potential return on investment (ROI) and the projects timing in relationship to other undertakings in the Plan area and order projects accordingly. In the context of the Master Plan, ROI includes both tangible and intangible factors such as: visual/aesthetic impact, enhancement/expansion of existing community assets, economic potential as a development catalyst, and infrastructure standardization or completion.

Groundwork

Groundwork recommendations should receive immediate attention and action; largely policy-based, these elements create the framework of requirements and guidelines to construct the vision established by the Master Plan. These policy changes will establish the rules that future development proposals must follow, and provide critical guidance for current and future Town staff as they work through the development approval process. An example of this 'groundwork' recommendation is the creation of a formal Town policy - whether zoning, overlay district or other mechanism - only allowing ground-floor, Mainstreet abutting space to be active uses. This recommendation is a foundational principle of the Master Plan, and since land use decisions can have long-term impacts, this new policy should be adopted and put in place immediately.

Phase 1 (1-5 years)

Phase 1 elements are intended to be accomplished within the next five years. This group of improvements tend to be those which correct an existing issue or bring existing conditions to a minimum standard. Investigating a special-event parking shuttle is intended to address a pressing, current need and is an example of this type of action.

Phase 2 (6-10 years)

Phase 2 elements represent downtown enhancements of low- to medium-complexity or urgency, intended to be accomplished in a six to 10 year timeframe. These projects contribute to a pedestrian-oriented downtown and will serve to enhance the Town's character and capacity. The redesign of living wheel park as a more active space is an example of this type of action.

Phase 3 (11-20 years)

Phase 3 elements are generally more complex and require significantly more complicated preparation, including public/private partnerships, financing, property acquisition and/or aggregation, or market readiness. These projects are envisioned to be carried out in an 11 to 20 year time frame. The realignment of Stage Run falls into this category since it requires a coordinated design effort and is tied to redevelopment within the area.

As Appropriate

A final category of improvements, identified as 'AA' or 'As Appropriate' are projects with flexible timing. They may be implemented in the short-, mid- or long-term depending upon opportunities to combine with other project or future development. Introduction of Pikes Peak extensions through the existing strip malls east and west of Parker Road fall into this category, since they are dependent on the property owner's redevelopment of private property.

Schedule, Benchmarks and Monitoring

As described above, the accompanying matrix divides recommendations into broad five-year categories. The next step toward implementation is for the Town to create a further refined schedule for groundwork and Phase 1 activities, and to identify specific individuals that will 'champion' each action.

The Town should specifically consider which actions can be accommodated within existing departmental budgets, which will need specific allocation requests at the next yearly budget, and which could be incorporated into or added to current or planned programs and initiatives. Other actions may fall under a grey area, in which they could be executed by public funds or be included as a condition of private development; in this case, the Town may wish to set a date by which the action must be allocated to one or the other. In other words, if it has not been added to a development proposal by a certain date, the Town will move forward with funding and executing internally and seek reimbursement at time of development.

As the 'owner' of the Mainstreet Master Plan, the Town's Community Development Department should schedule a yearly evaluation of plan progress. This evaluation need not be elaborate, but should track which recommendations have been executed, which are in process, and which may need to be updated due to changing conditions. This evaluation should also identify which recommendations should be initiated or completed in the coming year.

Funding

The funding for a plan of this magnitude will come from a variety of sources, including federal, state and local funds, as well as private and public grants and individual contributions. Some potential sources of funding for public improvements, private development and individual business development include:

Federal

- Allocations for transportation improvements or enhancements
- Housing assistance
- Public infrastructure grants

State

- Strategic Fund Incentive program
- Enterprise Zone mechanisms (tax credits)
- Colorado Creative Industries grants
- State lottery funds

Local

- Local car and bed tax
- Capital improvement tax
- Business Improvement districts
- Special Improvement districts
- On-going monies allocated for operations and maintenance
- Tax increment financing
- Parker Authority for Reinvestment

Private

- Private foundation grants
- Angel networks
- Venture capital firms
- Small Business Investment Companies (SBIC)
- Donations from individuals and groups

Plan Order	Plan Area	RECOMMENDATION	Potential Mechanism										Lead Department / Organization	Phase
			Zoning or Overlay District	Incentives	Other Policy Update (Plans, Engineering Details)	Marketing / Promotion	Business Recruitment	Design / Approvals Process	Publicly Funded Project	Privately Funded Project	Public Private Partnership	Further Study / Design		
Land Use Recommendations														
Future Uses														
3.1	PA	Promote active ground-floor uses of Mainstreet-abutting building to only retail, restaurants and other active uses.	x	x		x	x						Economic Development, Council, Community Development	Groundwork
3.2	PA	Encourage creative-industry uses such as cafes, art galleries, music stores and unique shops.		x		x	x						Chamber, Economic Development, Cultural, Parker Arts Council	AA
3.3	OT	(Aspirational): Promote an opportunity for an artisan/'makers' market in Old Town.		x		x	x						Chamber, Economic Development	AA
3.4	PA	Seek opportunities for pulling active ground floor uses 'around the corner' at Mainstreet intersections.	x		x			x					Community Development, Economic Development	AA
3.5	PA	Promote increased residential uses throughout the plan area, with a focus on higher-density multi-family products.	x										Community Development, Planning Commission, Council	1
3.6	PA	Limit development of ground-floor residential to side and secondary streets.	x							x	x		Community Development, Planning Commission, Council	1
3.7	OT	Redevelop existing Town and Country Townhomes adjacent to Mainstreet to higher-density, vertically mixed use development in the long-term.		x						x	x		PPP, Economic Development	AA
3.8	OT	Encourage redevelopment of the Parker Park-n-Ride with increased density and intensity of land uses to create a transit oriented development.	x	x						x	x	x	Economic Development, Council, Community Development	
3.9	PA	Continue to promote office as an upper-floor use throughout the Plan area; direct development of ground-floor office uses to side and secondary streets.	x	x		x	x						Community Development, Economic Development, Council	AA
3.10	PA	Diversify the inventory of office space to include larger floor-plates and more flexible configurations.	x				x						Economic Development, Council, PPP	1
3.11	PA	Actively pursue higher density office employment areas and educational/institutional uses that support a daytime economy.	x	x			x						Chamber, Economic Development, PPP	1

Table 4 Implementation

Plan Order	Plan Area	RECOMMENDATION	Potential Mechanism										Lead Department / Organization	Phase	
			Zoning or Overlay District	Incentives	Other policy updates (Plans, Engineering Details)	Marketing / Promotion	Business Recruitment	Design / Approvals Process	Publicly Funded Project	Privately Funded Project	Public Private Partnership	Further Study / Design			
3.12	PA	Encourage creative industry tenants such as architects, artists, designers, publishing, software and marketing to locate to Plan area.		x		x	x							Chamber, Economic Development, Parker Arts Council	AA
3.13	PA	Update existing planning and development policies in order to attract viable office/commercial business and activities.	x											Community Development, Economic Development, Planning Commission, Council	Groundwork
3.14	PA	Develop regulatory and other incentive tools that encourage development and redevelopment consistent with recommendations in this Plan as well as the Parker Transportation Master Plan and the Parker 2035 - Master Plan	x	x	x							x		Community Development, Economic Development, Planning Commission, Council	Groundwork
3.15	OT	Design the Civic Centre new park and plaza as a special event-ready venue.							x					Community Development, Parks	1
3.16	OT	Integrate a plaza space into future development and display art, as an intimate outdoor programming venue							x			x		Community Development, Cultural, Parks, PPP	AA
3.17	WE	Redesign the existing 20 Mile and Living Wheel Parks to promote activity within the parks and create a pedestrian-friendly street edge.									x	x		Community Development, Parks	2
3.18	WE	Construct a new public open space abutting Stage Run, in tandem with development.							x			x		Community Development, PPP	2
3.19	WE	Improve the gravel parking area south of Bar CCC Park, on the west side of 20 Mile Road to act as a public parking reservoir for the West End.									x			Public Works, Parks	2
3.20	WE	Evaluate the potential for the future conversion of Bar CCC Park into an active or passive, but unprogrammed, park.											x	Community Development, Parks	3
3.21	OT	Complete an O'Brien Park expansion master plan to establish a comprehensive vision and design for improvements to the park and expansion area.									x		x	Parks, Public Works, Community Development	1
Opportunities to Realign Development with Land Use Vision															
3.22	PA	Modify existing policy to guide redevelopment of parcels that are inconsistent with the land use vision.	x											Community Development, Planning Commission, Council	Groundwork

Table 4 Implementation

Plan Order	Plan Area	RECOMMENDATION	Potential Mechanism										Lead Department / Organization	Phase		
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3.23	PA	Seek opportunities and utilize incentives to support property development and redevelopment that is consistent with the Plan's vision.		x											Economic Development, Council	AA
Priority Parcels																
3.24	WE	Promote redevelopment of the NW corner of the Mainstreet/Parker Road intersection.		x		x	x					x	x		Economic Development, Chamber, PPP	AA
3.25	WE	Actively promote development of the Stage Run Center parcel, south of the existing movie theater.		x		x	x					x	x		Economic Development, Chamber, PPP	AA
3.26	OT	Capitalize on existing strength of the Mainstreet/Pikes Peak Drive intersection.		x		x	x					x	x		Economic Development, Community Development Chamber, PPP	1
3.27	OT	Capitalize on current redevelopment at the Mainstreet/PACE Center Drive intersection.		x		x	x			x	x	x			Economic Development, Community Development Chamber, PPP	AA
3.28	PA	Promote unity and cohesion along the entire length of Mainstreet, regardless of development phasing.	x							x					Community Development, Public Works	1
Urban Form Recommendations																
Design Districts																
4.1	PA	Redraw Greater Downtown Zone Districts to more closely align with and support the vision of the Mainstreet Master Plan.	x												Community Development, Planning Commission, Council	Groundwork
4.2	PA	Amend the General Land Use Plan contained in Parker 2035: Changes and Choices Master Plan to extend the Downtown Core character area west to Twenty Mile Road.				x									Community Development, Planning Commission, Council	Groundwork
Height, Massing and Transition																
4.3	OT	Required height for all new Mainstreet-facing buildings in the West End should be 2 floors minimum and 4 floors maximum. Parcels on the northwest and southwest corners of Parker Road should be a minimum of 3 stories and a maximum of 5 stories.	x												Community Development, Planning Commission, Council	Groundwork
4.4	OT	Required height for all new development that does not abut Mainstreet should be a minimum of 2 stories and a maximum of 5 stories	x												Community Development, Planning Commission, Council	Groundwork

Table 4 Implementation

Plan Order	Plan Area	RECOMMENDATION	Potential Mechanism										Lead Department / Organization	Phase		
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4.5	WE	Front setback for all new Mainstreet-facing buildings in the West End is a minimum of 12 and maximum of 20 feet from back of sidewalk.	x												Community Development, Planning Commission, Council	Groundwork
4.6	WE	Side setbacks for all new Mainstreet-facing buildings in the West end is a minimum of zero and a maximum of 10 feet. When the side setback area is to be used for outdoor dining, up to a 24-foot setback may be considered, depending upon adjacent use.	x		x				x						Community Development, Planning Commission, Council	Groundwork
4.7	WE	All new buildings in the West End shall have primary, active pedestrian entrances facing Mainstreet.	x												Community Development, Planning Commission, Council	Groundwork
4.8	WE	Ground floor elevation of all new buildings in the West End shall have universal access.	x												Community Development, Planning Commission, Council	Groundwork
4.9	WE	Drive-thrus, gas stations and automobile repair shops of any configuration should not be permitted on Mainstreet-abutting buildings in the West End.	x												Community Development, Planning Commission, Council	Groundwork
4.10	PA	Buildings on the northwest, southwest and southeast corners of Parker Road should be a minimum of 3 stories and a maximum of 5 stories with integrated signature architectural elements.							x						Community Development, Planning Commission, Council	Groundwork
4.11	PA	Minimize setbacks from both roadways on the Parker Road/Mainstreet corners.	x						x						Community Development, Planning Commission, Council	AA
4.12	OT	Mainstreet-abutting properties between Parker Road and the east leg of Victorian Drive should be a minimum of two and a maximum of four stories for the parcel on the southeast corner of Mainstreet and Parker Road should be minimum of three and a maximum of five stories.	x												Community Development, Planning Commission, Council	Groundwork
4.13	OT	On the south side of Mainstreet, including the PACE Center and the municipal campus, require two to three stories with up to 20% of the Mainstreet façade in front of the PACE Center being allowed to consist of small one-story elements to emphasize height variation.	x												Community Development, Planning Commission, Council	Groundwork
4.14	OT	Allow a maximum of one story on the eastern portion of the Plan area immediately adjacent to the Parker Vista neighborhood.	x						x						Community Development, Planning Commission, Council	Groundwork

Table 4 Implementation

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			Zoning or Overlay District	Incentives	Other Policy Update (plans, Engineering Details)	Marketing / Promotion	Business Recruitment	Design / Approvals Process	Publicly Funded Project	Privately Funded Project	Public Private Partnership	Further Study / Design				
4.15	WE	For development not abutting Mainstreet, buildings should be a minimum of two and a maximum of five stories.	x		x										Community Development, Planning Commission, Council	Groundwork
4.16	OT	All new buildings in Old Town shall adhere to a zero lot line on their Mainstreet frontage. Outdoor dining or merchant zones may be setback a maximum of ten feet with appropriate 42" tall fencing for dining areas. Doors should be recessed.	x												Community Development, Planning Commission, Council	Groundwork
4.17	OT	All new buildings in Old Town shall have primary, active pedestrian entrances facing Mainstreet.	x												Community Development, Planning Commission, Council	Groundwork
4.18	OT	Ground floor elevation of all new buildings in the Old Town shall have universal access.	x		x										Community Development, Planning Commission, Council	Groundwork
4.19	OT	Drive-thrus, gas station and automobile repair of any configuration are not permitted on Mainstreet-abutting buildings in Old Town.	x												Community Development, Planning Commission, Council	Groundwork
4.20	OT	Gradually transition new Old Town development using height and massing to existing, adjacent single-family residential development.	x						x						Community Development, Planning Commission, Council	AA
Architectural Character																
4.21	PA	New development should emphasize architectural authenticity in place of adhering to a prescribed style.	x						x						Community Development, Planning Commission, Council	Groundwork
4.22	PA	New development should not mix architectural character themes within a single building.	x						x						Community Development, Planning Commission, Council	Groundwork
4.23	PA	Use high quality materials and prohibit faux veneer finishes.	x						x						Community Development, Planning Commission, Council	Groundwork
4.24	PA	All new buildings shall have a minimum of 60% transparency at the ground level, and 30% transparency on upper levels.	x						x						Community Development, Planning Commission, Council	Groundwork
4.25	PA	All glass shall be transparent and all windows must have lintels and ledges consistent with the architectural style of the facade.	x						x						Community Development, Planning Commission, Council	Groundwork
4.26	PA	All new construction should provide architectural lighting to create visual nighttime interest.							x						Community Development, Planning Commission, Council	Groundwork

Table 4 Implementation

Plan Order	Plan Area	RECOMMENDATION	Potential Mechanism										Lead Department / Organization	Phase		
			Zoning or Overlay District	Incentives	Other Policy Updates (Plans, Engineering Details)	Marketing / Promotion	Business Recruitment	Design / Approvals Process	Publicly Funded Project	Privately Funded Project	Public Private Partnership	Further Study / Design				
4.27	PA	All architectural lighting shall comply with local energy efficiency and Dark Skies requirements.								x					Community Development, Planning Commission, Council	Groundwork
Sidewalk and Streetscape																
4.28	PA	Differentiate Mainstreet and other downtown streets through the use of wider, attached sidewalks.			x										Public Works, Community Development	AA
4.29	PA	Evaluate and inventory all ramps within the plan area. Upgrade missing and sub-standard/inconsistent ramps to meet Town of Parker and ADA standards, with an emphasis on consistency in the plan area									x			x	Public Works	1
4.30	PA	In tandem with redevelopment of the adjacent property, redesign the eastern intersection of Pine Drive and Mainstreet to be more pedestrian friendly including important design elements such as pedestrian refuges, reduced curve radii, directional ramps, crosswalks on all four sides of the intersection, stop lines and a higher level of finish and design for crosswalks.									x			x	Public Works, Community Development	AA
4.31	PA	Create a consistent higher level of finish and design for all crosswalks at Mainstreet intersections.									x				Public Works, Community Development	AA
4.32	PA	Work with CDOT to make improvements to the intersection and crosswalk at Parker Road and Mainstreet to improve pedestrian safety, comfort and convenience.									x			x	Public Works, Community Development	AA
4.33	PA	Evaluate and make upgrades to all crosswalks within the Plan area.											x	x	Public Works, Community Development	AA
4.34	PA	Design new development to provide access exclusively off secondary streets or alleys, with no new curb cuts on Mainstreet.								x					Community Development, Public Works	AA
4.35	PA	Work with existing development to evaluate the feasibility of closing existing Mainstreet curb cuts.												x	Public Works, Community Development	AA
4.36	PA	Use the existing lighting fixtures identified and constructed as part of the 2008 Mainstreet Streetscape improvements as the only street/pedestrian light combination to be used throughout the entire Plan area.			x						x				Public Works, Community Development	Groundwork

Table 4 Implementation

Plan Order	Plan Area	RECOMMENDATION	Potential Mechanism										Lead Department / Organization	Phase				
			Zoning or Overlay District	Other Policy Update (Plans, Engineering Details)	Incentives	Marketing / Promotion	Business Recruitment	Design / Approvals Process	Publicly Funded Project	Privately Funded Project	Public Private Partnership	Further Study / Design						
4.37	PA	Relocate existing Town owned 5-globe lights to medians and Mainstreet intersections and re-pattern their locations within the plan area.										x					Public Works, Community Development	AA
4.38	PA	Install a consistent line of street trees in the amenity zone that are spaced at 30 feet on center, along the entire length of Mainstreet. Where trees cannot be installed adjacent to the curb in the amenity zone, install at the same spacing at back of sidewalk										x	x	x			Public Works, Parks, Community Development	AA
4.39	PA	Locate all furnishings in the amenity zone or behind the sidewalk so as to maintain pedestrian circulation.				x											Public Works, Community Development	1
4.40	PA	Use public art as an additional common thread to tie together the public realm of the West End and Old Town.										x					Cultural, Community Development	AA
4.41	PA	Identify locations for permanent and temporary/rotating art installations.													x		Cultural Department, Community Development, Public Works	1
4.42	PA	Locate new utilities and relocate old utilities underground and outside the public realm, as opportunity arises.										x	x	x			Utility Companies, Public Works	AA
4.43	PA	Consider the effect of the location of underground utility lines and easements on the design of public and private spaces.				x				x					x		Utility Companies, Public Works, Community Development	AA
4.44	OT	Create a consistent streetscape throughout Old Town with a combined sidewalk/amenity zone of 15 – 18 feet. Sidewalk should be attached, with trees in grates at 30 feet on center. Colored concrete banding should follow tree spacing.				x						x	x	x			Public Works, Parks, Community Dev	AA
4.45	OT	Widen sidewalk and install trees at southwest corner of Pine Drive and Mainstreet with development.											x	x			Public Works	AA
4.46	OT	Install trees at 30 feet on center behind the sidewalk along the entire frontage of the Mainstreet Center.										x					Public Works, Parks	1

Table 4 Implementation

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			Zoning or Overlay District	Incentives	Other policy Updates (Plans, Engineering Details)	Marketing / Promotion	Business Recruitment	Design / Approvals Process	Publicly Funded Project	Privately Funded Project	Public Private Partnership	Further Study / Design				
4.47	OT	Redesign the sidewalk and streetscape along south edge of O'Brien Park to extend some of the Old Town streetscape elements west towards Parker Road.									x				Public Works, Parks, Community Development	1
4.48	OT	Reconstruct the sidewalk on south side of Mainstreet from Parker Road east to the strip mall drive to extend the Old Town streetscape elements.									x				Public Works, Community Dev	2
4.49	WE	Create a consistent streetscape throughout the West End with a 16-foot wide colored concrete sidewalk and trees in 8'x12' cut-outs at 30 feet on center.			x						x	x			Public Works, Community Development	AA
4.50	PA	Public streets other than Mainstreet should have a minimum 6' sidewalk and 8' amenity zone with trees planted at 30-feet on center in a sod or planted amenity zone.			x						x	x			Public Works, Community Development	2
Catalytic and Special Projects																
4.51	PA	Introduce art and other gateway elements at the Parker Road, Twenty Mile Road and Pine Drive entrances to the Plan Area.									x			x	Cultural, Community Dev, Public Works	1
4.52	PA	Parker Road and Mainstreet should be the primary gateway and the preferred gateway form would be a series of gateway art and/or monument components.									x				Cultural, Community Development, Public Works	AA
4.53	OT	The gateway monument at Pine Drive and Mainstreet should include an archway element over Mainstreet Located north of Town Hall.									x				Cultural, Community Development, Public Works	AA
4.54	WE	The gateway monument at Twenty Mile Road and Mainstreet should include art and monumentation that takes it cues from the Pine Drive and Parker Road gateway features.									x				Cultural, Community Development, Public Works	AA
4.55	PA	Coordinate wayfinding signage with other urban design elements in the Plan area.			x										Community Development	1
Table 4 Implementation																
4.56	PA	Introduce wayfinding based on information level so that all signage is complete and useful at every phase.									x				Community Development	1

Table 4 Implementation

Plan Order	Plan Area	RECOMMENDATION	Potential Mechanism										Lead Department / Organization	Phase		
			Zoning or Overlay District	Incentives	Other Policy Update (Plans, Engineering Details)	Marketing / Promotion	Business Recruitment	Design / Approvals Process	Publicly Funded Project	Privately Funded Project	Public Private Partnership	Further Study / Design				
4.57	WE	The following design principles shall provide direction for future Crossroads shopping center redevelopment: (1) provide signature architecture and massing at the Parker Road/Mainstreet intersection, (2) provide an architectural edge along the entire Parker Road edge of the property, (3) use building placement and design to integrate existing parks into the development and provide park programming and activation, (4) provide a significant plaza/gateway treatment at the west portal of Kieffer's Crossing.								x					Community Development, Planning Commission, Council	AA
4.58	WE	The following design principles shall provide direction for future Stage Run plaza design: (1) align the plaza to provide visual and physical connection between Mainstreet and the existing movie theater site, (2) provide visually interesting uses compatible with the underground utility easement, (3) provide 'rooms' or areas within the larger plaza with a particular eye to event programming, (4) provide scale, activity and enclosure through an architectural edge on the east and west sides of the plaza, rather than surface parking.								x					Community Development, Planning Commission, Council	AA
4.59	OT	Redesign Pilgrims Place to provide improved pedestrian access to Mainstreet and the strip mall area to the west.													Public Works, Community Development	1

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4.60	OT	Redesign Pilgrims Place as a shared, multi-modal 'flexible' space that can be closed as a special event space.									x				x	Public Works, Community Development, Parks	1
4.61	OT	Widen the sidewalk and add street trees and street amenities to create an urban public realm in front of the Town and Country townhomes adjacent to Mainstreet.									x					Public Works, Community Development	1
4.62	OT	The following design principles should provide direction for future PACE Lot 2 redevelopment: (1) maximize pedestrian access from Mainstreet to the PACE Center and to new development, (2) ensure active, ground-floor uses abutting Mainstreet, (3) incorporate public space and public art into the development, (4) provide primary facades on all four sides of new development and (5) utilize an architectural style that complements but does not duplicate or compete with the PACE Center.	x							x						Community Development, Economic Development, Cultural	AA
4.63	OT	Explore options to activate the southeast corner of PACE Center Drive and Mainstreet, giving priority to active uses that complement PACE and library activities.														Parks, Community Development	AA
Circulation Recommendations																	
Motor Vehicles																	
5.1	PA	Pursue circulation measures that create and promote alternate routes to Mainstreet and a finer grain, gridded roadway network.			x						x				x	Public Works, Community Development	AA
5.2	OT	Extend Pikes Peak Avenue from Pikes Peak Drive to Parker Road.										x	x	x		Public Works, Community Development, PPP	AA

Table 4 Implementation

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5.3	WE	Introduce new east-west segment of Pikes Peak Avenue between Parker Road and the north-south extension of Briargate Lane.									x	x	x	Public Works, Community Development, PPP	AA
5.4	WE	In coordination with development/redevelopment create a four-way, full movement signalized intersection at Mainstreet/Briargate Lane, in tandem with the closure of the adjacent Stage Run intersection.									x		x	Public Works, Community Development	AA
5.5	OT	In coordination with development and redevelopment, extend the east leg of Victorian Drive to Pikes Peak Avenue.									x		x	Public Works, Community Development	AA
5.6	OT	Extend Pikes Peak Avenue to Stonehenge Way.									x		x	Public Works, Community Development	AA
5.7	PA	Signalize the Longs Way and Parker Road intersection.									x		x	CDOT, Public Works	1
5.8	OT	Signalize the intersections of Mainstreet and Victorian Drive.									x		x	Public Works	AA
5.9	WE	In coordination with redevelopment, remove the existing signal at Mainstreet and Stage Run signal in tandem with the installation of the proposed Briargate Lane signal.									x		x	Public Works	AA
5.10	PA	Use marketing and promotional materials for downtown campaigns and event to promote alternate, non-Mainstreet routes for arriving downtown.					x							Chamber, Private Event Coordinators, Communications	1
5.11	PA	Use regional and local directional signage to promote alternate routes to Mainstreet, particularly for motorists driving to non Mainstreet destinations.					x							Public Works, CDOT, Community Development	1
5.12	OT	Maintain existing two-way function of Mainstreet.	n/a										n/a	n/a	
5.13	PA	Promote Twenty Mile Road and Longs Way as alternative, parallel routes to Mainstreet.					x							Public Works, CDOT, Communications	AA

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Bicycles															
5.14	PA	Use pilot projects to introduce new bicycle facilities in a measurable time frame and to fine-tune and test facilities.			x					x			x	Public Works, Community Development	1
5.15	PA	Leave pilot projects in place for a minimum of one year; support pilots with appropriate outreach and well-thought out evaluation metrics								x				Public Works	n/a
5.16	PA	Provide 'spur ' north-south connections for bicycles between Sulphur Gulch Trail and Mainstreet on the following roadways: Dransfeldt Road, Pikes Peak Drive, PACE Center Drive.								x				Public Works, Community Development	AA
5.17	WE	Construct an east-west trail connection over Cherry Creek between Clarkeville Way and Twenty Mile Road.								x				Public Works, Parks, Community Development	3
5.18	PA	When the Parker Road/Longs Way signal is installed, delineate an east-west bike lane along Longs Way connecting the Cherry Creek Trail to Old Town with buffered bike lanes from Twenty Mile Road to Brownstone Drive and bike lanes from Brownstone Drive to Victorian Drive.								x				Public Works, Community Development	Subject to Traffic Light at Longs Way and Parker Road
5.19	OT	Introduce striped on-street bike lanes along the entire length of Victorian Drive.								x				Public Works	2
5.20	OT	Convert existing angle parking on the east side of Parker Station to parallel parking in order to allow for recommended bike lanes.								x				Public Works	2
5.21	OT	Introduce an off-road, shared-use path on PACE Center Drive.								x				Public Works	1
5.22	WE	Introduce an off-road, shared use path on the west side of Dransfeldt Road between Mainstreet and the Sulphur Gulch Trail.								x				Public Works	2
5.23	WE	Introduce an off-road, shared use path on the west side of Dransfeldt Road between Mainstreet and Longs Way.								x				Public Works	2
5.24	WE	Consider introducing sharrows on Stage Run (a private street).								x			x	Public Works, Community Development	2

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5.25	WE	Continue the future Ridgeway/Mainstreet multi-use trail on the south side of Mainstreet from Twenty Mile Road to Dransfeldt Road.														x						Public Works, Community Development	1	
5.26	WE	Introduce improved wayfinding signage, on-street sharrows and widened sidewalks on Pikes Peak Drive.														X						Public Works, Community Development	1	
5.27	OT	Study and consider a long term outcome that requires acquiring additional right-of-way, removing on-street parking on one side and introduce on-street bike lanes with improved sidewalks on Pikes Peak Drive.														x						Public Works, Community Development	3	
5.28	PA	Designate 'inverted U' racks as the standard for downtown bicycle parking and encourage/incorporate artistic bike racks.														x						Public Works, Cultural, Community Development	1	
5.29	PA	Locate bike parking in high-visibility areas within 50 feet of destinations.						x														Public Works, Community Development	AA	
5.30	PA	Install bicycle racks with sufficient clear distances from roadway, other racks, and adjacent vertical elements.						x														Public Works, Community Development	1	
5.31	OT	Consider installation of a bike corral within the core area of Old Town.						x														Public Works, Community Development	1	
5.32	PA	Organize group rides and events to advertise and promote the use of new bicycle facilities.								x												Communication	1	
Pedestrians																								
5.33	WE	Introduce a mid-block pedestrian crossing on Mainstreet between Twenty Mile and Dransfeldt Roads in coordination with development of the park/plaza to the north.														x						Public Works, Community Development	AA	
5.34	PA	Introduce additional crosswalk enhancement to increase visibility, safety, comfort and convenience at priority pedestrian intersections.														x						Public Works, Community Development	1	
5.35	PA	Work with CDOT to develop pedestrian enhancements to improve the safety, comfort and aesthetics of the Mainstreet and Parker Road intersection														x						Public Works, Community Development	1	

Table 4 Implementation

Plan Order	Plan Area	RECOMMENDATION	Potential Mechanism										Lead Department / Organization	Phase			
			Zoning or Overlay District	Incentives	Other Policy Update (Plans, Engineering Details)	Marketing / Promotion	Business Recruitment	Design / Approvals Process	Publicly Funded Project	Privately Funded Project	Public/Private Partnership	Further Study / Design					
5.36	PA	Re-align Kieffer's Crossing to allow an unobstructed line of sight from one end to the other, reconfigure entries to integrate into adjacent context and provide more prominent wayfinding signage.										x				Public Works, Community Development	1
Public Transit and Shuttles																	
5.37	PA	Explore the potential for a shared-use Park-n-Ride/public parking structure more convenient to Mainstreet.												x	x	Council, Community Development, RTD, PPP	1
5.38	WE	Maintain enough right of way to allow for two lanes and wide sidewalks along the potential BRT route in order to allow for construction of the future facility.	n/a										n/a	n/a			
5.39	PA	Investigate potential special-event shuttle route in tandem with a temporary parking management plan.													x	Public Works, Community Development, Communications	1
5.40	PA	Consider introduction of a Downtown Shuttle once sufficient use and user density has been reached.													x	Public Works, Community Development	AA
Parking																	
5.41	PA	Maintain existing public parking locations and quantities until parking structure is built.	n/a										n/a	n/a			
5.42	PA	Continue to evaluate shared parking opportunities as new development is proposed.								x					x	Community Development	AA
5.43	OT	Close the Mainstreet access drive to Mainstreet Center parking in coordination with the extension of the east leg of Victorian Drive south of Mainstreet.										x				Public Works, Cultural, Community Development	AA
5.44	OT	Close the Mainstreet access drive to the parcel on the northeast corner of Mainstreet and Victorian Way in coordination with redevelopment.										x				Public Works, Community Development	AA
5.45	OT	Close the Mainstreet access drive to the parcel on the southeast corner of Mainstreet and Pikes Peak Drive.										x				Public Works, Community Development	AA
5.46	WE	Introduce an alley curb cut in the West End in tandem with expansion of pedestrian uses to the Mainstreet right-of-way edge.										x				Public Works	AA

Table 4 Implementation

Plan Order	Plan Area	RECOMMENDATION	Potential Mechanism										Lead Department / Organization	Phase	
			Zoning or Overlay District	Incentives	Other policy Update (Plans, Engineering Details)	Marketing / Promotion	Business Recruitment	Design / Approvals Process	Publicly Funded Project	Privately Funded Project	Public Private Partnership	Further Study / Design			
5.47	PA	Prepare a strategic special event parking plan that includes consideration of the following: <ul style="list-style-type: none"> • Temporary event parking on the Town-owned parcels • Valet parking locations and management for events • Shuttle bus service from outlying lots 											x	Community Development, Police, Communications, Public Works	1
5.48	OT	Investigate a formal jointuse agreement for weekend use of the RTD Park-n-Ride lot.											x	Community Development, RTD	1
5.49	PA	Evaluate a parking cap or structured parking requirement for new development.			x								x	Community Dev	1
5.50	PA	Commission a full parking study to analyze current parking utilization, appropriate short term management strategies and long term parking solutions. This parking study should include, but not be limited to, consideration of the following: <ul style="list-style-type: none"> • Introduce time limits on street parking, with appropriate enforcement • Installation of real-time dynamic parking signage showing parking availability • Valet parking • Parking wayfinding signage • Evaluation of a parking cap or structured parking requirement for new development • Evaluation of a parking district or fee based system for new development • Evaluation of the size, location, configuration and timing of a future parking structure 			x								x	Community Development, Public Works	1

Table 4 Implementation

Plan Order	Plan Area	RECOMMENDATION	Potential Mechanism										Lead Department / Organization	Phase
			Zoning or Overlay District	Incentives	Other Policy Update (Plans, Engineering Details)	Marketing / Promotion	Business Recruitment	Design / Approvals Process	Publicly Funded Project	Privately Funded Project	Public Private Partnership	Further Study / Design		
5.51	PA	Track land use, property sales and development proposals which may present opportunity for or preclude a future parking structure.	n/a										Economic Development	Groundwork

PA: Plan Area WE: West End OT: Old Town

Phases

Groundwork: Immediate
 Phase 1: 0-5 years
 Phase 2: 6-10 years
 Phase 3: 11-20 years
 AA: As Appropriate/As Opportunity Arises

Notes:

1. Policy Change: policy changes are assumed to be absorbed into normal staff and Council workload and have no additional cost.
2. Marketing Task: marketing/promotion is assumed to be absorbed into normal tasks and budget of Town, Chamber and other similar entities.
3. Private Development: non-site specific private development or public-private partnership cannot be estimated without site and use information.

Table 4 Implementation